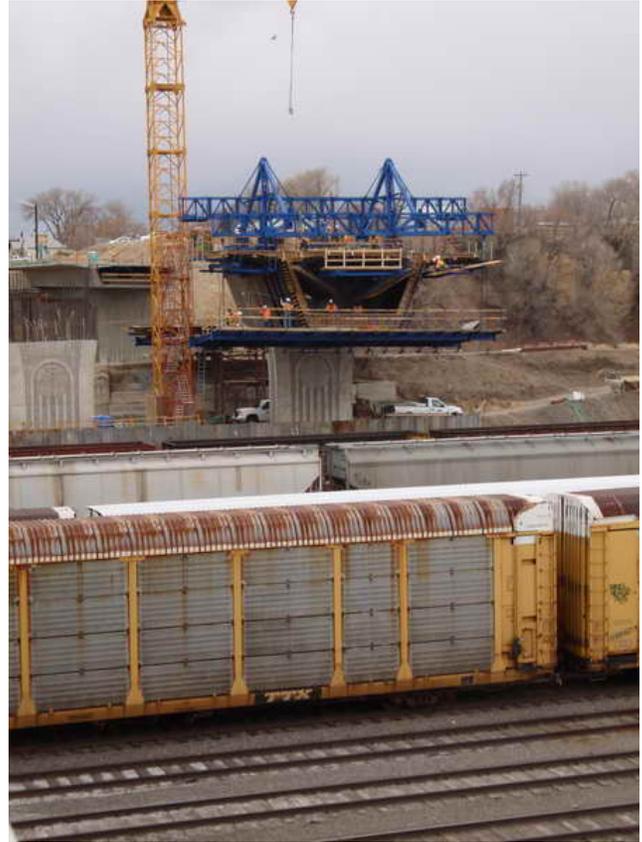


### Recent Construction Highlights

Flatiron Constructors Intermountain cast the first main-span segment, launched the upstation form traveler, erected the downstation form traveler, and cast the first side-span segment at Cantilever 3 WB. Also, construction continued at Pier Table 4 WB with placing the bottom slab, Span 5 WB was completed with placement of the top slab, and Pier Table 3 EB falsework assembly has begun. The following is a summary of the construction progress for the last month.

**Figure 1 – Cantilever 3 WB Segmental Construction – February 10, 2009:**

With assembly of the upstation form traveler complete, Flatiron begins forming and installing reinforcing for Segment W3-1W, the first main-span segment to be cast.



**Figure 2 – Pier Table 4 WB Construction – February 11, 2009:**

A BNSF coal train passes by Pier Table 4 WB, as crews continue to form and install reinforcing and post-tensioning for the bottom slab pour. Minimum temporary clearances are required on all falsework, including the form traveler, by both the BNSF and UPRR Railroads.

**Figure 3 – Pier Table 4 WB Construction – February 18, 2009:**  
The reinforcing and post-tensioning installation for the webs and diaphragm nears completion.



**Figure 4 – Cantilever 3 WB Segmental Construction – February 24, 2009:**  
The upstation form traveler launches on to the newly cast Segment W3-1E to allow for installation of the downstation form traveler.

**Figure 5 – Pier Table 3 WB Form Traveler Erection – February 25, 2009:**  
The interior section of the rear transverse truss is flown in between the saw-horses of the downstation form traveler.





**Figure 6 – Pier Table 4 WB Construction – February 25, 2009:**  
Flatiron makes final preparations for casting the bottom slab of the Pier Table.

**Figure 7 – Pier Table 4 WB Construction – February 26, 2009:**  
The crew places and consolidates the concrete for the bottom slab portion at Pier Table 4 WB.



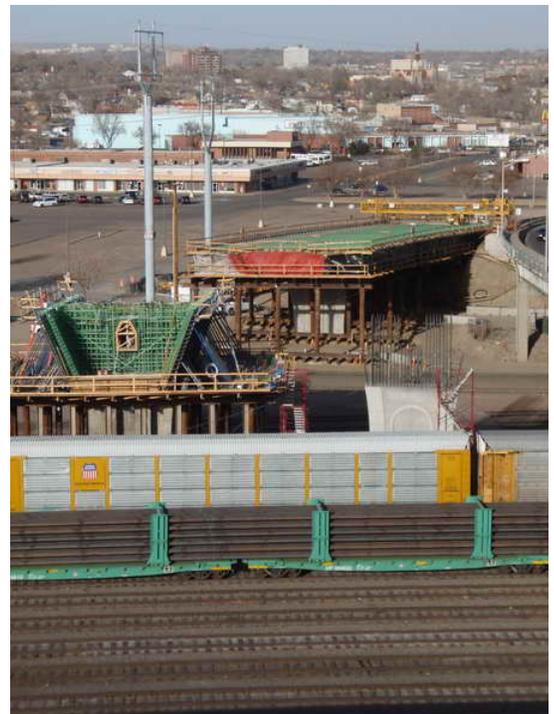
**Figure 8 – Span 5 WB CIP Superstructure Construction – March 4, 2009:**  
The crews continue forming and installing the reinforcing and post-tensioning for the top slab of Span 5 WB. The vertical post-tensioning bars in Pier 5 WB diaphragm are visible at the bottom of the photo.

**Figure 9 – Cantilever 3 WB Segmental Construction– March 5, 2009:**  
The downstation traveler assembly is complete and forming continues for Segment W3-1W, the first side-span segment.



**Figure 10 – Cantilever 3 WB Segmental Construction– March 5, 2009:**  
Looking down from the tower crane, the form traveler rails can be seen overlapping each other. The downstation traveler (left) contains two rails full length to straddle the single rail of the upstation traveler (right) when configured over the pier table.

**Figure 11 – Span 5 WB CIP Superstructure Construction– March 5, 2009:**  
Span 5 WB forming and installation of reinforcing and post-tensioning is complete. The Bidwell finishing machine is near Abutment 6, where concrete placement will begin the following day.





**Figure 12 – Span 5 WB CIP Superstructure Construction– March 6, 2009:**

Concrete placement occurs for the top slab portion of Span 5 WB. The “pink tops” shown in the photo are attached to the barrier reinforcing for future locating purposes. The construction phasing requires four lanes of traffic to be placed on the westbound bridge prior to completing the eastbound bridge. Then, it requires coupling of the barrier reinforcing after two lanes are placed on the eastbound structure.

**Figure 13 – Cantilever 3 WB Segmental Construction– March 11, 2009:**  
Flatiron continues forming and installing reinforcing and post-tensioning for Segment W3-1W.



**Figure 14 – Cantilever 3 WB Segmental Construction– March 11, 2009:**

Work begins on Segment W3-2E over the UPRR railroad tracks, as Segment W3-1W nears completion. Concrete placement is scheduled for Segment W3-1W on Friday, March 13<sup>th</sup>.



**Project Summary:**

March 13, 2009  
 Day 453 of 1278

<b>Substructure Construction</b>	<b><u>To</u> <u>Date</u></b>		<b><u>Total</u></b>	<b><u>Unit</u></b>	<b><u>% Complete</u></b>
48" Diameter Drilled Shafts (Monuments)	3	of	4	Each	75%
48" Diameter Drilled Shafts (Abutments)	11	of	14	Each	79%
60" Diameter Drilled Shafts (Pier 2 & 5)	6	of	8	Each	75%
96" Diameter Drilled Shafts (Pier 3 & 4)	8	of	8	Each	100%
Type I Footings (Pier 2 & 5)	3	of	4	Each	75%
Type II Footings (Pier 3 & 4)	4	of	4	Each	100%
3'-6" Piers (Pier 2 & 5)	3	of	4	Each	75%
7'-1" Piers (Pier 3 & 4)	4	of	4	Each	100%
Abutments	3/4	of	2	Each	38%

<b>Superstructure Construction</b>	<b><u>To</u> <u>Date</u></b>		<b><u>Total</u></b>	<b><u>Unit</u></b>	<b><u>% Complete</u></b>
<b>Westbound</b>					
End Span CIP Westbound	2	of	2	Each	100%
Abutment Diaphragm Westbound	2	of	2	Each	100%
Pier Diaphragm Westbound	2	of	2	Each	100%
Pier Table Westbound	1	of	2	Each	50%
Cantilever 3 Segments Westbound	2	of	22	Each	9%
Cantilever 4 Segments Westbound	0	of	20	Each	0%
Closure Segments Westbound	0	of	3	Each	0%
<b>Eastbound</b>					
End Span CIP Eastbound	0	of	2	Each	0%
Abutment Diaphragm Eastbound	0	of	2	Each	0%
Pier Diaphragm Eastbound	0	of	2	Each	0%
Pier Table Eastbound	0	of	2	Each	0%
Cantilever 3 Segments Eastbound	0	of	22	Each	0%
Cantilever 4 Segments Eastbound	0	of	20	Each	0%
Closure Segments Eastbound	0	of	3	Each	0%



**Project Summary:**

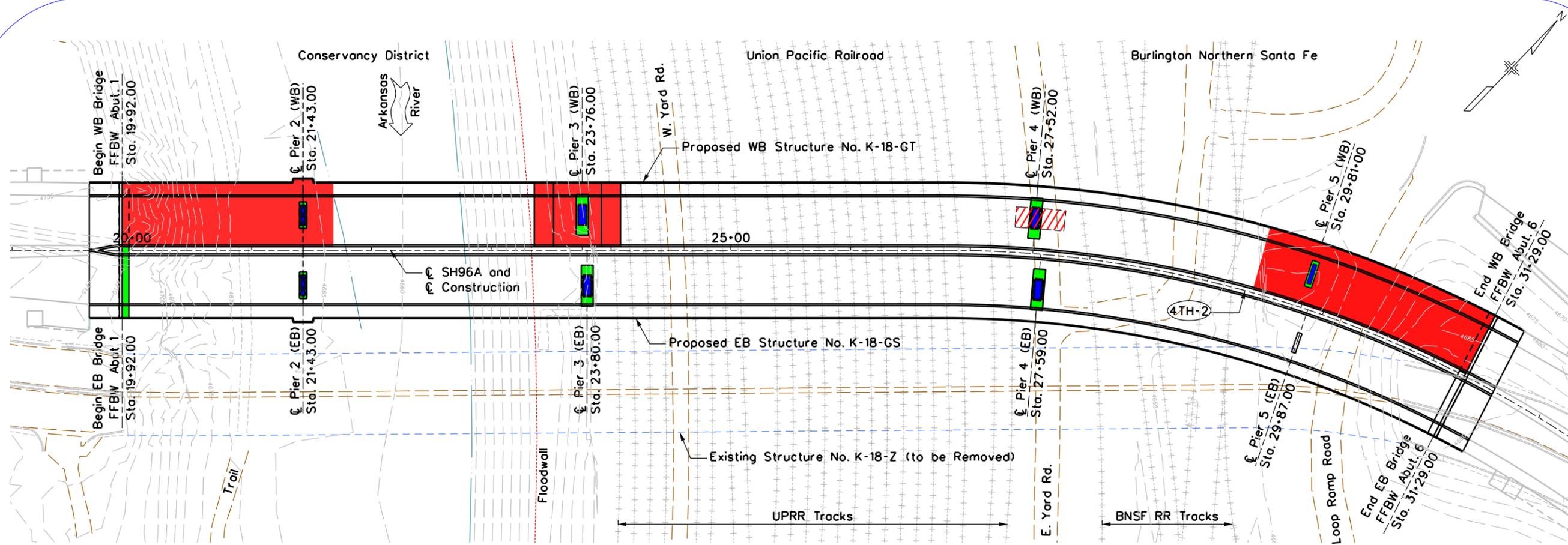
March 13, 2009  
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**Project Milestone Dates**

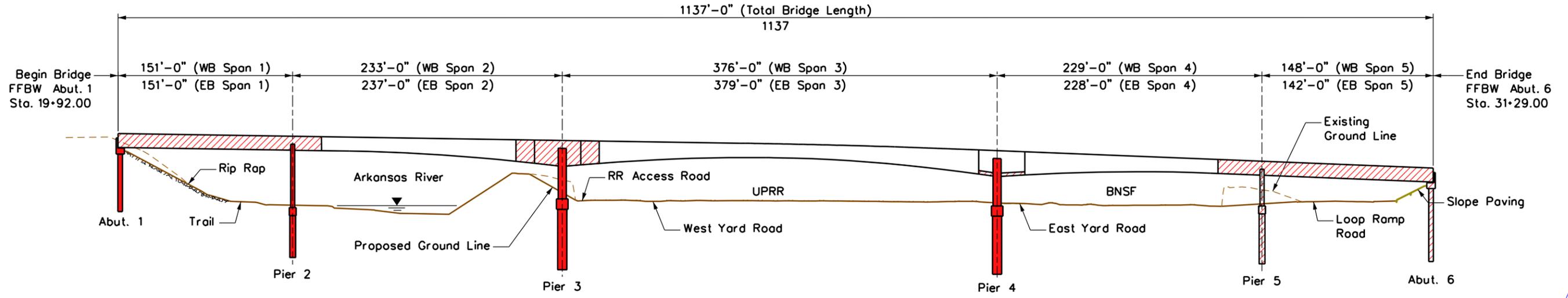
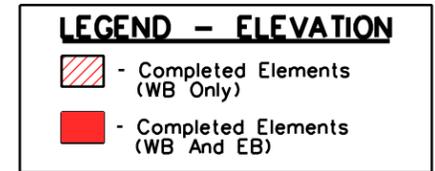
Milestone Event	April 2008 Baseline Finish Date	Actual
Project Award	October 18, 2007	October 18, 2007
Notice to Proceed	November 8, 2007	November 8, 2007
Abutment 1 Drill Caissons	February 15, 2008	February 15, 2008
Abutment 1 Cap Form/Rebar/Pour	March 6, 2008	March 6, 2008
Pier 2 EB Drill Caissons	March 3, 2008	March 3, 2008
Pier 2 WB Form/Rebar/Pour Footing	March 24, 2008	March 24, 2008
Pier 2 WB Column Form/Rebar/Pour	April 29, 2008	April 29, 2008
Pier 3 EB Drill Caissons	April 17, 2008	May 1, 2008
Pier 3 WB Form/Rebar/Pour Footing	May 15, 2008	June 4, 2008
Pier 3 WB Column Form/Rebar/Pour	July 1, 2008	August 5, 2008
Pier 4 EB Drill Caissons	May 8, 2008	July 1, 2008
Pier 4 WB Form/Rebar/Pour Footing	May 13, 2008	August 8, 2008
Pier 4 WB Column Form/Rebar/Pour	August 20, 2008	September 18, 2008
Pier 5 WB Drill Caissons	April 17, 2008	May 30, 2008
Pier 5 WB Form/Rebar/Pour Footing	April 21, 2008	June 12, 2008
Pier 5 WB Column Form/Rebar/Pour	October 21, 2008	July 11, 2008
Abutment 6 WB Drill Caissons	April 18, 2008	April 22, 2008
Abutment 6 WB Cap Form/Rebar/Pour	May 8, 2008	May 8, 2008
Span 1 WB Form/Rebar/Pour Bottom Slab/Webs/Diaphragms	June 17, 2008	August 29, 2008
Span 1 WB Form/Rebar/Pour Deck	July 8, 2008	October 10, 2008
Pier Table 3 WB Form/Rebar/Pour Bottom Slab	August 18, 2008	November 21, 2008
Pier Table 3 WB Form/Rebar/Pour Diaphragm & Webs	September 4, 2008	December 5, 2008
Pier Table 3 WB Form/Rebar/Pour Deck	October 7, 2008	December 31, 2008
Span 5 WB Form/Rebar/Pour Bottom Slab/Webs/Diaphragms	February 12, 2009	January 29, 2009
Span 5 WB Form/Rebar/Pour Deck	March 12, 2009	March 6, 2009
Span 1 EB Form/Rebar/Pour Bottom Slab/Webs/Diaphragms	October 7, 2008	
Span 1 EB Form/Rebar/Pour Deck	October 27, 2008	
Form and Pour First Segment – W3-1E	November 19, 2008	February 16, 2009
Form and Pour First Closure – Span 2 WB	May 19, 2009	
Shift Traffic to New WB Structure	February 17, 2010	
Install Last Drilled Caissons – Abutment 6 (EB Only)	April 26, 2010	
Form and Pour Last Segment – E4-10E	October 12, 2010	
Form and Pour Last Closure – Span 3 EB	November 16, 2010	
Complete Structure and Final Traffic Configuration	March 4, 2011	

**All items are based on the April 2008 Baseline Schedule. All dates represent the “Finish” of the activity, unless otherwise noted.**

The actual milestone dates are later than originally projected. This is mainly due to the drilled shaft subcontractor requiring more time for drilling the 8' diameter shafts than originally anticipated. Cantilever construction is critical path, and Flatiron is constructing Pier Table 4 WB simultaneous with Cantilever 3 WB. This will allow installation of the main-span form traveler immediately after Cantilever 3 WB construction is complete.



**PLAN**



**ELEVATION**

