

SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## **Appendix D: Agency Coordination**



**Appendix D  
Table of Contents**

Document	Subject
<b>Historic Preservation — Section 106</b>	
Pages 1-186	
November 4, 2011 Letter To: Mark Wolfe or Ms. Amy Pallante, Colorado Historical Society From: Josh Cullen, CDOT-Region 3	Agency Scoping Meeting
November 4, 2011 Letter To: Cindy Hines, Frontier Historical Society From: Josh Cullen, CDOT-Region 3	Agency Scoping Meeting
February 7, 2012 Meeting Minutes With: Colorado Historical Society, Office of Archaeology and Historic Preservation	Define the APE and Discuss Survey Needs
August 2, 2013 Letter To: Edward Nichols, State Historic Preservation Officer, Colorado Historical Society From: Jane Hann, CDOT Environmental Programs Branch	Determinations of Eligibility and Effects
August 5, 2013 Letter To: Jane Daniels, Colorado Preservation Incorporated From: Jane Hann, CDOT Environmental Programs Branch	Determinations of Eligibility and Effects
August 5, 2013 Letter To: Cindy Hines, Frontier Historical Society From: Jane Hann, CDOT Environmental Programs Branch	Determinations of Eligibility and Effects
August 5, 2013 Letter To: Gretchen Ricehill, City of Glenwood Springs Historic Preservation Commission From: Jane Hann, CDOT Environmental Programs Branch	Determinations of Eligibility and Effects
August 9, 2013 Letter To: John Martin & Tom Jankovsky, Board of County Commissioners From: Jane Hann, CDOT Environmental Programs Branch	Determinations of Eligibility and Effects
August 14, 2013 Letter To: Jane Hann, CDOT Environmental Programs Branch From: Edward Nichols, State Historic Preservation Officer, History Colorado	Determinations of Eligibility and Effects
August 15, 2013 Letter To: Jane Hann, CDOT Environmental Programs Branch From: Gretchen Ricehill, City of Glenwood Springs Community Development Department	Determinations of Eligibility and Effects
March 28, 2014 Letter To: Edward Nichols, State Historic Preservation Officer, Colorado Historical Society From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Eligibility and Effects and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception
April 2, 2014 Letter To: John Martin and Tom Jankovsky, Board of County Commissioners From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Eligibility and Effects

**Appendix D  
Table of Contents**

Document	Subject
April 2, 2014 Letter To: Cindy Hines, Frontier Historical Society From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Eligibility and Effects
April 2, 2014 Letter To: Jane Daniels, Colorado Preservation Incorporated From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Eligibility and Effects
April 2, 2014 Letter To: Gretchen Ricehill, City of Glenwood Springs Historic Preservation Commission From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Eligibility and Effects
April 24, 2014 Letter To: Jane Hann, CDOT Environmental Programs Branch From: Edward Nichols, State Historic Preservation Officer	Concurrence on Eligibility and Effects, and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception
August 27, 2014 Letter To: Edward Nichols, State Historic Preservation Officer, Colorado Historical Society From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Eligibility and Effects and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception
August 27, 2014 Letter To: Rachel Parris, Colorado Preservation Incorporated From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Determinations of Eligibility and Effect
August 27, 2014 Letter To: Gretchen Ricehill, City of Glenwood Springs Historic Preservation Commission From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Determinations of Eligibility and Effect
August 27, 2014 Letter To: John Martin and Tom Jankovsky, Board of County Commissioners From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Determinations of Eligibility and Effect
August 27, 2014 Letter To: Cindy Hines, Frontier Historical Society From: Jane Hann, CDOT Environmental Programs Branch	Additional Information on Determinations of Eligibility and Effect
September 5, 2014 To: Jane Hann, CDOT Environmental Programs Branch From: Edward Nichols, State Historic Preservation Officer	Concurrence on Effect Determinations, and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception
September 15, 2014 To: Lisa Schoch, CDOT Environmental Programs Branch From: Rachel Parris, Colorado Preservation Inc.	Determinations are acceptable; no additional comment.
<b>Wildlife</b> <span style="float: right;">Pages 187-204</span>	
October 25, 2012 Meeting Minutes With: Colorado Parks and Wildlife	Wildlife and Aquatic Species Construction Recommendations

**Appendix D  
Table of Contents**

Document	Subject
March 6, 2013 Meeting Minutes With: Colorado Parks and Wildlife	Wildlife and Aquatic Species Construction Recommendations
April 17, 2013 Letter To: Alison Dean-Michael, U.S. Fish and Wildlife Service From: Jeff Peterson, CDOT Wildlife Programs Manager	Federally Listed Species Assessment
May 9, 2013 Letter To: Dan Soucy, Jacobs Engineering Group, Inc. From: Michael Menefee, Colorado State University for Colorado Natural Heritage Program	Natural Heritage Resources
December 9, 2013 To: Jeff Peterson, CDOT Wildlife Programs Manager From: Susan C. Linner, U.S. Fish and Wildlife Service	Agreement Letter for Federally Listed Species
<b>Hazardous Materials</b> <span style="float: right;">Pages 205-224</span>	
April 17, 2013 E-Mail To: Ann Crouse, Department of Public Health and Environment From: Robyn Kullas, Pinyon Environmental Inc.	Environmental Site Assessment Request for Files
April 17, 2013 E-Mail To: Diana Huber, Department of Public Health and Environment From: Robyn Kullas, Pinyon Environmental Inc.	Environmental Site Assessment Request for Files
April 22, 2013 E-Mail To: Barbara Morris, Glenwood Springs Fire Department From: Robyn Kullas, Pinyon Environmental Inc.	Environmental Site Assessment Records Request
April 22, 2013 File Review Request Form To: Department of Labor and Employment, Division of Oil and Public Safety From: Robyn Kullas, Pinyon Environmental Inc.	Public Records – File Review Request Form
<b>Water Resources</b> <span style="float: right;">Pages 225-226</span>	
April 18, 2013 Call To: Eddie Rubin, Department of Natural Resources From: Robyn Kullas, Pinyon Environmental Inc.	Contact Report – Public Water Supply Sources Within the Study Area
<b>Relocation/Right-of-Way Impacts</b> <span style="float: right;">Pages 227-228</span>	
June 19, 2013 Letter To: John Cater, Federal Highway Administration From: Joseph Elsen, Colorado Department of Transportation	FHWA Concurrence Regarding Permissible Design Activities
<b>Other Resources — Archaeological</b> <span style="float: right;">Pages 229-232</span>	
January 16, 2014 Letter To: Jimmy R. Newton, Jr., Chairman, Southern Ute Indian Tribe From: John M. Cater, FHWA Division Administrator by Stephanie Gibson, Environmental Program Manager Includes list of additional letter recipients.	Request for Section 106 Consultation

**Appendix D  
Table of Contents**

Document	Subject
<b>Wetlands and Waters of the United States</b>	
	Pages 233-234
February 21, 2014 Meeting Minutes on Call With: U.S. Army Corps of Engineers	Waters of the United States Impacts/Section 404 Issues
<b>Section 6(f)</b>	
	Pages 235-236
September 17, 2014 To: Thomas Morrissey, Melanie Gose, Colorado Parks and Wildlife From: Troy Halouska, Planning and Environmental Linkages Program Manager, 4(f)/6(f) Specialist, Colorado Department of Transportation	Section 6(f) Question and Boundary Concurrence
October 8, 2014 To: Troy Halouska, Planning and Environmental Linkages Program Manager, 4(f)/6(f) Specialist, Colorado Department of Transportation From: Thomas Morrissey, Colorado Parks and Wildlife	Agreement on Section 6(f) Boundary and No Direct Park Impacts

SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Historic Preservation — Section 106



# STATE OF COLORADO

## DEPARTMENT OF TRANSPORTATION

Region 3  
222 South Sixth Street, Room 317  
Grand Junction, CO 81501-2769



November 4, 2011

Mark Wolfe or Ms. Amy Pallante  
Colorado Historical Society  
Office of Archaeology and Historic Preservation  
1200 Broadway  
Denver, CO 80203

**Re: Colorado Department of Transportation, Region 3  
SH 82 Grand Avenue Bridge Project  
Agency Scoping Meeting**

Dear Mr. Wolfe or Ms. Pallante:

The Colorado Department of Transportation in conjunction with the Federal Highway Administration, is preparing an Environmental Assessment to study transportation improvements to address the functionally obsolete Grand Avenue Bridge on SH 82 that connects Glenwood Springs across the Colorado River.

**We have scheduled a resource agency scoping meeting on November 15, 2010, from 1:00 p.m. until 3:00 p.m.** to be held **at the Glenwood Community Center on 100 Wulfsohn Road** (see map below). At the meeting, we will provide information on the project purpose and need, a preliminary assessment of environmental issues present within the study area, and a project schedule. We would like to hear from you about any issues of concern or areas of analysis you believe will require special consideration as we move forward with this project. We will also ask for your help in identifying the resources that should be evaluated for cumulative effects.

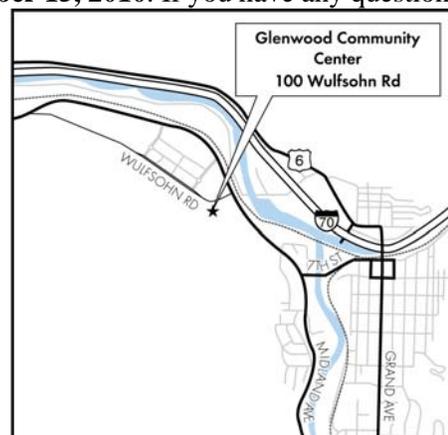
Please respond to Jim Clarke at 303-820-5218 or [jim.clarke@jacobs.com](mailto:jim.clarke@jacobs.com) about your availability. If you are unable to attend, please designate another person from your agency to attend. Alternately, you are invited to submit written scoping comments by December 1, 2010, to Mr. Clarke at Jacobs, 707 17<sup>th</sup> Street, Suite 2300, Denver, CO 80202.

We look forward to seeing you or your representative on **November 15, 2010**. If you have any questions, please feel free to call Jim Clarke at 303-820-5218.

Sincerely,

Josh Cullen  
CDOT Project Manager

cc: Eva LaDow, FHWA  
Tammie Smith, CDOT – Region 3  
C. Gaskill, Jacobs



# STATE OF COLORADO

## DEPARTMENT OF TRANSPORTATION

Region 3  
222 South Sixth Street, Room 317  
Grand Junction, CO 81501-2769



November 4, 2011

Cindy Hines  
Frontier Historical Society  
1001 Colorado Avenue  
Glenwood Springs, CO 81601

**Re: Colorado Department of Transportation, Region 3  
SH 82 Grand Avenue Bridge Project  
Agency Scoping Meeting**

Dear Ms. Hines:

The Colorado Department of Transportation in conjunction with the Federal Highway Administration, is preparing an Environmental Assessment to study transportation improvements to address the functionally obsolete Grand Avenue Bridge on SH 82 that connects Glenwood Springs across the Colorado River.

**We have scheduled a resource agency scoping meeting on November 15, 2010, from 1:00 p.m. until 3:00 p.m.** to be held **at the Glenwood Community Center on 100 Wulfsohn Road** (see map below). At the meeting, we will provide information on the project purpose and need, a preliminary assessment of environmental issues present within the study area, and a project schedule. We would like to hear from you about any issues of concern or areas of analysis you believe will require special consideration as we move forward with this project. We will also ask for your help in identifying the resources that should be evaluated for cumulative effects.

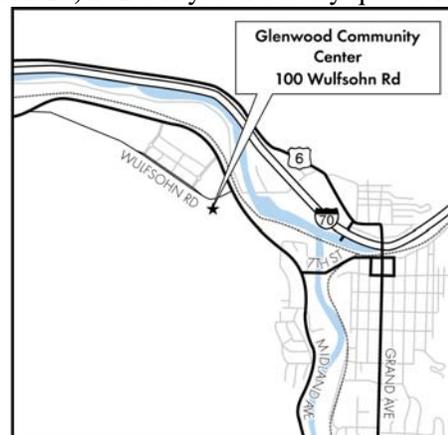
Please respond to Jim Clarke at 303-820-5218 or [jim.clarke@jacobs.com](mailto:jim.clarke@jacobs.com) about your availability. If you are unable to attend, please designate another person from your agency to attend. Alternately, you are invited to submit written scoping comments by December 1, 2010, to Mr. Clarke at Jacobs, 707 17<sup>th</sup> Street, Suite 2300, Denver, CO 80202.

We look forward to seeing you or your representative on **November 15, 2010**. If you have any questions, please feel free to call Jim Clarke at 303-820-5218.

Sincerely,

Josh Cullen  
CDOT Project Manager

cc: Eva LaDow, FHWA  
Tammie Smith, CDOT – Region 3  
C. Gaskill, Jacobs



## Meeting Minutes

**Project:** CDOT Region 3 – SH 82 Grand Avenue Bridge

**Purpose:** Define the APE and discuss Survey Needs

**Date Held:** February 7, 2012

**Location:** CHS, Office of Archaeology and Historic Preservation, 1200 Broadway, Denver

**Attendees:**

**CDOT:** Lisa Schoch  
**CHS:** Amy Pallante  
**Hermesen Consultants:** Gail Keeley  
**Jacobs:** Jim Clarke

**Copies:** File

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### SUMMARY OF DISCUSSION:

The purpose of the meeting was to define the APE and discuss survey needs. Jim provided a general overview of the project. Gail briefly described the historic properties within the APE.

1. There are two field eligible historic districts in the study area - - the Hot Springs District and the Downtown district. Neither of them have had an official determination of eligibility. Lisa and Amy both felt that this project would provide a good opportunity to look at the boundaries of those potential districts. The Hot Springs District boundaries currently include the Hotel Colorado, the Hot Springs Lodge and the D & RG Railroad station on the south side of the river. The bridge is within this district. However, the bridge was built in 1953 and is not within the period of significance for the Hot Springs.
2. Gail said that the properties along Grand Ave. were most intact in the 700 block and that there were many buildings of varying ages and architectural styles in the 800 and 900 blocks of Grand Ave. The City has never designated any historic districts. Amy said that for the purposes of Section 106 review, we could consider a commercial downtown district even though the city hasn't designated one. That way we could assess impacts to the district.
3. A discussion ensued on survey needs. Gail described how most of the properties on Grand Ave. had previously been surveyed. Some surveys were prepared in 1981 on the pink and green survey forms and other surveys were prepared in the late 1990's. The 1981 surveys

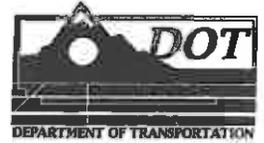
did not include much information whereas the late 1990's surveys were very complete. Amy felt that new survey forms should be prepared for all of the 1981 surveys and that re-visitation forms could be prepared for the more complete surveys that had been prepared in the late 1990's. It was agreed the main survey effort would be in the 700 block of Grand Ave. since the bridge touchdown would be within that block. Surveys would also be prepared on the north side of the Colorado River along 6th St. to the west of the Hot Springs Pool.

4. A discussion followed on boundaries for the Area of Potential Effects (APE). Since there are two potential historic districts in the area, it was agreed that the boundary for the APE should generally follow the boundaries of those districts. There is the possibility that the bridge structure may curve to the west on the north side of the river in order to make a better connection to I-70. A potential APE line was drawn to include the area to the west of the hot springs district. The parking lot for the hot springs is included within the APE. The APE line may be revised depending on the actual definition of the boundaries for the historic districts.
5. Current options under consideration include one-way couplets on Grand Ave. and Colorado Ave. or on Grand Ave. and Cooper Ave. It is likely that the new bridge will span the entire river. Level II and Level III screening of alternatives will take place in the upcoming months. The preferred alternative will be selected by August 2012. Amy would like all the alternatives considered described in CDOT's letter to the SHPO requesting their determination of eligibility and effects.

# STATE OF COLORADO

## DEPARTMENT OF TRANSPORTATION

Environmental Programs Branch  
4201 East Arkansas Avenue  
Shumate Building  
Denver, Colorado 80222  
(303) 757-9281



August 2, 2013

Mr. Edward Nichols  
State Historic Preservation Officer  
History Colorado  
1200 Broadway  
Denver, CO 80203

**SUBJECT:** Determinations of Eligibility and Effects, Notification of Section 4(f) *DeMinimis* and Section 4(f) Exception for Temporary Occupancy Criteria, Project FBR 0821-094, Grand Avenue Bridge Replacement, Glenwood Springs

Dear Mr. Nichols:

This letter and associated materials constitute a request for concurrence on Determinations of Eligibility and Effects for the project referenced above, as well as acknowledgement of Section 4(f) criteria. The City of Glenwood Springs and Colorado Department of Transportation (CDOT) propose to replace the State Highway 82/Grand Avenue bridge spanning the Colorado River. The project is being documented as an Environmental Assessment (EA) by the Federal Highway Administration (FHWA) and CDOT.

### **PROJECT DESCRIPTION**

The Build Alternative would replace the SH 82/Grand Avenue bridge over the Colorado River with a five-span bridge with haunched or constant depth girders. The south end of the new bridge would start just north of the 8<sup>th</sup> Street/Grand Avenue intersection, continue on the existing alignment as it crosses 7<sup>th</sup> Street and the Union Pacific Railroad, curve west just past the railroad as it crosses the Colorado River, then touch down on the north side of the river near the 6<sup>th</sup> and Laurel Streets intersection, connecting to I-70 Exit 116 (Figure 1).

The existing pedestrian bridge located immediately east of the highway bridge would also be replaced. Three pedestrian bridge types are currently being considered: arch bridge, symmetric cable supported bridge, and asymmetric cable supported bridge. A staircase would lead pedestrians from the south side of the pedestrian bridge to downtown Glenwood Springs. To meet requirements of the Americans with Disability Act, the bicyclist/pedestrian connection on the south side would include either an elevator system and/or a scissor ramp connecting to 7<sup>th</sup> Street on the east side of the bridge (see Figure 5).

The Build Alternative would reconfigure the signalized intersection where the new highway bridge would touch down north of the river at 6<sup>th</sup> and Laurel Streets. A grade-separated pedestrian connection is proposed in a box culvert under the relocated intersection, connecting the sidewalks on the northeast and southwest sides. The sidewalk connection on the north side of the culvert comes out at grade and rises up to 6<sup>th</sup> Street. The Build Alternative also includes several improvements to existing facilities adjacent to the alignment and bridges as part of the construction detour route that would be used during the approximately 30- to 60-day bridge closure. Although constructed for the detour, several of these improvements would be permanent because they would also provide long-term transportation benefits, while several changes would only be in place during the detour period. The permanent improvements would be minor surface improvements and would occur within the existing road right-of-way.

Permanent improvements include the following: extending the I-70 westbound acceleration and deceleration lanes at Exit 116; extending the eastbound off-ramp deceleration lane at Exit 114; modifying the Exit 114 roundabouts to better accommodate large trucks; and restriping at several intersections along Midland Avenue and 27<sup>th</sup> St. The temporary changes include modifying the Midland/27<sup>th</sup> roundabout and removing traffic calming on Midland Avenue at 10<sup>th</sup> and 13<sup>th</sup> Streets. A temporary detour near the Yampah Vapor Caves is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 for bridge demolition/construction (see Figure 6 illustrating detour route). The detour would be built entirely within CDOT or City right-of-way. The temporary changes would be removed and the original features restored or reconstructed at the end of the project.

**AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) includes the parcels adjacent to 6<sup>th</sup> Street, Grand Avenue, and the area within the Hot Springs Historic District. The limits of the APE on each of these streets are as follows:

- **Grand Avenue** from 6<sup>th</sup> Street on the north to 9<sup>th</sup> Street on the south
- **6th St.** from North River Drive on the west to its end point at the Yampah Vapor Caves adjacent to I-70 on the east

Direct impacts from the project will affect some parcels on these streets. Indirect effects will include temporary construction impacts that are unlikely to extend past those properties that front along these streets. The APE includes the parcels that have structures built at least 45 years ago that are adjacent to these streets. For more information about the APE please refer to page 4 of the enclosed report.

**ELIGIBILITY DETERMINATIONS**

As summarized in the following table, a total of 37 properties were newly surveyed or documented on re-visitation forms. Survey was conducted between December 2011 and August 2012 by Gail Keeley of Hermsen Consultants. Site forms are included herewith along with the survey report. For more detailed information regarding eligibility determinations, please review pp. 21-34 of the report.

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
<b>6<sup>th</sup> St. (listed west to east)</b>			
5GF.4726	101 6 <sup>th</sup> St.	Subway	NE
5GF.4718	105 6 <sup>th</sup> St.	Kum & Go	NE
5GF.4721	106 6 <sup>th</sup> St.	Shell	NE
5GF.4722	112 6 <sup>th</sup> St.	Dairy Kreme / Glenwood Activities	NE
5GF.4723	114 6 <sup>th</sup> St.	Sioux Villa Curio	NE
5GF.4719	115 6 <sup>th</sup> St.	Eastwood Motor Company	NE
5GF.3298	205 6 <sup>th</sup> St.	Lincicome Auto / Mountain Sports Outlet	NE
5GF.4724	208-214 6 <sup>th</sup> St.; 631 Grand Ave.	Flower Mart / Gear Exchange / Liquors/ One Love / Bridge Bldg.	NE
5GF.4725	216 6 <sup>th</sup> St.	Polanka Polish Rest. / Natural Medicals	NE
5GF.767	526 Pine St.	Hotel Colorado	E; NRHP; CHD
5GF.1050	401 N. River St.	Glenwood Hot Springs Historic District	E
5GF.1050.2	401 N River St.	Hot Springs Bath House / Natatorium	OE; CHD
5GF.2441	601 6 <sup>th</sup> St.	Glenwood Springs Hydroelectric Plant	E; SRHP; NRHP; CHD
5GF.1258	709 6 <sup>th</sup> St.	Yampah Hot Springs Vapor Caves	NE; CHD

**Surveyed Properties in the APE**

ID Number	Address	Name / Description	Assessment
<b>Bridge and Railroad</b>			
5GF.2717	SH 82 Bridge over Colorado River	Glenwood Springs Viaduct / SH 82 / Grand Ave.	OE
5GF.1000.7	South edge of Colorado River	Denver & Rio Grande Railroad Tracks	E; CHD
5GF.1050.3	413 7 <sup>th</sup> St.	Denver & Rio Grande Railroad Station	E; CHD
<b>Grand Ave. (listed north to south)</b>			
5GF.4727	701 Grand Ave.	Rib City	NE
5GF.4731	702 Grand Ave.	Riviera Lounge / Peppo Nino Restaurant	NE
5GF.4732	710 Grand Ave.	Fins Grille and Raw Bar	NE
5GF.4728	711 Grand Ave.	Immortal Images Tattoo	NE
5GF.1035	714 Grand Ave.	Mirror Bar / Glenwood Shoe Service / King Barber Shop	NE
5GF.1015	715 Grand Ave.	Silver Club Bldg.	E
5GF.1034	716 Grand Ave.	Green Natural Solutions	NE
5GF.1016	717 Grand Ave.	Palace Hotel / Elizabeth Dean Boutique	E
5GF.1017	719 Grand Ave.	Parkison Bldg / The Candlestick Maker	E
5GF.4733	720 Grand Ave.	King Mall	NE
5GF.1018	721 Grand Ave.	Florindo's Italian Cuisine	NE
5GF.1033	722-724 Grand Ave.	Springs Restaurant / Doc Holliday Saloon	E
5GF.4729	723 Grand Ave.	The Book Train	NE
5GF.1019	725-727 Grand Ave.	Dougan Block / Sacred Grounds	E
5GF.1032	726 Grand Ave.	Ore Sample Room/ Narcissus Hair Studio	E
5GF.1020	729 Grand Ave.	Daily Bread Café and Bakery	NE
5GF.1021	731 Grand Ave.	Kamm-Dever Bldg.	ONE
5GF.4734	732 Grand Ave.	Bullock's Department Store / Summit Canyon Mountaineering	NE
5GF.1022	801 Grand Ave.	Citizens National Bank	E; NRHP
5GF.1023	805 Grand Ave.	Thorson Building	NE

E; NRHP = Eligible; listed on the NRHP

E; SRHP = Eligible; listed on the State Register of Historic Places

NE = Not Eligible for the NRHP

OE = Officially Eligible for the NRHP

ONE = Officially Not Eligible for the NRHP

CHD = Contributing Historic District

**EFFECTS DETERMINATIONS**

Fourteen eligible or listed properties and twenty-three non-eligible properties were identified. Detailed effects determinations are included for the fourteen eligible or listed properties; no effects determinations are presented for the twenty-three properties determined not eligible. Because those properties were determined not eligible, the project results in a finding of *no historic properties affected*.

**Hotel Colorado (5GF767, 526 Pine Street):** There will be no direct impact to this property as a result of the Build Alternative. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a

wider bridge that curves to the west. A temporary detour near the Yampah Vapor Caves is planned to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 ( Figure 6). The detour, to be built entirely within CDOT or City right-of-way, would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near this property. The temporary and indirect impacts will not impair the ability of the property to convey the significance that made it eligible for inclusion on the NRHP. Therefore, CDOT has determined that the project results in *no adverse effect* to 5GF767.

**Glenwood Hot Springs Historic District (5GF1050):** There will be no direct or permanent impacts to any of the NRHP-eligible properties within the historic district. Temporary effects from construction activities anticipated include construction equipment operation, demolition of the existing road and pedestrian bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. A temporary detour near the Yampah Vapor Caves (5GF1258) (a contributing element to the district) is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70. The detour would be built entirely within CDOT or City right-of-way. The detour would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street within the Glenwood Hot Springs Historic District.

Views from the Hot Springs Pool toward the bridges would change with construction of the new highway and pedestrian bridges. The new pedestrian bridge would be similar in height to the existing bridge, but the bridge deck would be 16 feet wide (versus the existing width of between 10 and 14 feet), and its touchdown location on the north side of the river would be similar to that of the existing pedestrian bridge. Views of the highway bridge would be reduced because the new bridge would curve away to the west as it crosses the river. The new pedestrian bridge would continue to partially obscure views of the new highway bridge. As noted above, three bridge types are being considered for the pedestrian bridge. The super structures associated with these bridge types would be taller than the existing structure, making the presence of the pedestrian bridge more noticeable and visually prominent than the existing pedestrian bridge. The noise analysis conducted for this project indicates that the Build Alternative would result in lower noise levels at the Hot Springs than the No Action Alternative, as discussed below under the Glenwood Hot Springs Bathhouse/Natatorium.

There will be temporary effects of increased dust, noise, and vibration during construction, nighttime traffic during periodic use of the 6<sup>th</sup> Street detour, and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics that qualify the district for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that the proposed plan will result in *no adverse effect*.

**Glenwood Hot Springs Bathhouse/Natatorium (5GF1050.2, 401 North River Street):** There will be no direct impact to this property. According to the noise analysis, the existing noise level at this property is 67.4 dBA, which is 1.4 dBA above FHWA's Noise Abatement Criteria of 66.0 dBA for this land use type. The 2035 noise level is projected to be 69.3 dBA under the No Action Alternative and 66.2 dBA under the Build Alternative. Therefore, the Build Alternative would result in lower noise levels (by 3.1 dBA) at this property. Temporary effects from construction activities would include construction equipment operation, demolition of the pedestrian bridge and bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. There also will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west and a new pedestrian bridge. The temporary detour near the Yampah Vapor Caves described above will also pass near this property, and would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street. The temporary and

indirect impacts will not alter the characteristics that qualify 5GF1050.2 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Hydroelectric Plant (5GF2441, 601 6th St.):** There will be no direct impact to this property. Temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west are anticipated. The temporary detour near the Yampah Vapor Caves would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near the plant. The temporary and indirect impacts will not alter the characteristics that qualify 5GF2441 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Viaduct/SH 82/Grand Avenue Bridge (5GF2717):** This eligible property will be removed and replaced with a new structure. CDOT has determined that this change will result in an *adverse effect*. A discussion regarding why the bridge is being replaced as well as a review of alternatives that avoid the bridge is included as Attachment 1. The information included in the attachment will be used in the Section 4(f) Evaluation for this resource.

**Denver & Rio Grande Railroad Tracks (5GF1000.7):** The new Grand Avenue bridge will be a maximum of approximately 62 feet wide, which is 22 feet wider than the existing structure. The railroad presently crosses under the bridge, which will not change other than the new bridge will be 1.0 to 1.5 ft. higher above the tracks and 22 feet wider than the existing bridge at the railroad crossing. There would be temporary effects within the railroad right-of-way (the historic boundary) during construction of the highway and pedestrian bridges, including construction equipment operation, demolition of the existing highway and pedestrian bridge and bridge piers, girder erection over the railroad, temporary falsework construction, regrading and temporary shoring construction, construction of adjacent bridge piers and retaining walls, and bridge deck construction over the railroad.

Additionally, a temporary grade crossing would be constructed to provide construction access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. The crossing would be built of railroad approved materials consisting of aggregate, wood, and pavement. The crossing would be located adjacent to the temporary construction impact area at a specific location to be determined in consultation with the railroad. It is anticipated that the crossing would be used at pre-scheduled times during a majority of the construction period, specifically to construct and remove cofferdams, remove existing bridge materials and construct the new bridge. Upon construction completion, the temporary rail grade crossing would be removed and the railroad right-of-way returned to its current condition and appearance. The proposed change to the width of the overhead bridge only affects a small portion of the overall resource and will not change the characteristics that make the D&RG eligible to the NRHP. The railroad will still be able to convey its significance and continue operations with the slightly wider bridge crossing overhead. Therefore, CDOT has determined that this wider crossing will result in *no adverse effect*.

**Denver & Rio Grande Railroad Station (5GF1050.3, 413 7th St.):** There will be no direct impact to this property. A scissor ramp would be built on the east side of the new pedestrian bridge to provide a connection from the pedestrian bridge to 7<sup>th</sup> Street (Figure 5), replacing the existing stair access and kiosk and resulting in minor visual changes from the station. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Silver Club Building (5GF1015, 715 Grand Ave.):** The proposed bridge will be between 8.5 and 12.5 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 21 ft. from the west edge of the bridge. The profile of the new structure will be approximately 3-4 ft. higher in front of the building than the existing configuration. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Palace Hotel (5GF1016, 717 Grand Ave.):** The proposed bridge will be an average of 11 ft. closer to this building. Currently it is about 30 ft. from the edge of the bridge, whereas it will be an average of 19 ft. from the west edge of the new bridge. The profile of the new bridge will be approximately 3-4 ft. higher in front of this building than the existing bridge. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Parkison Building (5GF1017, 719 Grand Ave.):** The proposed bridge will be 12 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 18 ft. from the west edge of the bridge. The profile of the new bridge will be approximately 3-4 feet higher in front of the building than the existing bridge. Figures 2-4 show an illustration of existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Springs Restaurant/Doc Holliday Tavern (5GF1033, 722-724 Grand Ave.):** With construction of the proposed bridge, Grand Ave. will be 7 ft. closer to this building, which is presently about 25.5 feet from the edge of Grand Ave.; after the bridge is built, that distance will be 18 ft. The profile of the new bridge will be approximately two feet higher in front of this building than the existing bridge (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Dougan Block / Sacred Grounds Coffeehouse/Dancing Bear Trading Post (5GF1019, 725-727 Grand Ave.):** With construction of the proposed bridge, Grand Avenue will be about 2 ft. closer to this building; the existing bridge lands just to the north. Grand Avenue is at-grade with the curb line about 22 ft. from the middle of the building. The new bridge approach will be on retaining walls in front of the building, with the west curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 feet higher in front of this building than the existing roadway, and a 1.5-2-foot bridge rail will be added due to the elevation change; therefore the building front will be approximately 18 feet from the back of the bridge rail (Figures 2-4). According to the noise analysis conducted for the project, the existing noise level at this property is 70.2 dBA, which is 0.8 dBA below FHWA's Noise Abatement Criteria of 71.0 dBA for this land use type. The 2035 noise levels at the property would be 72.3 dBA under the No Action Alternative and 72.8 dBA under the Build Alternative. Both alternatives would exceed the NAC; however, this slight increase would be imperceptible to the human ear. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Ore Sampling Room/Narcissus Hair Studio (5GF1032, 726 Grand Ave.):** With construction of the proposed bridge, Grand Avenue traffic will be about 2 ft. closer to this building. The existing bridge lands north of the building and Grand Avenue is at-grade, with the lanes about 22 feet away. The existing curb is about 8 feet. away, and there is on-street parking and an access lane for Wing Street in front of the building. After the bridge is constructed, the Wing Street access and parking will be removed and the bridge approach will be on retaining walls in front of the building, with the east curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 ft. higher in front of the building than the existing roadway, and a 1.5-2 ft. bridge rail will be added due to the elevation change so the building front

will be approximately 18 ft. from the back of the bridge rail (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* to 5GF1032.

**Citizens National Bank Building (5GF1022, 801 Grand Ave.):** There will be no direct impact to this property. The new bridge will touch down approximately one-half block to the north of the building. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. However, the temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no historic properties affected*.

#### **SECTION 4(F) APPLICATION**

CDOT may use one or both of the following evaluation processes to address Section 4(f) for this project. These include the *deminimis* finding and the Temporary Occupancy Exception (23 CFR 774.13(d)). Each of these options requires a different level of review.

#### **Notification of Section 4(f) DeMinimis Determination**

The project has been determined to have *no adverse effect* to 5GF767, 5GF1050, 5GF1050.2, 5GF1050.3, 5GF2441, and 5GF1000.7. Based on the findings outlined above, FHWA may make a *deminimis* finding for the Section 4(f) requirements for these properties.

#### **Section 4(f) Temporary Occupancy Exception**

CDOT has determined that this project may meet the five criteria for the temporary occupancy exception as outlined in 23 CFR 774.13(d) with regard to 5GF767, 5GF1050, 5GF1050.2, 5GF2441, 5GF1000.7, and 5GF1050.3. Criterion 5 of this exception requires documented agreement of the official with jurisdiction (SHPO) regarding the application of this exception. The five criteria for temporary occupancy are listed below:

#### **Criteria for Temporary Occupancy:**

- 1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.**

The Glenwood Hot Springs Historic District (5GF1050) includes contributing features that are also individually eligible (5GF767, 5GF1000.7, 5GF1050.2, 5GF1050.3, 5GF2441, 5GF1258). Effects related to the district and its associated properties include temporary construction activities, a traffic detour along an existing roadway, visual changes related to the new highway and pedestrian bridges, and changes to the noise setting as detailed in the Section 106 findings for these properties.

The project requires temporary effects within the historic boundary of the Denver & Rio Grande railroad (5GF1000.7), including construction equipment operation, demolition of the existing highway and pedestrian bridges, girder erection over the railroad, temporary falsework construction, retaining walls, and bridge deck construction over the railroad as noted in the Section 106 findings above. The project also requires a temporary railroad crossing.

There will be no change in ownership to any of the district and railroad properties, and for the most part the temporary effects will only last for the duration of the project.

- 2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal.**

For the Glenwood Hot Springs Historic District (5GF1050) and associated properties (5GF767, 5GF1000.7, 5GF1050.2, 5GF1050.3, 5GF2441, 5GF1258), work includes temporary construction activity, a temporary detour on an existing roadway, and some visual and noise effects.

For the Denver & Rio Grande railroad (5GF1000.7), the work involves temporary construction activities within the railroad's historic boundary as noted above, and a temporary railroad crossing that will protect the grade during construction.

The nature of any changes to the district and railroad properties will be minimal and temporary.

- 3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis.**

There will be no permanent adverse impacts to the Glenwood Hot Springs Historic District (5GF1050) and its associated properties. The effects to the district will be temporary and not interfere with its features and attributes. The district is significant under NRHP Criteria A and C as a resort associated with the growth of Glenwood Springs and for its examples of Romanesque architecture. The contributing features have individual significance as well.

There will be no permanent adverse physical impacts to 5GF1000.7. Work will occur within the railroad right-of-way (the historic boundary) and a temporary railroad crossing will be constructed, but these elements will not interfere with the features and attributes which are part of the district and are individually significant under Criterion A for its role in settlement and commercial growth in Glenwood Springs.

Findings of *no adverse effect* under Section 106 have been recommended for all of these properties.

- 4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project.**

The effects to the Glenwood Springs Historic District (5GF1050) and its contributing/individual eligible features include temporary construction activities and a temporary detour on an existing road. As noted above, there will be some changes to the views from the Historic District and associated Hot Springs pool based on the new pedestrian and highway bridges bridge but those structures already exist in the viewshed. For the overall district and its contributing/individually eligible features, the properties will be returned to a condition as good as that which existed prior to construction.

There will be work within the 5GF1000.7 right-of-way and a temporary railroad crossing will be constructed. The land within the railroad's historic boundary will be returned to a condition as good as that which existed prior to construction, as noted in the Section 106 findings above.

- 5. There must be documented agreement of the official with jurisdiction over the Section 4(f) resource regarding the above conditions.**

Based on the information provided herein, we request your agreement that this project meets the conditions of temporary occupancy under 23 CFR 774.13(d) and that Section 4(f) does not apply.

These materials have been sent to the following consulting parties for review: the Glenwood Springs Historic Preservation Commission, Frontier Historical Society, Glenwood Springs Downtown

Mr. Nichols  
August 2, 2013  
Page 9

Development Authority, owners of the Hot Springs, Garfield County, and Colorado Preservation, Inc.  
We will notify you of any responses received from these groups.

We request your concurrence with the Determinations of Eligibility and Effects, acknowledgement of the application of Finding of *DeMinimis*, and written agreement with the temporary occupancy exception for Section 4(f) . Your response is necessary for the Federal Highway Administration's compliance with Section 106 of the National Historic Preservation Act (as amended) and with the Advisory Council on Historic Preservation's regulations.

If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,



Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Survey Report  
Attachment 1

Figure 1: Build Alternative Impacts

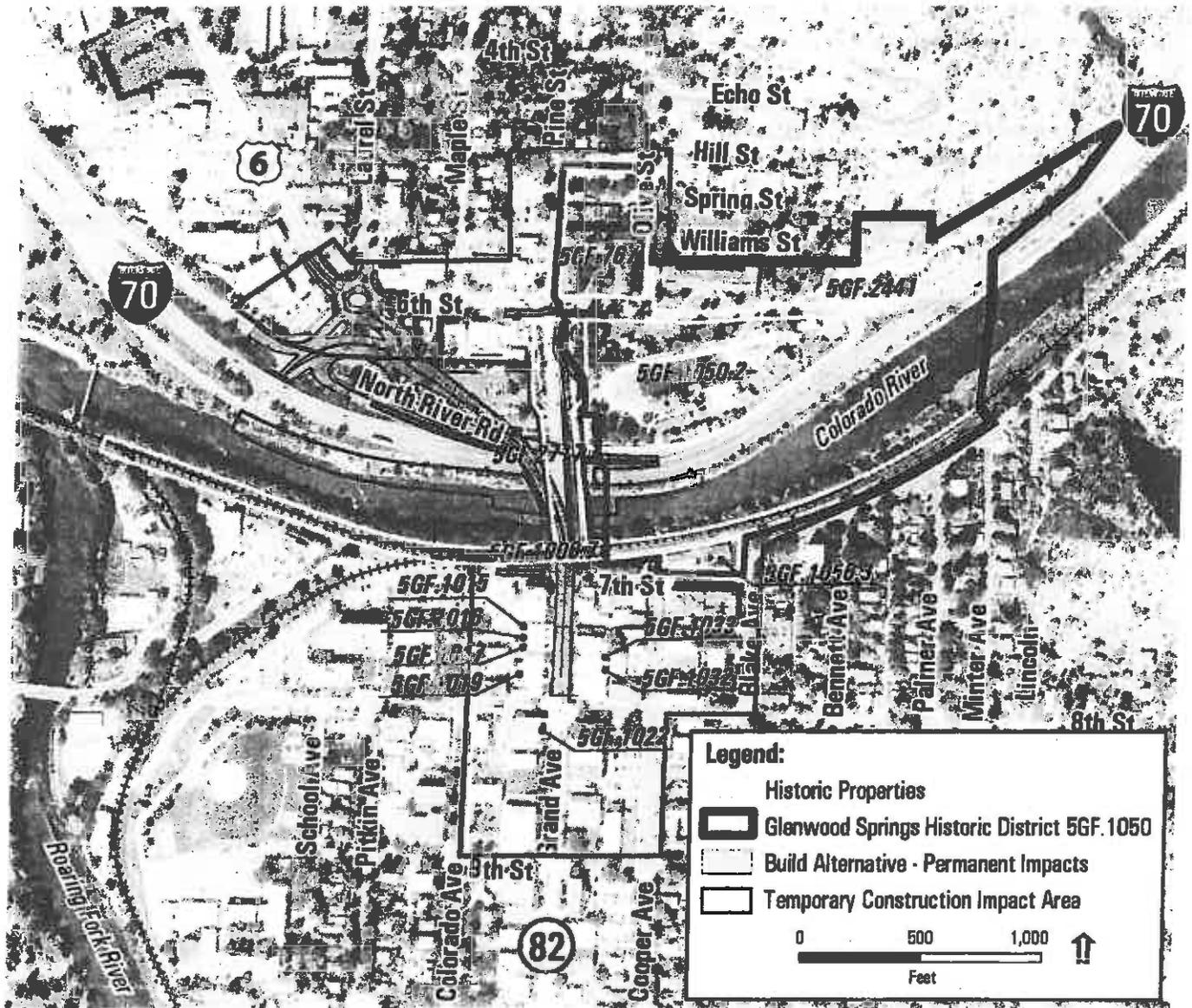
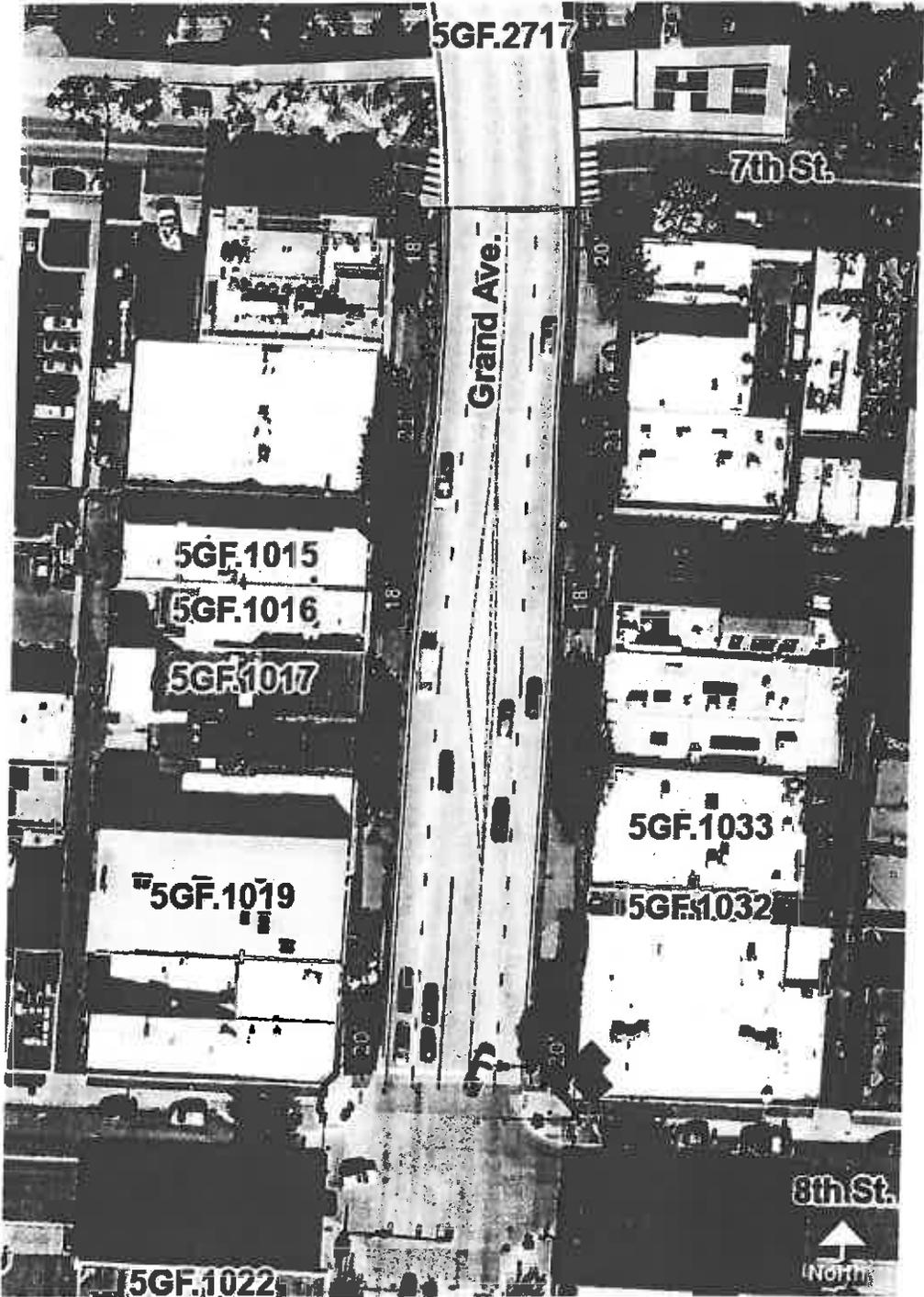


Figure 2: Existing Grand Avenue between 7<sup>th</sup> Street and 8<sup>th</sup> Street, Illustrates existing bridge/roadway width and distance from adjacent buildings



**Figure 3: Build Alternative along Grand Avenue between 7<sup>th</sup> and 8<sup>th</sup> Streets, Illustrates Build Alternative and distance from adjacent buildings**

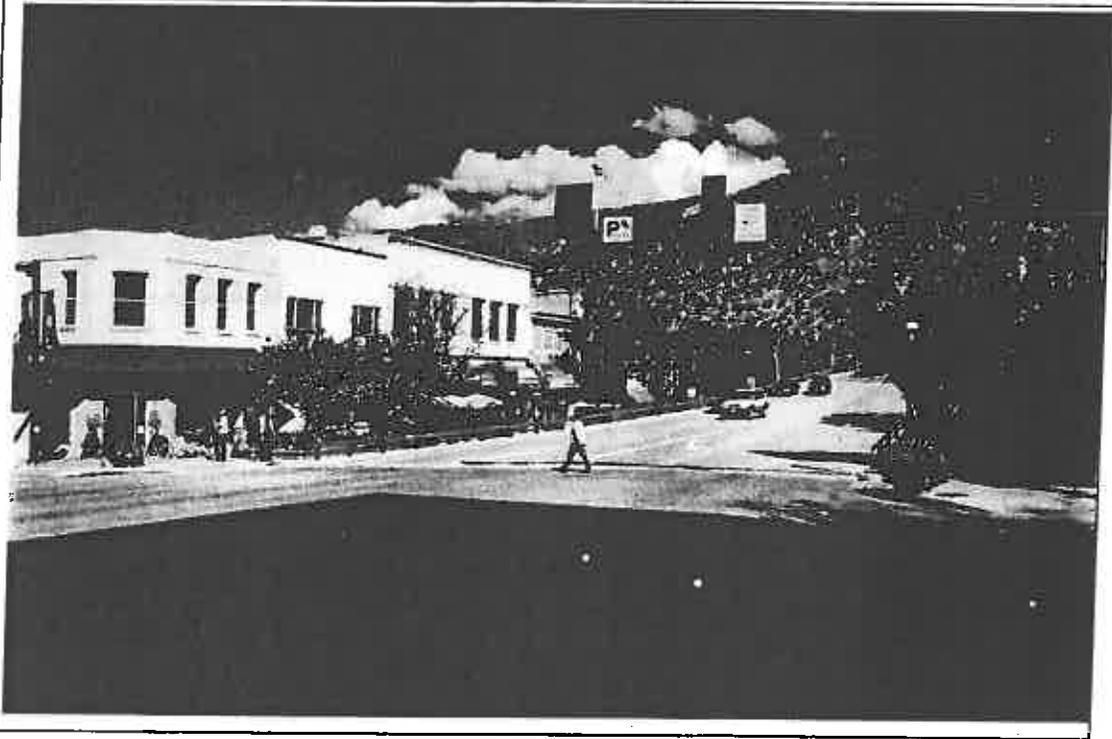


**Figure 4: Existing and Proposed Bridge Heights, Illustrates existing and proposed bridge height relative to buildings adjacent to Grand Avenue**

**Existing  
Grand  
Avenue at  
8<sup>th</sup> Street  
Looking  
North  
Toward  
River /  
Bridge**



**Build  
Alterna-  
tive from  
same view-  
point as  
above**



**Figure 5: Proposed Scissor Ramp on East Side of Bridge**

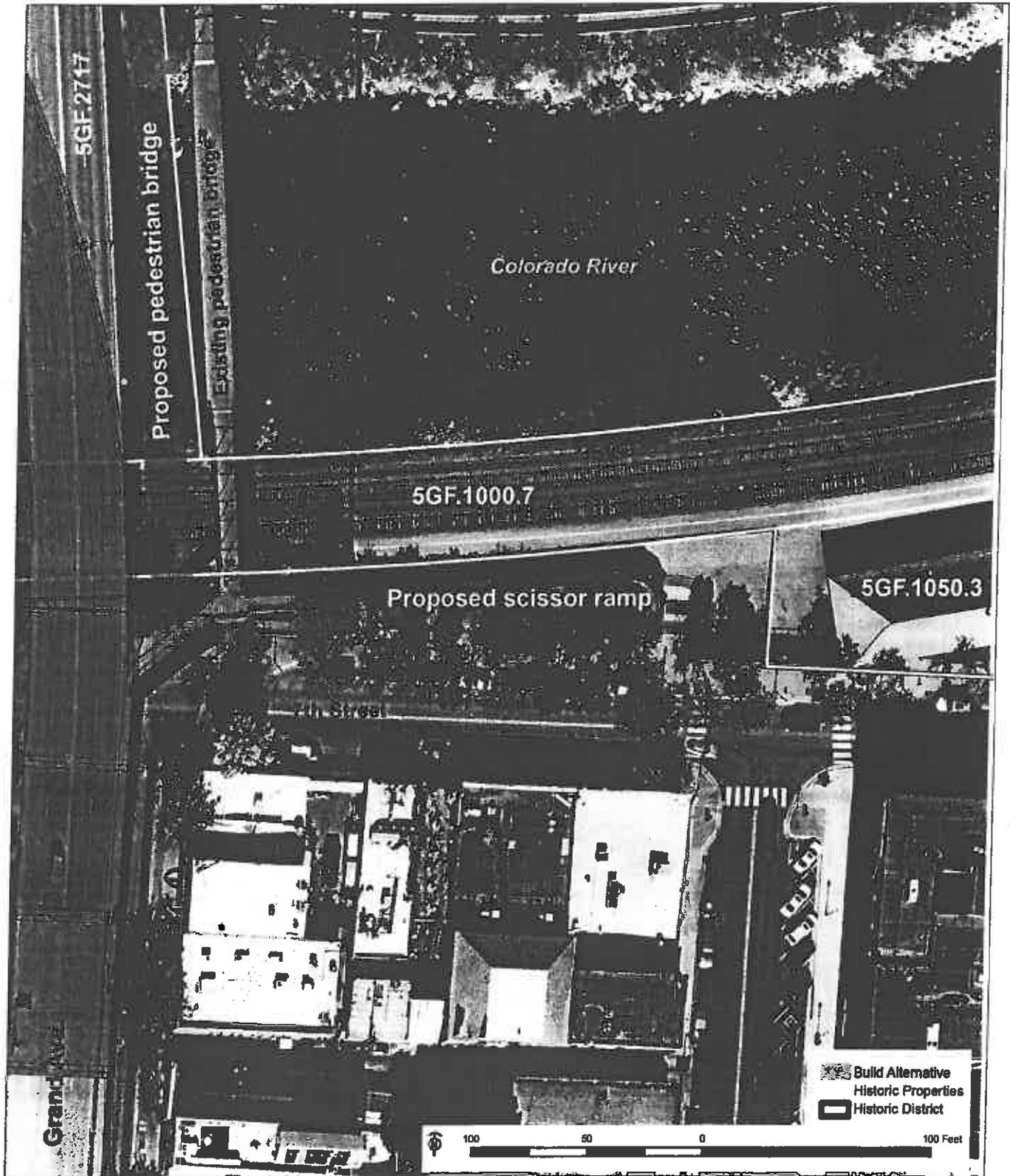
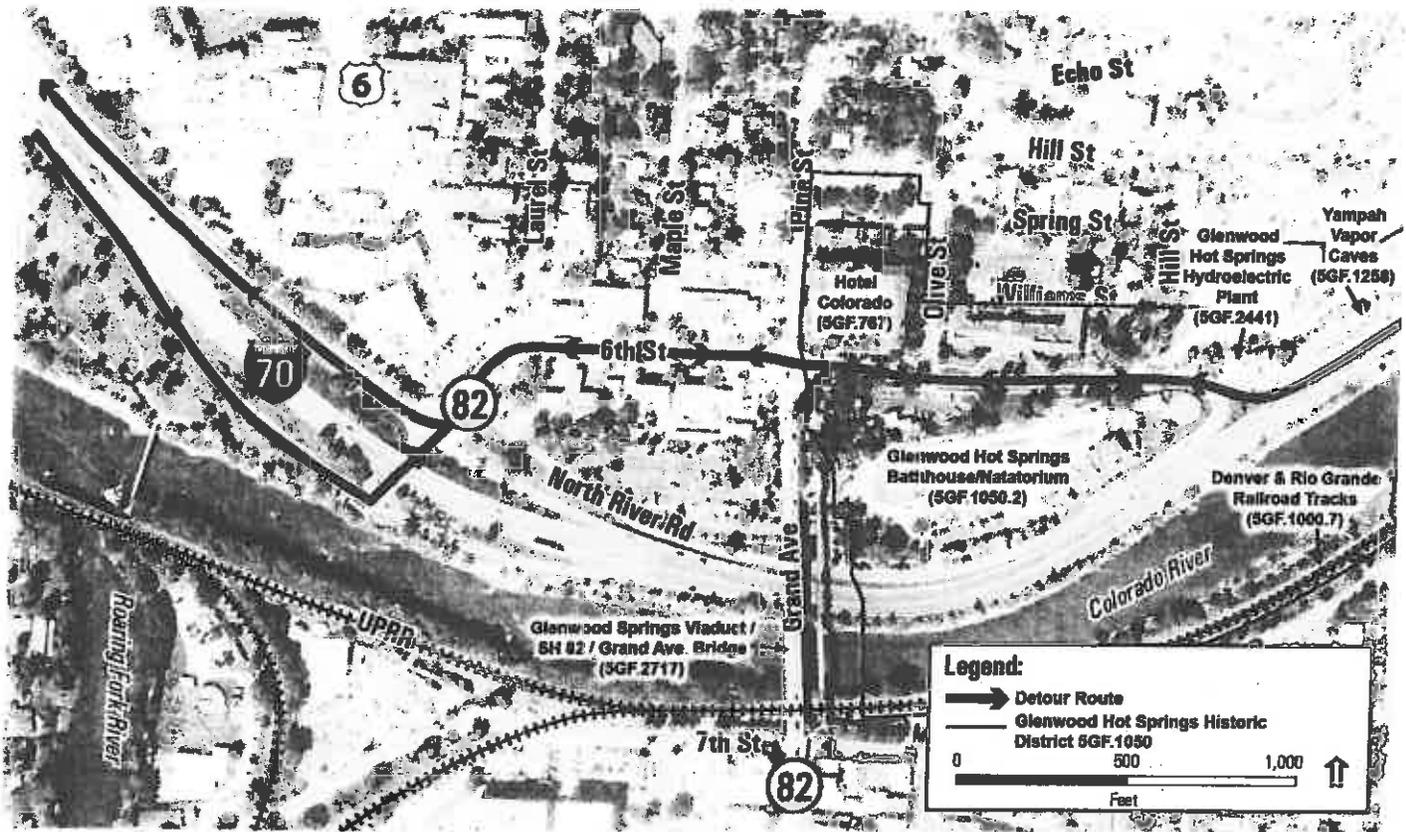


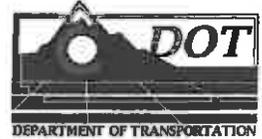
Figure 6: Temporary Detour near the Yampah Vapor Caves



# STATE OF COLORADO

## DEPARTMENT OF TRANSPORTATION

Environmental Programs Branch  
4201 East Arkansas Avenue  
Shumate Building  
Denver, Colorado 80222  
(303) 757-9281



August 5, 2013

Ms. Jane Daniels  
Colorado Preservation Incorporated  
1420 Ogden Street, Suite 103  
Denver, CO 80218

**SUBJECT:** Determinations of Eligibility and Effects, Notification of Section 4(f) *DeMinimis* and Section 4(f) Exception for Temporary Occupancy Criteria, Project FBR 0821-094, Grand Avenue Bridge Replacement, Glenwood Springs

Dear Ms. Daniels:

This letter and associated materials constitute a request for comments on Determinations of Eligibility and Effects for the project referenced above, as well as acknowledgement of Section 4(f) criteria. The City of Glenwood Springs and Colorado Department of Transportation (CDOT) propose to replace the State Highway 82/Grand Avenue bridge spanning the Colorado River. The project is being documented as an Environmental Assessment (EA) by the Federal Highway Administration (FHWA) and CDOT.

CDOT is submitting this to you in compliance with Section 106 of the National Historic Preservation Act, which requires federal agencies to evaluate the effects of their undertakings on historic properties. Colorado Preservation Incorporated has been identified as a potential consulting party for this project. For more information about Section 106 and how you can participate as a consulting party, please visit the Advisory Council on Historic Preservation's web site, which contains the Citizen's Guide to Section 106 Review at <http://www.achp.gov/docs/CitizenGuide.pdf>.

### **PROJECT DESCRIPTION**

The Build Alternative would replace the SH 82/Grand Avenue bridge over the Colorado River with a five-span bridge with haunched or constant depth girders. The south end of the new bridge would start just north of the 8<sup>th</sup> Street/Grand Avenue intersection, continue on the existing alignment as it crosses 7<sup>th</sup> Street and the Union Pacific Railroad, curve west just past the railroad as it crosses the Colorado River, then touch down on the north side of the river near the 6<sup>th</sup> and Laurel Streets intersection, connecting to I-70 Exit 116 (Figure 1).

The existing pedestrian bridge located immediately east of the highway bridge would also be replaced. Three pedestrian bridge types are currently being considered: arch bridge, symmetric cable supported bridge, and asymmetric cable supported bridge. A staircase would lead pedestrians from the south side of the pedestrian bridge to downtown Glenwood Springs. To meet requirements of the Americans with Disability Act, the bicyclist/pedestrian connection on the south side would include either an elevator system and/or a scissor ramp connecting to 7<sup>th</sup> Street on the east side of the bridge (see Figure 5).

The Build Alternative would reconfigure the signalized intersection where the new highway bridge would touch down north of the river at 6<sup>th</sup> and Laurel Streets. A grade-separated pedestrian connection is proposed in a box culvert under the relocated intersection, connecting the sidewalks on the northeast and southwest sides. The sidewalk connection on the north side of the culvert comes out at grade and rises up

to 6<sup>th</sup> Street. The Build Alternative also includes several improvements to existing facilities adjacent to the alignment and bridges as part of the construction detour route that would be used during the approximately 30- to 60-day bridge closure. Although constructed for the detour, several of these improvements would be permanent because they would also provide long-term transportation benefits, while several changes would only be in place during the detour period. The permanent improvements would be minor surface improvements and would occur within the existing road right-of-way. Permanent improvements include the following: extending the I-70 westbound acceleration and deceleration lanes at Exit 116; extending the eastbound off-ramp deceleration lane at Exit 114; modifying the Exit 114 roundabouts to better accommodate large trucks; and restriping at several intersections along Midland Avenue and 27<sup>th</sup> St. The temporary changes include modifying the Midland/27<sup>th</sup> roundabout and removing traffic calming on Midland Avenue at 10<sup>th</sup> and 13<sup>th</sup> Streets. A temporary detour near the Yampah Vapor Caves is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 for bridge demolition/construction (see Figure 6 illustrating detour route). The detour would be built entirely within CDOT or City right-of-way. The temporary changes would be removed and the original features restored or reconstructed at the end of the project.

**AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) includes the parcels adjacent to 6<sup>th</sup> Street, Grand Avenue, and the area within the Hot Springs Historic District. The limits of the APE on each of these streets are as follows:

- Grand Avenue from 6<sup>th</sup> Street on the north to 9<sup>th</sup> Street on the south
- 6<sup>th</sup> St. from North River Drive on the west to its end point at the Yampah Vapor Caves adjacent to I-70 on the east

Direct impacts from the project will affect some parcels on these streets. Indirect effects will include temporary construction impacts that are unlikely to extend past those properties that front along these streets. The APE includes the parcels that have structures built at least 45 years ago that are adjacent to these streets. For more information about the APE please refer to page 4 of the enclosed report.

**ELIGIBILITY DETERMINATIONS**

As summarized in the following table, a total of 37 properties were newly surveyed or documented on re-visitation forms. Survey was conducted between December 2011 and August 2012 by Gail Keeley of Hermesen Consultants. Site forms are included herewith along with the survey report. For more detailed information regarding eligibility determinations, please review pp. 21-34 of the report.

<b>Surveyed Properties in the APE</b>			
<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
<b>6<sup>th</sup> St. (listed west to east)</b>			
5GF.4726	101 6 <sup>th</sup> St.	Subway	NE
5GF.4718	105 6 <sup>th</sup> St.	Kum & Go	NE
5GF.4721	106 6 <sup>th</sup> St.	Shell	NE
5GF.4722	112 6 <sup>th</sup> St.	Dairy Kreme / Glenwood Activities	NE
5GF.4723	114 6 <sup>th</sup> St.	Sioux Villa Curio	NE
5GF.4719	115 6 <sup>th</sup> St.	Eastwood Motor Company	NE
5GF.3298	205 6 <sup>th</sup> St.	Lincicome Auto / Mountain Sports Outlet	NE
5GF.4724	208-214 6 <sup>th</sup> St.; 631 Grand Ave.	Flower Mart / Gear Exchange / Liquors/ One Love / Bridge Bldg.	NE
5GF.4725	216 6 <sup>th</sup> St.	Polanka Polish Rest. / Natural Medicals	NE
5GF.767	526 Pine St.	Hotel Colorado	E; NRHP; CHD

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
5GF.1050	401 N. River St.	Glenwood Hot Springs Historic District	E
5GF.1050.2	401 N River St.	Hot Springs Bath House / Natatorium	OE; CHD
5GF.2441	601 6 <sup>th</sup> St.	Glenwood Springs Hydroelectric Plant	E; SRHP; NRHP; CHD
5GF.1258	709 6 <sup>th</sup> St.	Yampah Hot Springs Vapor Caves	NE; CHD
<b>Bridge and Railroad</b>			
5GF.2717	SH 82 Bridge over Colorado River	Glenwood Springs Viaduct / SH 82 / Grand Ave.	OE
5GF.1000.7	South edge of Colorado River	Denver & Rio Grande Railroad Tracks	E; CHD
5GF.1050.3	413 7 <sup>th</sup> St.	Denver & Rio Grande Railroad Station	E; CHD
<b>Grand Ave. (listed north to south)</b>			
5GF.4727	701 Grand Ave.	Rib City	NE
5GF.4731	702 Grand Ave.	Riviera Lounge / Peppo Nino Restaurant	NE
5GF.4732	710 Grand Ave.	Fins Grille and Raw Bar	NE
5GF.4728	711 Grand Ave.	Immortal Images Tattoo	NE
5GF.1035	714 Grand Ave.	Mirror Bar / Glenwood Shoe Service / King Barber Shop	NE
5GF.1015	715 Grand Ave.	Silver Club Bldg.	E
5GF.1034	716 Grand Ave.	Green Natural Solutions	NE
5GF.1016	717 Grand Ave.	Palace Hotel / Elizabeth Dean Boutique	E
5GF.1017	719 Grand Ave.	Parkison Bldg / The Candlestick Maker	E
5GF.4733	720 Grand Ave.	King Mall	NE
5GF.1018	721 Grand Ave.	Florindo's Italian Cuisine	NE
5GF.1033	722-724 Grand Ave.	Springs Restaurant / Doc Holliday Saloon	E
5GF.4729	723 Grand Ave.	The Book Train	NE
5GF.1019	725-727 Grand Ave.	Dougan Block / Sacred Grounds	E
5GF.1032	726 Grand Ave.	Ore Sample Room/ Narcissus Hair Studio	E
5GF.1020	729 Grand Ave.	Daily Bread Café and Bakery	NE
5GF.1021	731 Grand Ave.	Kamm-Dever Bldg.	ONE
5GF.4734	732 Grand Ave.	Bullock's Department Store / Summit Canyon Mountaineering	NE
5GF.1022	801 Grand Ave.	Citizens National Bank	E; NRHP
5GF.1023	805 Grand Ave.	Thorson Building	NE

E; NRHP = Eligible; listed on the NRHP  
 E; SRHP = Eligible; listed on the State Register of Historic Places  
 NE = Not Eligible for the NRHP  
 OE = Officially Eligible for the NRHP  
 ONE = Officially Not Eligible for the NRHP  
 CHD = Contributing Historic District

**EFFECTS DETERMINATIONS**

Fourteen eligible or listed properties and twenty-three non-eligible properties were identified. Detailed effects determinations are included for the fourteen eligible or listed properties; no effects determinations

are presented for the twenty-three properties determined not eligible. Because those properties were determined not eligible, the project results in a finding of *no historic properties affected*.

**Hotel Colorado (5GF767, 526 Pine Street):** There will be no direct impact to this property as a result of the Build Alternative. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west. A temporary detour near the Yampah Vapor Caves is planned to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 (6). The detour, to be built entirely within CDOT or City right-of-way, would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near this property. The temporary and indirect impacts will not impair the ability of the property to convey the significance that made it eligible for inclusion on the NRHP. Therefore, CDOT has determined that the project results in *no adverse effect* to 5GF767.

**Glenwood Hot Springs Historic District (5GF1050):** There will be no direct or permanent impacts to any of the NRHP-eligible properties within the historic district. Temporary effects from construction activities anticipated include construction equipment operation, demolition of the existing road and pedestrian bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. A temporary detour near the Yampah Vapor Caves (5GF1258) (a contributing element to the district) is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70. The detour would be built entirely within CDOT or City right-of-way. The detour would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street within the Glenwood Hot Springs Historic District.

Views from the Hot Springs Pool toward the bridges would change with construction of the new highway and pedestrian bridges. The new pedestrian bridge would be similar in height to the existing bridge, but the bridge deck would be 16 feet wide (versus the existing width of between 10 and 14 feet), and its touchdown location on the north side of the river would be similar to that of the existing pedestrian bridge. Views of the highway bridge would be reduced because the new bridge would curve away to the west as it crosses the river. The new pedestrian bridge would continue to partially obscure views of the new highway bridge. As noted above, three bridge types are being considered for the pedestrian bridge. The super structures associated with these bridge types would be taller than the existing structure, making the presence of the pedestrian bridge more noticeable and visually prominent than the existing pedestrian bridge. The noise analysis conducted for this project indicates that the Build Alternative would result in lower noise levels at the Hot Springs than the No Action Alternative, as discussed below under the Glenwood Hot Springs Bathhouse/Natatorium.

There will be temporary effects of increased dust, noise, and vibration during construction, nighttime traffic during periodic use of the 6<sup>th</sup> Street detour, and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics that qualify the district for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that the proposed plan will result in *no adverse effect*.

**Glenwood Hot Springs Bathhouse/Natatorium (5GF1050.2, 401 North River Street):** There will be no direct impact to this property. According to the noise analysis, the existing noise level at this property is 67.4 dBA, which is 1.4 dBA above FHWA's Noise Abatement Criteria of 66.0 dBA for this land use type. The 2035 noise level is projected to be 69.3 dBA under the No Action Alternative and 66.2 dBA under the Build Alternative. Therefore, the Build Alternative would result in lower noise levels (by 3.1 dBA) at this property. Temporary effects from construction activities would include construction equipment operation, demolition of the pedestrian bridge and bridge piers; temporary falsework

construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. There also will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west and a new pedestrian bridge. The temporary detour near the Yampah Vapor Caves described above will also pass near this property, and would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street. The temporary and indirect impacts will not alter the characteristics that qualify 5GF1050.2 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Hydroelectric Plant (5GF2441, 601 6th St.):** There will be no direct impact to this property. Temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west are anticipated. The temporary detour near the Yampah Vapor Caves would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near the plant. The temporary and indirect impacts will not alter the characteristics that qualify 5GF2441 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Viaduct/SH 82/Grand Avenue Bridge (5GF2717):** This eligible property will be removed and replaced with a new structure. CDOT has determined that this change will result in an *adverse effect*. A discussion regarding why the bridge is being replaced as well as a review of alternatives that avoid the bridge is included as Attachment 1. The information included in the attachment will be used in the Section 4(f) Evaluation for this resource.

**Denver & Rio Grande Railroad Tracks (5GF1000.7):** The new Grand Avenue bridge will be a maximum of approximately 62 feet wide, which is 22 feet wider than the existing structure. The railroad presently crosses under the bridge, which will not change other than the new bridge will be 1.0 to 1.5 ft. higher above the tracks and 22 feet wider than the existing bridge at the railroad crossing. There would be temporary effects within the railroad right-of-way (the historic boundary) during construction of the highway and pedestrian bridges, including construction equipment operation, demolition of the existing highway and pedestrian bridge and bridge piers, girder erection over the railroad, temporary falsework construction, regrading and temporary shoring construction, construction of adjacent bridge piers and retaining walls, and bridge deck construction over the railroad.

Additionally, a temporary grade crossing would be constructed to provide construction access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. The crossing would be built of railroad approved materials consisting of aggregate, wood, and pavement. The crossing would be located adjacent to the temporary construction impact area at a specific location to be determined in consultation with the railroad. It is anticipated that the crossing would be used at pre-scheduled times during a majority of the construction period, specifically to construct and remove cofferdams, remove existing bridge materials and construct the new bridge. Upon construction completion, the temporary rail grade crossing would be removed and the railroad right-of-way returned to its current condition and appearance. The proposed change to the width of the overhead bridge only affects a small portion of the overall resource and will not change the characteristics that make the D&RG eligible to the NRHP. The railroad will still be able to convey its significance and continue operations with the slightly wider bridge crossing overhead. Therefore, CDOT has determined that this wider crossing will result in *no adverse effect*.

**Denver & Rio Grande Railroad Station (5GF1050.3, 413 7th St.):** There will be no direct impact to this property. A scissor ramp would be built on the east side of the new pedestrian bridge to provide a

connection from the pedestrian bridge to 7<sup>th</sup> Street (Figure 5), replacing the existing stair access and kiosk and resulting in minor visual changes from the station. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Silver Club Building (5GF1015, 715 Grand Ave.):** The proposed bridge will be between 8.5 and 12.5 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 21 ft. from the west edge of the bridge. The profile of the new structure will be approximately 3-4 ft. higher in front of the building than the existing configuration. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Palace Hotel (5GF1016, 717 Grand Ave.):** The proposed bridge will be an average of 11 ft. closer to this building. Currently it is about 30 ft. from the edge of the bridge, whereas it will be an average of 19 ft. from the west edge of the new bridge. The profile of the new bridge will be approximately 3-4 ft. higher in front of this building than the existing bridge. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Parkison Building (5GF1017, 719 Grand Ave.):** The proposed bridge will be 12 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 18 ft. from the west edge of the bridge. The profile of the new bridge will be approximately 3-4 feet higher in front of the building than the existing bridge. Figures 2-4 show an illustration of existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Springs Restaurant/Doc Holliday Tavern (5GF1033, 722-724 Grand Ave.):** With construction of the proposed bridge, Grand Ave. will be 7 ft. closer to this building, which is presently about 25.5 feet from the edge of Grand Ave.; after the bridge is built, that distance will be 18 ft. The profile of the new bridge will be approximately two feet higher in front of this building than the existing bridge (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Dougan Block / Sacred Grounds Coffeehouse/Dancing Bear Trading Post (5GF1019, 725-727 Grand Ave.):** With construction of the proposed bridge, Grand Avenue will be about 2 ft. closer to this building; the existing bridge lands just to the north. Grand Avenue is at-grade with the curb line about 22 ft. from the middle of the building. The new bridge approach will be on retaining walls in front of the building, with the west curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 feet higher in front of this building than the existing roadway, and a 1.5-2-foot bridge rail will be added due to the elevation change; therefore the building front will be approximately 18 feet from the back of the bridge rail (Figures 2-4). According to the noise analysis conducted for the project, the existing noise level at this property is 70.2 dBA, which is 0.8 dBA below FHWA's Noise Abatement Criteria of 71.0 dBA for this land use type. The 2035 noise levels at the property would be 72.3 dBA under the No Action Alternative and 72.8 dBA under the Build Alternative. Both alternatives would exceed the NAC; however, this slight increase would be imperceptible to the human ear. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Ore Sampling Room/Narcissus Hair Studio (5GF1032, 726 Grand Ave.):** With construction of the proposed bridge, Grand Avenue traffic will be about 2 ft. closer to this building. The existing bridge

lands north of the building and Grand Avenue is at-grade, with the lanes about 22 feet away. The existing curb is about 8 feet. away, and there is on-street parking and an access lane for Wing Street in front of the building. After the bridge is constructed, the Wing Street access and parking will be removed and the bridge approach will be on retaining walls in front of the building, with the east curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 ft. higher in front of the building than the existing roadway, and a 1.5-2 ft. bridge rail will be added due to the elevation change so the building front will be approximately 18 ft. from the back of the bridge rail (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* to 5GF1032.

**Citizens National Bank Building (5GF1022, 801 Grand Ave.):** There will be no direct impact to this property. The new bridge will touch down approximately one-half block to the north of the building. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. However, the temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no historic properties affected*.

## **SECTION 4(F) AND DE MINIMIS**

### **Background**

In addition to Section 106 of the NHPA, FHWA must comply with Section 4(f), which is codified at both 49 U.S.C § 303 and 23 U.S.C. § 138. Congress amended Section 4(f) when it enacted the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (Public Law 109-59, enacted August 10, 2005) (“SAFETEA-LU”). Section 6009 of SAFETEA-LU added a new subsection to Section 4(f), which authorizes FHWA to approve a project that uses Section 4(f) lands that are part of a historic property without preparation of an Avoidance Analysis, if it makes a finding that such uses would have “*de minimis*” impacts upon the Section 4(f) resource, with the concurrence of the SHPO.

On December 12, 2005, the Federal Highway Administration issued its “Guidance for Determining *De Minimis* Impacts to Section 4(f) Resources” which indicates that a finding of *de minimis* can be made when the Section 106 process results in a *no adverse effect* or *no historic properties affected* determination, when the SHPO is informed of the FHWA’s intent to make a *de minimis* impact finding based on their written concurrence in the Section 106 determination, and when FHWA has considered the views of any Section 106 consulting parties participating in the Section 106 process. This new provision of Section 4(f) and the associated guidance are in part the basis of this letter, and of FHWA’s determination and notification of *de minimis* impacts to Colorado Preservation Incorporated with respect to the proposed project. At this time we are notifying the Section 106 consulting parties per section 6009(b)(2)(C). On March 12, 2008, FHWA issued a Final Rule on Section 4(f), which clarifies and implements the procedures for determining a *de minimis* impact. In addition the Final Rule moves the Section 4(f) regulation to 23 CFR 774.

### **Notification of Section 4(f) DeMinimis Determination**

The project has been determined to have *no adverse effect* to 5GF767, 5GF1050, 5GF1050.2, 5GF1050.3, 5GF2441, and 5GF1000.7. Based on the findings outlined above, FHWA may make a *de minimis* finding for the Section 4(f) requirements for these properties.

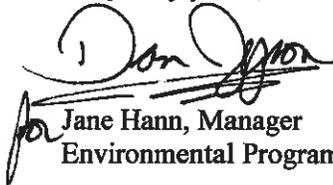
These materials have been sent to the State Historic Preservation Officer (SHPO) for review.

Ms. Daniels  
August 5, 2013  
Page 8

As a statewide historic preservation organization, we welcome your comments on this project. Should you choose to respond, we request your comments within 30 days of receipt of these materials. If we do not hear from you in this time frame, we will assume you do not plan to comment. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Historian, Lisa Schoch, at 303-512-4258.

If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

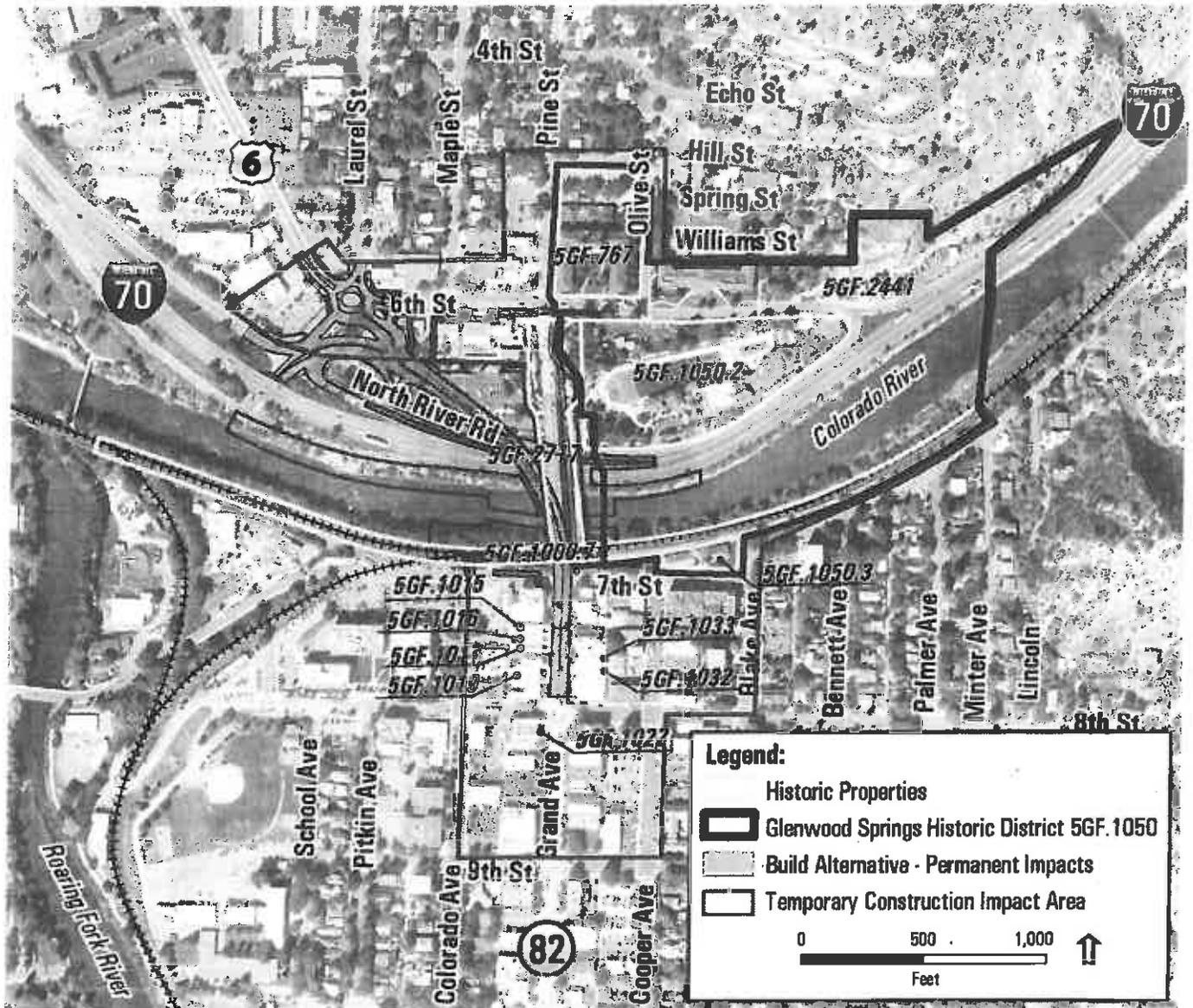
Very truly yours,

A handwritten signature in black ink, appearing to read "Jane Hann", is written over a horizontal line. The signature is stylized and cursive.

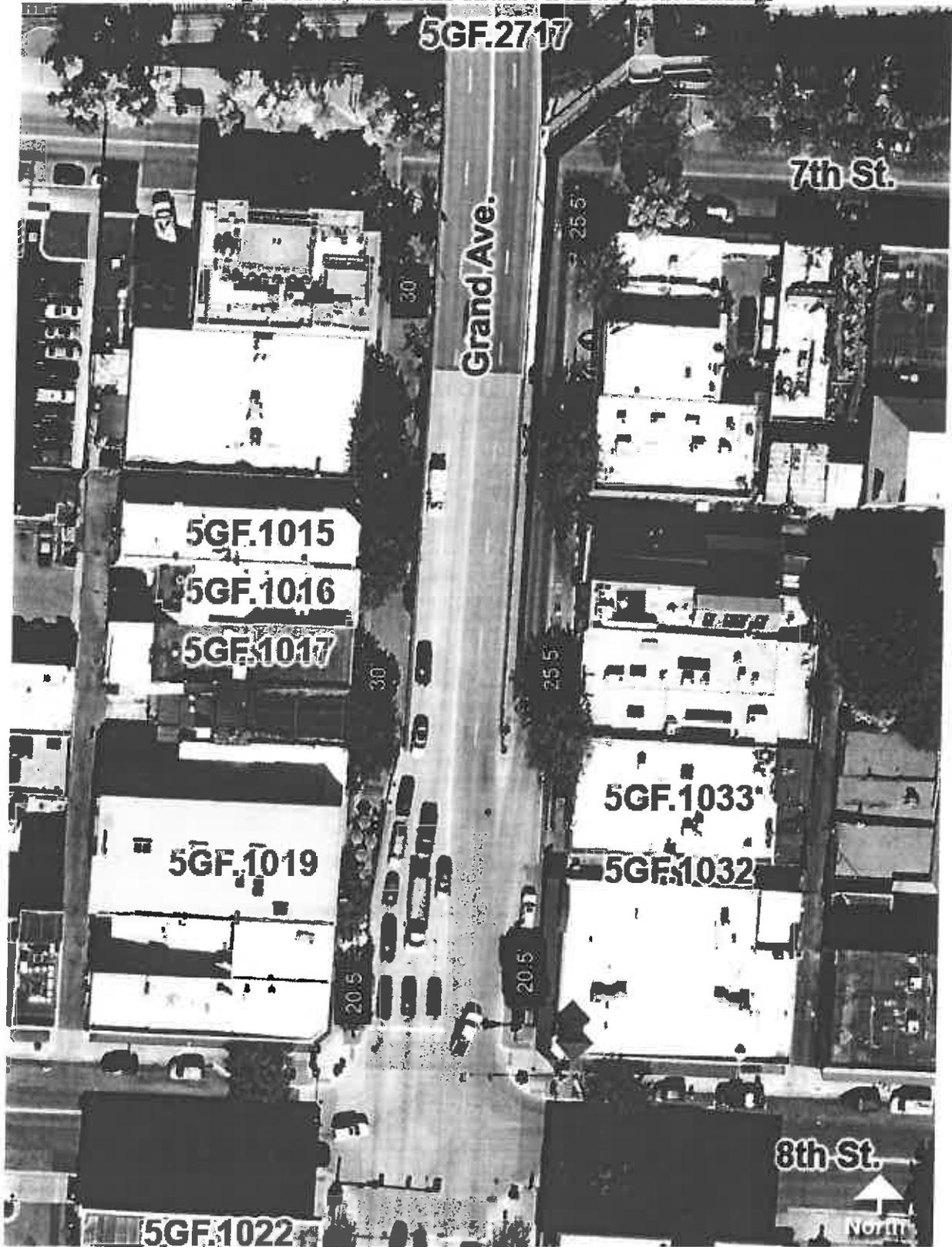
Jane Hann, Manager  
Environmental Programs Branch

Enclosures:      Survey Report  
                         Attachment I

Figure 1: Build Alternative Impacts



**Figure 2: Existing Grand Avenue between 7<sup>th</sup> Street and 8<sup>th</sup> Street, Illustrates existing bridge/roadway width and distance from adjacent buildings**



**Figure 3: Build Alternative along Grand Avenue between 7<sup>th</sup> and 8<sup>th</sup> Streets, Illustrates Build Alternative and distance from adjacent buildings**



**Figure 4: Existing and Proposed Bridge Heights, Illustrates existing and proposed bridge height relative to buildings adjacent to Grand Avenue**

**Existing  
Grand  
Avenue at  
8<sup>th</sup> Street  
Looking  
North  
Toward  
River /  
Bridge**



**Build  
Alterna-  
tive from  
same view-  
point as  
above**



**Figure 5: Proposed Scissor Ramp on East Side of Bridge**

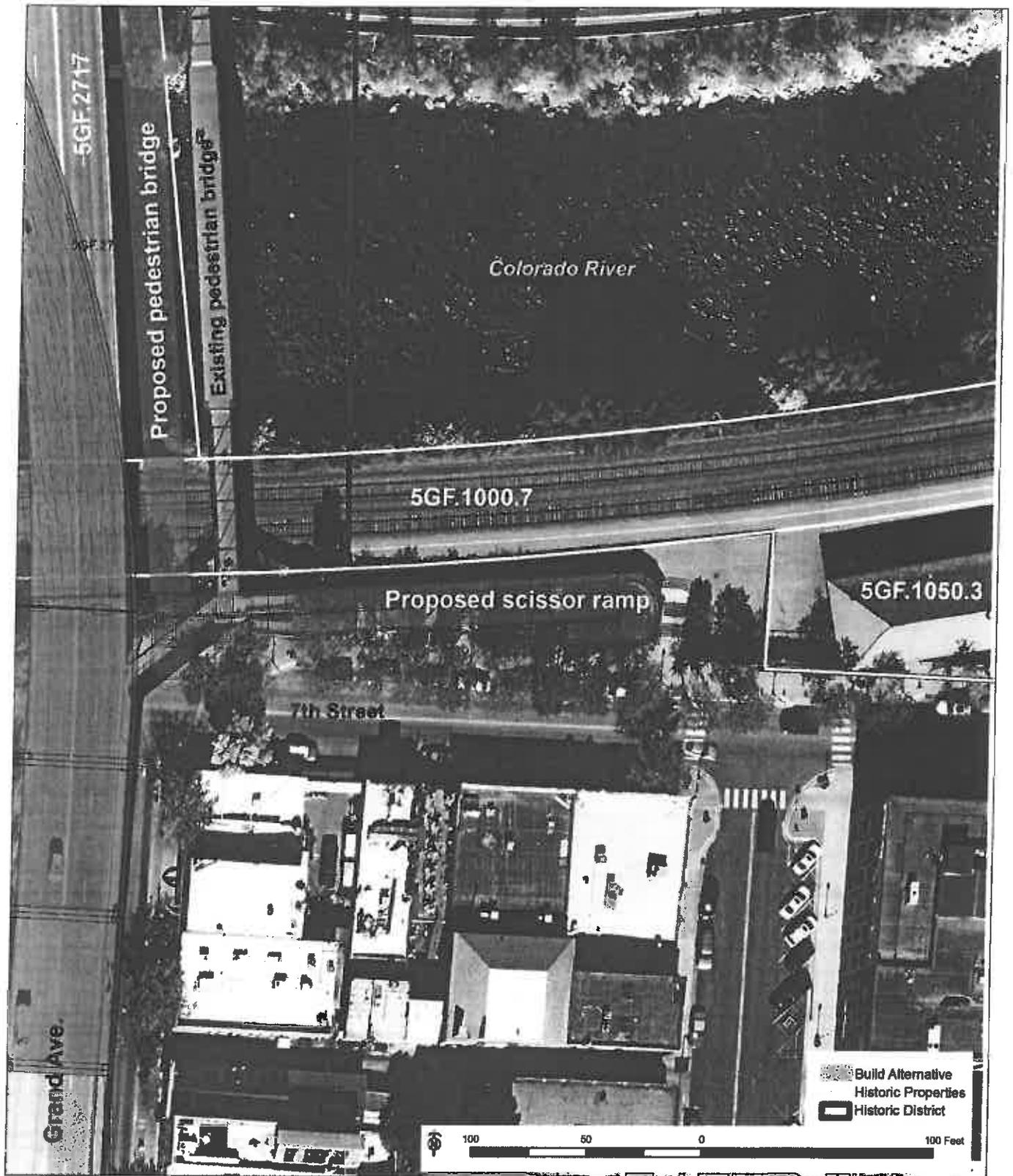
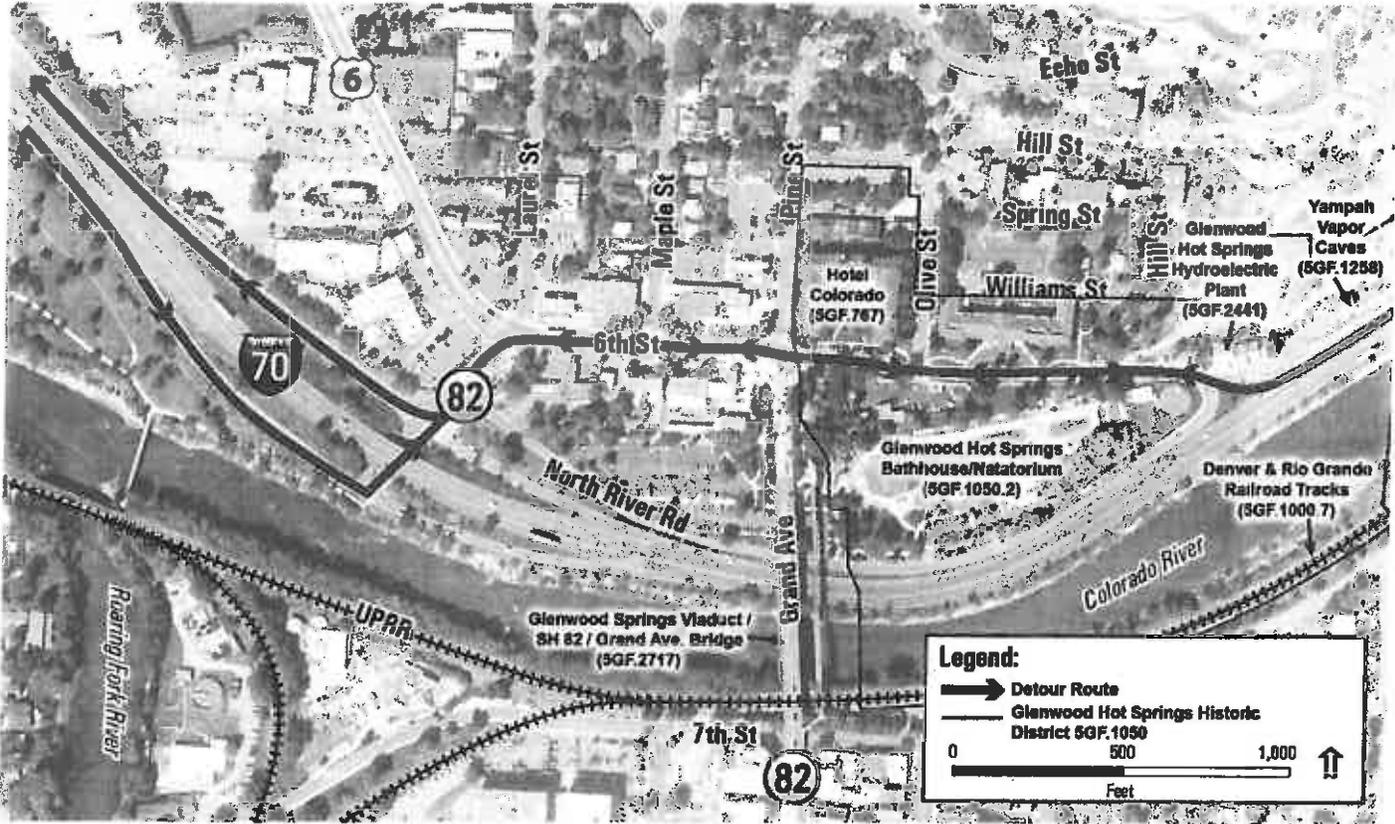
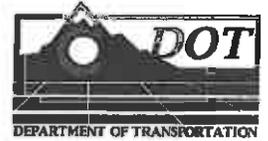


Figure 6: Temporary Detour near the Yampah Vapor Caves



# STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION  
Environmental Programs Branch  
4201 East Arkansas Avenue  
Shumate Building  
Denver, Colorado 80222  
(303) 757-9281



August 5, 2013

Ms. Cindy Hines, Director  
Frontier Historical Society  
1001 Colorado Avenue  
Glenwood Springs, CO81601

**SUBJECT:** Determinations of Eligibility and Effects, Notification of Section 4(f) *DeMinimis* and Section 4(f) Exception for Temporary Occupancy Criteria, Project FBR 0821-094, Grand Avenue Bridge Replacement, Glenwood Springs

Dear Ms. Hines:

This letter and associated materials constitute a request for comments on Determinations of Eligibility and Effects for the project referenced above, as well as acknowledgement of Section 4(f) criteria. The City of Glenwood Springs and Colorado Department of Transportation (CDOT) propose to replace the State Highway 82/Grand Avenue bridge spanning the Colorado River. The project is being documented as an Environmental Assessment (EA) by the Federal Highway Administration (FHWA) and CDOT.

CDOT is submitting this to you in compliance with Section 106 of the National Historic Preservation Act, which requires federal agencies to evaluate the effects of their undertakings on historic properties. The Frontier Historical Society has been identified as a potential consulting party for this project. For more information about Section 106 and how you can participate as a consulting party, please visit the Advisory Council on Historic Preservation's web site, which contains the Citizen's Guide to Section 106 Review at <http://www.achp.gov/docs/CitizenGuide.pdf>.

## **PROJECT DESCRIPTION**

The Build Alternative would replace the SH 82/Grand Avenue bridge over the Colorado River with a five-span bridge with haunched or constant depth girders. The south end of the new bridge would start just north of the 8<sup>th</sup> Street/Grand Avenue intersection, continue on the existing alignment as it crosses 7<sup>th</sup> Street and the Union Pacific Railroad, curve west just past the railroad as it crosses the Colorado River, then touch down on the north side of the river near the 6<sup>th</sup> and Laurel Streets intersection, connecting to I-70 Exit 116 (Figure 1).

The existing pedestrian bridge located immediately east of the highway bridge would also be replaced. Three pedestrian bridge types are currently being considered: arch bridge, symmetric cable supported bridge, and asymmetric cable supported bridge. A staircase would lead pedestrians from the south side of the pedestrian bridge to downtown Glenwood Springs. To meet requirements of the Americans with Disability Act, the bicyclist/pedestrian connection on the south side would include either an elevator system and/or a scissor ramp connecting to 7<sup>th</sup> Street on the east side of the bridge (see Figure 5).

The Build Alternative would reconfigure the signalized intersection where the new highway bridge would touch down north of the river at 6<sup>th</sup> and Laurel Streets. A grade-separated pedestrian connection is proposed in a box culvert under the relocated intersection, connecting the sidewalks on the northeast and southwest sides. The sidewalk connection on the north side of the culvert comes out at grade and rises up

to 6<sup>th</sup> Street. The Build Alternative also includes several improvements to existing facilities adjacent to the alignment and bridges as part of the construction detour route that would be used during the approximately 30- to 60-day bridge closure. Although constructed for the detour, several of these improvements would be permanent because they would also provide long-term transportation benefits, while several changes would only be in place during the detour period. The permanent improvements would be minor surface improvements and would occur within the existing road right-of-way. Permanent improvements include the following: extending the I-70 westbound acceleration and deceleration lanes at Exit 116; extending the eastbound off-ramp deceleration lane at Exit 114; modifying the Exit 114 roundabouts to better accommodate large trucks; and restriping at several intersections along Midland Avenue and 27<sup>th</sup> St. The temporary changes include modifying the Midland/27<sup>th</sup> roundabout and removing traffic calming on Midland Avenue at 10<sup>th</sup> and 13<sup>th</sup> Streets. A temporary detour near the Yampah Vapor Caves is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 for bridge demolition/construction (see Figure 6 illustrating detour route). The detour would be built entirely within CDOT or City right-of-way. The temporary changes would be removed and the original features restored or reconstructed at the end of the project.

**AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) includes the parcels adjacent to 6<sup>th</sup> Street, Grand Avenue, and the area within the Hot Springs Historic District. The limits of the APE on each of these streets are as follows:

- Grand Avenue from 6<sup>th</sup> Street on the north to 9<sup>th</sup> Street on the south
- 6<sup>th</sup> St. from North River Drive on the west to its end point at the Yampah Vapor Caves adjacent to I-70 on the east

Direct impacts from the project will affect some parcels on these streets. Indirect effects will include temporary construction impacts that are unlikely to extend past those properties that front along these streets. The APE includes the parcels that have structures built at least 45 years ago that are adjacent to these streets. For more information about the APE please refer to page 4 of the enclosed report.

**ELIGIBILITY DETERMINATIONS**

As summarized in the following table, a total of 37 properties were newly surveyed or documented on re-visitation forms. Survey was conducted between December 2011 and August 2012 by Gail Keeley of Hermsen Consultants. Site forms are included herewith along with the survey report. For more detailed information regarding eligibility determinations, please review pp. 21-34 of the report.

<b>Surveyed Properties in the APE</b>			
<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
<b>6<sup>th</sup> St. (listed west to east)</b>			
5GF.4726	101 6 <sup>th</sup> St.	Subway	NE
5GF.4718	105 6 <sup>th</sup> St.	Kum & Go	NE
5GF.4721	106 6 <sup>th</sup> St.	Shell	NE
5GF.4722	112 6 <sup>th</sup> St.	Dairy Kreme / Glenwood Activities	NE
5GF.4723	114 6 <sup>th</sup> St.	Sioux Villa Curio	NE
5GF.4719	115 6 <sup>th</sup> St.	Eastwood Motor Company	NE
5GF.3298	205 6 <sup>th</sup> St.	Lincicome Auto / Mountain Sports Outlet	NE
5GF.4724	208-214 6 <sup>th</sup> St.; 631 Grand Ave.	Flower Mart / Gear Exchange / Liquors/ One Love / Bridge Bldg.	NE
5GF.4725	216 6 <sup>th</sup> St.	Polanka Polish Rest. / Natural Medicals	NE
5GF.767	526 Pine St.	Hotel Colorado	E; NRHP; CHD

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
5GF.1050	401 N. River St.	Glenwood Hot Springs Historic District	E
5GF.1050.2	401 N River St.	Hot Springs Bath House / Natatorium	OE; CHD
5GF.2441	601 6 <sup>th</sup> St.	Glenwood Springs Hydroelectric Plant	E; SRHP; NRHP; CHD
5GF.1258	709 6 <sup>th</sup> St.	Yampah Hot Springs Vapor Caves	NE: CHD
<b>Bridge and Railroad</b>			
5GF.2717	SH 82 Bridge over Colorado River	Glenwood Springs Viaduct / SH 82 / Grand Ave.	OE
5GF.1000.7	South edge of Colorado River	Denver & Rio Grande Railroad Tracks	E; CHD
5GF.1050.3	413 7 <sup>th</sup> St.	Denver & Rio Grande Railroad Station	E; CHD
<b>Grand Ave. (listed north to south)</b>			
5GF.4727	701 Grand Ave.	Rib City	NE
5GF.4731	702 Grand Ave.	Riviera Lounge / Peppo Nino Restaurant	NE
5GF.4732	710 Grand Ave.	Fins Grille and Raw Bar	NE
5GF.4728	711 Grand Ave.	Immortal Images Tattoo	NE
5GF.1035	714 Grand Ave.	Mirror Bar / Glenwood Shoe Service / King Barber Shop	NE
5GF.1015	715 Grand Ave.	Silver Club Bldg.	E
5GF.1034	716 Grand Ave.	Green Natural Solutions	NE
5GF.1016	717 Grand Ave.	Palace Hotel / Elizabeth Dean Boutique	E
5GF.1017	719 Grand Ave.	Parkison Bldg / The Candlestick Maker	E
5GF.4733	720 Grand Ave.	King Mall	NE
5GF.1018	721 Grand Ave.	Florindo's Italian Cuisine	NE
5GF.1033	722-724 Grand Ave.	Springs Restaurant / Doc Holliday Saloon	E
5GF.4729	723 Grand Ave.	The Book Train	NE
5GF.1019	725-727 Grand Ave.	Dougan Block / Sacred Grounds	E
5GF.1032	726 Grand Ave.	Ore Sample Room/ Narcissus Hair Studio	E
5GF.1020	729 Grand Ave.	Daily Bread Café and Bakery	NE
5GF.1021	731 Grand Ave.	Kamm-Dever Bldg.	ONE
5GF.4734	732 Grand Ave.	Bullock's Department Store / Summit Canyon Mountaineering	NE
5GF.1022	801 Grand Ave.	Citizens National Bank	E; NRHP
5GF.1023	805 Grand Ave.	Thorson Building	NE

E; NRHP = Eligible; listed on the NRHP  
 E; SRHP = Eligible; listed on the State Register of Historic Places  
 NE = Not Eligible for the NRHP  
 OE = Officially Eligible for the NRHP  
 ONE = Officially Not Eligible for the NRHP  
 CHD = Contributing Historic District

**EFFECTS DETERMINATIONS**

Fourteen eligible or listed properties and twenty-three non-eligible properties were identified. Detailed effects determinations are included for the fourteen eligible or listed properties; no effects determinations

are presented for the twenty-three properties determined not eligible. Because those properties were determined not eligible, the project results in a finding of *no historic properties affected*.

**Hotel Colorado (5GF767, 526 Pine Street):** There will be no direct impact to this property as a result of the Build Alternative. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west. A temporary detour near the Yampah Vapor Caves is planned to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 (Figure 6). The detour, to be built entirely within CDOT or City right-of-way, would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near this property. The temporary and indirect impacts will not impair the ability of the property to convey the significance that made it eligible for inclusion on the NRHP. Therefore, CDOT has determined that the project results in *no adverse effect* to 5GF767.

**Glenwood Hot Springs Historic District (5GF1050):** There will be no direct or permanent impacts to any of the NRHP-eligible properties within the historic district. Temporary effects from construction activities anticipated include construction equipment operation, demolition of the existing road and pedestrian bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. A temporary detour near the Yampah Vapor Caves (5GF1258) (a contributing element to the district) is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70. The detour would be built entirely within CDOT or City right-of-way. The detour would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street within the Glenwood Hot Springs Historic District.

Views from the Hot Springs Pool toward the bridges would change with construction of the new highway and pedestrian bridges. The new pedestrian bridge would be similar in height to the existing bridge, but the bridge deck would be 16 feet wide (versus the existing width of between 10 and 14 feet), and its touchdown location on the north side of the river would be similar to that of the existing pedestrian bridge. Views of the highway bridge would be reduced because the new bridge would curve away to the west as it crosses the river. The new pedestrian bridge would continue to partially obscure views of the new highway bridge. As noted above, three bridge types are being considered for the pedestrian bridge. The super structures associated with these bridge types would be taller than the existing structure, making the presence of the pedestrian bridge more noticeable and visually prominent than the existing pedestrian bridge. The noise analysis conducted for this project indicates that the Build Alternative would result in lower noise levels at the Hot Springs than the No Action Alternative, as discussed below under the Glenwood Hot Springs Bathhouse/Natatorium.

There will be temporary effects of increased dust, noise, and vibration during construction, nighttime traffic during periodic use of the 6<sup>th</sup> Street detour, and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics that qualify the district for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that the proposed plan will result in *no adverse effect*.

**Glenwood Hot Springs Bathhouse/Natatorium (5GF1050.2, 401 North River Street):** There will be no direct impact to this property. According to the noise analysis, the existing noise level at this property is 67.4 dBA, which is 1.4 dBA above FHWA's Noise Abatement Criteria of 66.0 dBA for this land use type. The 2035 noise level is projected to be 69.3 dBA under the No Action Alternative and 66.2 dBA under the Build Alternative. Therefore, the Build Alternative would result in lower noise levels (by 3.1 dBA) at this property. Temporary effects from construction activities would include construction equipment operation, demolition of the pedestrian bridge and bridge piers; temporary falsework

construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. There also will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west and a new pedestrian bridge. The temporary detour near the Yampah Vapor Caves described above will also pass near this property, and would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street. The temporary and indirect impacts will not alter the characteristics that qualify 5GF1050.2 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Hydroelectric Plant (5GF2441, 601 6th St.):** There will be no direct impact to this property. Temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west are anticipated. The temporary detour near the Yampah Vapor Caves would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near the plant. The temporary and indirect impacts will not alter the characteristics that qualify 5GF2441 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Viaduct/SH 82/Grand Avenue Bridge (5GF2717):** This eligible property will be removed and replaced with a new structure. CDOT has determined that this change will result in an *adverse effect*. A discussion regarding why the bridge is being replaced as well as a review of alternatives that avoid the bridge is included as Attachment 1. The information included in the attachment will be used in the Section 4(f) Evaluation for this resource.

**Denver & Rio Grande Railroad Tracks (5GF1000.7):** The new Grand Avenue bridge will be a maximum of approximately 62 feet wide, which is 22 feet wider than the existing structure. The railroad presently crosses under the bridge, which will not change other than the new bridge will be 1.0 to 1.5 ft. higher above the tracks and 22 feet wider than the existing bridge at the railroad crossing. There would be temporary effects within the railroad right-of-way (the historic boundary) during construction of the highway and pedestrian bridges, including construction equipment operation, demolition of the existing highway and pedestrian bridge and bridge piers, girder erection over the railroad, temporary falsework construction, regrading and temporary shoring construction, construction of adjacent bridge piers and retaining walls, and bridge deck construction over the railroad.

Additionally, a temporary grade crossing would be constructed to provide construction access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. The crossing would be built of railroad approved materials consisting of aggregate, wood, and pavement. The crossing would be located adjacent to the temporary construction impact area at a specific location to be determined in consultation with the railroad. It is anticipated that the crossing would be used at pre-scheduled times during a majority of the construction period, specifically to construct and remove cofferdams, remove existing bridge materials and construct the new bridge. Upon construction completion, the temporary rail grade crossing would be removed and the railroad right-of-way returned to its current condition and appearance. The proposed change to the width of the overhead bridge only affects a small portion of the overall resource and will not change the characteristics that make the D&RG eligible to the NRHP. The railroad will still be able to convey its significance and continue operations with the slightly wider bridge crossing overhead. Therefore, CDOT has determined that this wider crossing will result in *no adverse effect*.

**Denver & Rio Grande Railroad Station (5GF1050.3, 413 7th St.):** There will be no direct impact to this property. A scissor ramp would be built on the east side of the new pedestrian bridge to provide a

connection from the pedestrian bridge to 7<sup>th</sup> Street (Figure 5), replacing the existing stair access and kiosk and resulting in minor visual changes from the station. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Silver Club Building (5GF1015, 715 Grand Ave.):** The proposed bridge will be between 8.5 and 12.5 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 21 ft. from the west edge of the bridge. The profile of the new structure will be approximately 3-4 ft. higher in front of the building than the existing configuration. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Palace Hotel (5GF1016, 717 Grand Ave.):** The proposed bridge will be an average of 11 ft. closer to this building. Currently it is about 30 ft. from the edge of the bridge, whereas it will be an average of 19 ft. from the west edge of the new bridge. The profile of the new bridge will be approximately 3-4 ft. higher in front of this building than the existing bridge. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Parkison Building (5GF1017, 719 Grand Ave.):** The proposed bridge will be 12 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 18 ft. from the west edge of the bridge. The profile of the new bridge will be approximately 3-4 feet higher in front of the building than the existing bridge. Figures 2-4 show an illustration of existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Springs Restaurant/Doc Holliday Tavern (5GF1033, 722-724 Grand Ave.):** With construction of the proposed bridge, Grand Ave. will be 7 ft. closer to this building, which is presently about 25.5 feet from the edge of Grand Ave.; after the bridge is built, that distance will be 18 ft. The profile of the new bridge will be approximately two feet higher in front of this building than the existing bridge (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Dougan Block / Sacred Grounds Coffeeshouse/Dancing Bear Trading Post (5GF1019, 725-727 Grand Ave.):** With construction of the proposed bridge, Grand Avenue will be about 2 ft. closer to this building; the existing bridge lands just to the north. Grand Avenue is at-grade with the curb line about 22 ft. from the middle of the building. The new bridge approach will be on retaining walls in front of the building, with the west curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 feet higher in front of this building than the existing roadway, and a 1.5-2-foot bridge rail will be added due to the elevation change; therefore the building front will be approximately 18 feet from the back of the bridge rail (Figures 2-4). According to the noise analysis conducted for the project, the existing noise level at this property is 70.2 dBA, which is 0.8 dBA below FHWA's Noise Abatement Criteria of 71.0 dBA for this land use type. The 2035 noise levels at the property would be 72.3 dBA under the No Action Alternative and 72.8 dBA under the Build Alternative. Both alternatives would exceed the NAC; however, this slight increase would be imperceptible to the human ear. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Ore Sampling Room/Narcissus Hair Studio (5GF1032, 726 Grand Ave.):** With construction of the proposed bridge, Grand Avenue traffic will be about 2 ft. closer to this building. The existing bridge

lands north of the building and Grand Avenue is at-grade, with the lanes about 22 feet away. The existing curb is about 8 feet away, and there is on-street parking and an access lane for Wing Street in front of the building. After the bridge is constructed, the Wing Street access and parking will be removed and the bridge approach will be on retaining walls in front of the building, with the east curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 ft. higher in front of the building than the existing roadway, and a 1.5-2 ft. bridge rail will be added due to the elevation change so the building front will be approximately 18 ft. from the back of the bridge rail (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* to 5GF1032.

**Citizens National Bank Building (5GF1022, 801 Grand Ave.):** There will be no direct impact to this property. The new bridge will touch down approximately one-half block to the north of the building. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. However, the temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no historic properties affected*.

## **SECTION 4(F) AND DE MINIMIS**

### **Background**

In addition to Section 106 of the NHPA, FHWA must comply with Section 4(f), which is codified at both 49 U.S.C § 303 and 23 U.S.C. § 138. Congress amended Section 4(f) when it enacted the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (Public Law 109-59, enacted August 10, 2005) (“SAFETEA-LU”). Section 6009 of SAFETEA-LU added a new subsection to Section 4(f), which authorizes FHWA to approve a project that uses Section 4(f) lands that are part of a historic property without preparation of an Avoidance Analysis, if it makes a finding that such uses would have “*de minimis*” impacts upon the Section 4(f) resource, with the concurrence of the SHPO.

On December 12, 2005, the Federal Highway Administration issued its “Guidance for Determining *De Minimis* Impacts to Section 4(f) Resources” which indicates that a finding of *de minimis* can be made when the Section 106 process results in a *no adverse effect* or *no historic properties affected* determination, when the SHPO is informed of the FHWA’s intent to make a *de minimis* impact finding based on their written concurrence in the Section 106 determination, and when FHWA has considered the views of any Section 106 consulting parties participating in the Section 106 process. This new provision of Section 4(f) and the associated guidance are in part the basis of this letter, and of FHWA’s determination and notification of *de minimis* impacts to the Frontier Historical Society with respect to the proposed project. At this time we are notifying the Section 106 consulting parties per section 6009(b)(2)(C). On March 12, 2008, FHWA issued a Final Rule on Section 4(f), which clarifies and implements the procedures for determining a *de minimis* impact. In addition the Final Rule moves the Section 4(f) regulation to 23 CFR 774.

### **Notification of Section 4(f) DeMinimis Determination**

The project has been determined to have *no adverse effect* to 5GF767, 5GF1050, 5GF1050.2, 5GF1050.3, 5GF2441, and 5GF1000.7. Based on the findings outlined above, FHWA may make a *de minimis* finding for the Section 4(f) requirements for these properties.

These materials have been sent to the State Historic Preservation Officer (SHPO) for review.

Ms. Hines  
August 5, 2013  
Page 8

As a local historical society, we welcome your comments on this project. Should you choose to respond, we request your comments within 30 days of receipt of these materials. If we do not hear from you in this time frame, we will assume you do not plan to comment. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Historian, Lisa Schoch, at 303-512-4258.

If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

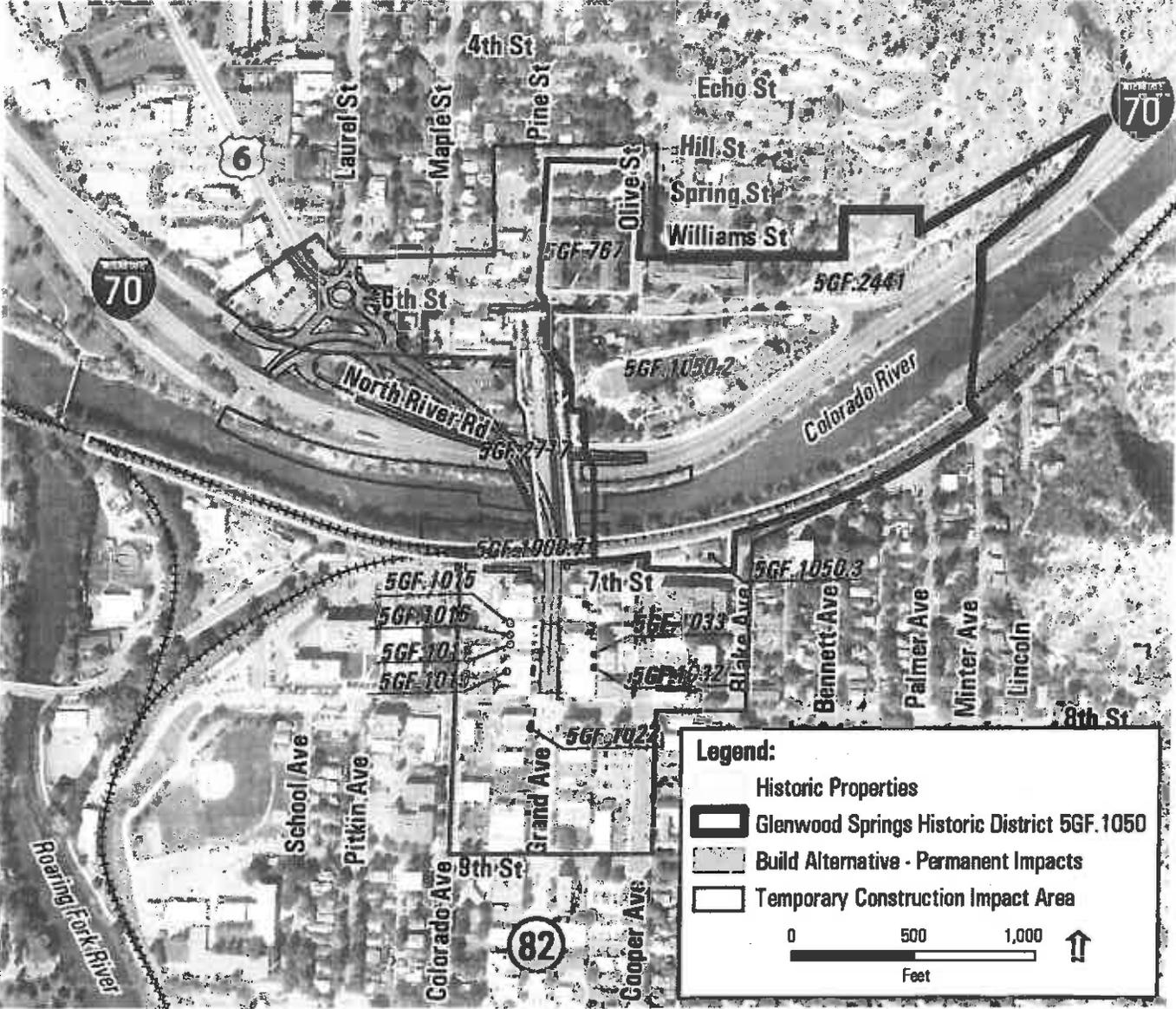
Very truly yours,



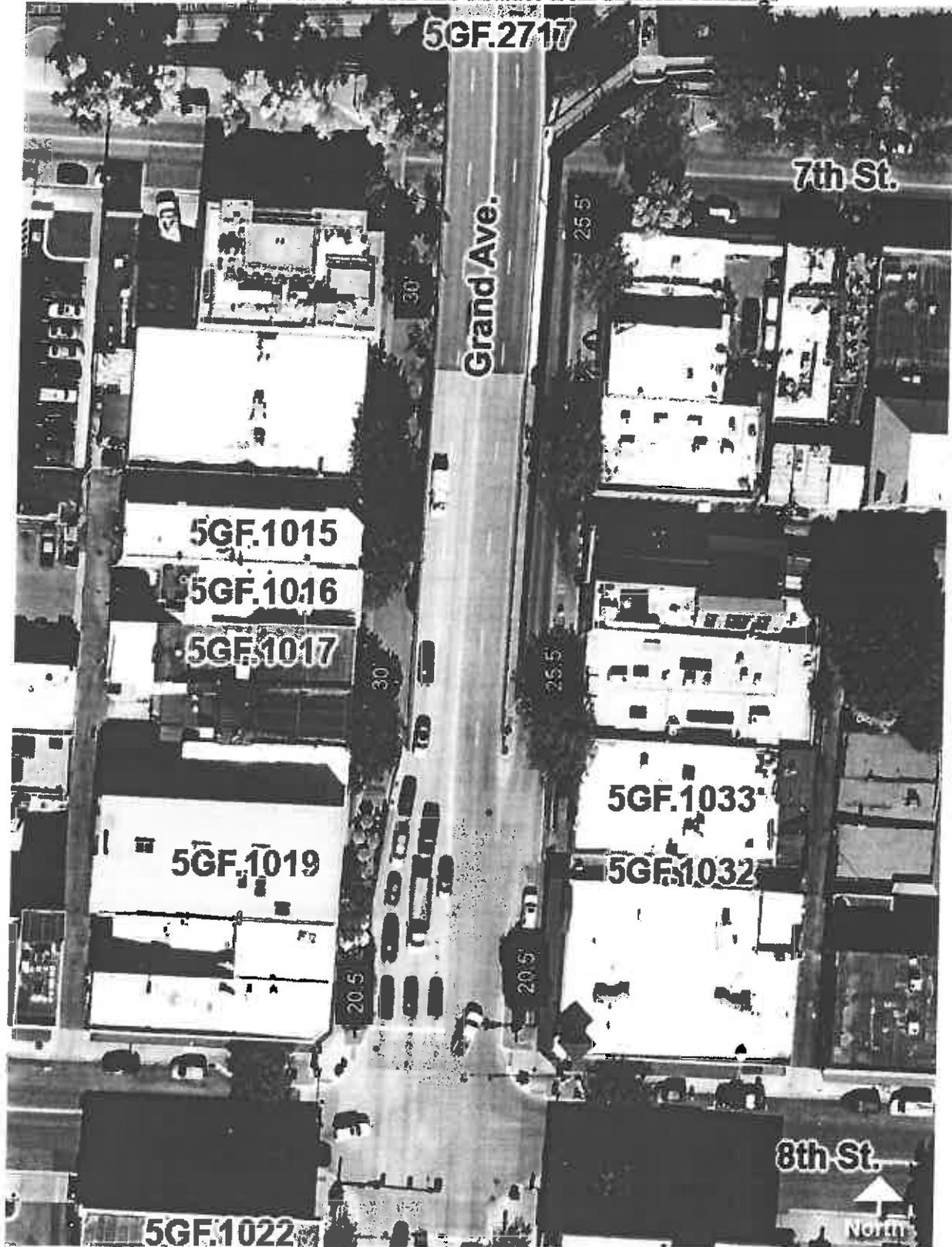
Jane Hann, Manager  
Environmental Programs Branch

Enclosures:      Survey Report  
                         Attachment 1

Figure 1: Build Alternative Impacts



**Figure 2: Existing Grand Avenue between 7<sup>th</sup> Street and 8<sup>th</sup> Street, Illustrates existing bridge/roadway width and distance from adjacent buildings**



**Figure 3: Build Alternative along Grand Avenue between 7<sup>th</sup> and 8<sup>th</sup> Streets, Illustrates Build Alternative and distance from adjacent buildings**

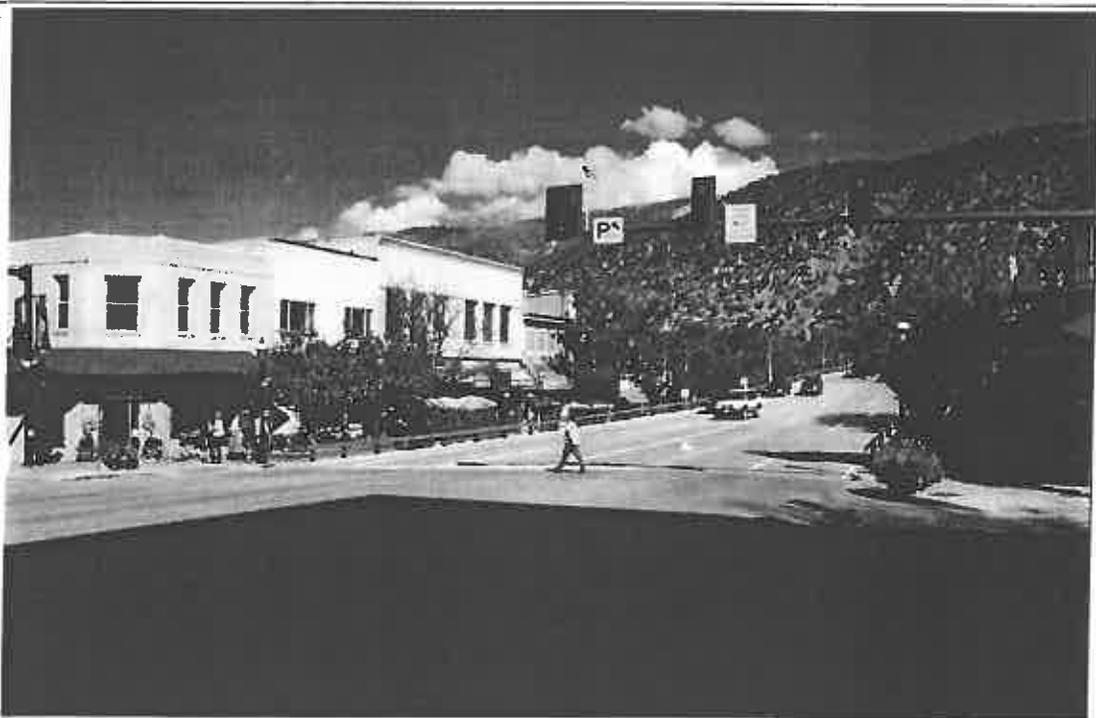


**Figure 4: Existing and Proposed Bridge Heights, Illustrates existing and proposed bridge height relative to buildings adjacent to Grand Avenue**

**Existing  
Grand  
Avenue at  
8<sup>th</sup> Street  
Looking  
North  
Toward  
River /  
Bridge**



**Build  
Alterna-  
tive from  
same view-  
point as  
above**



**Figure 5: Proposed Scissor Ramp on East Side of Bridge**

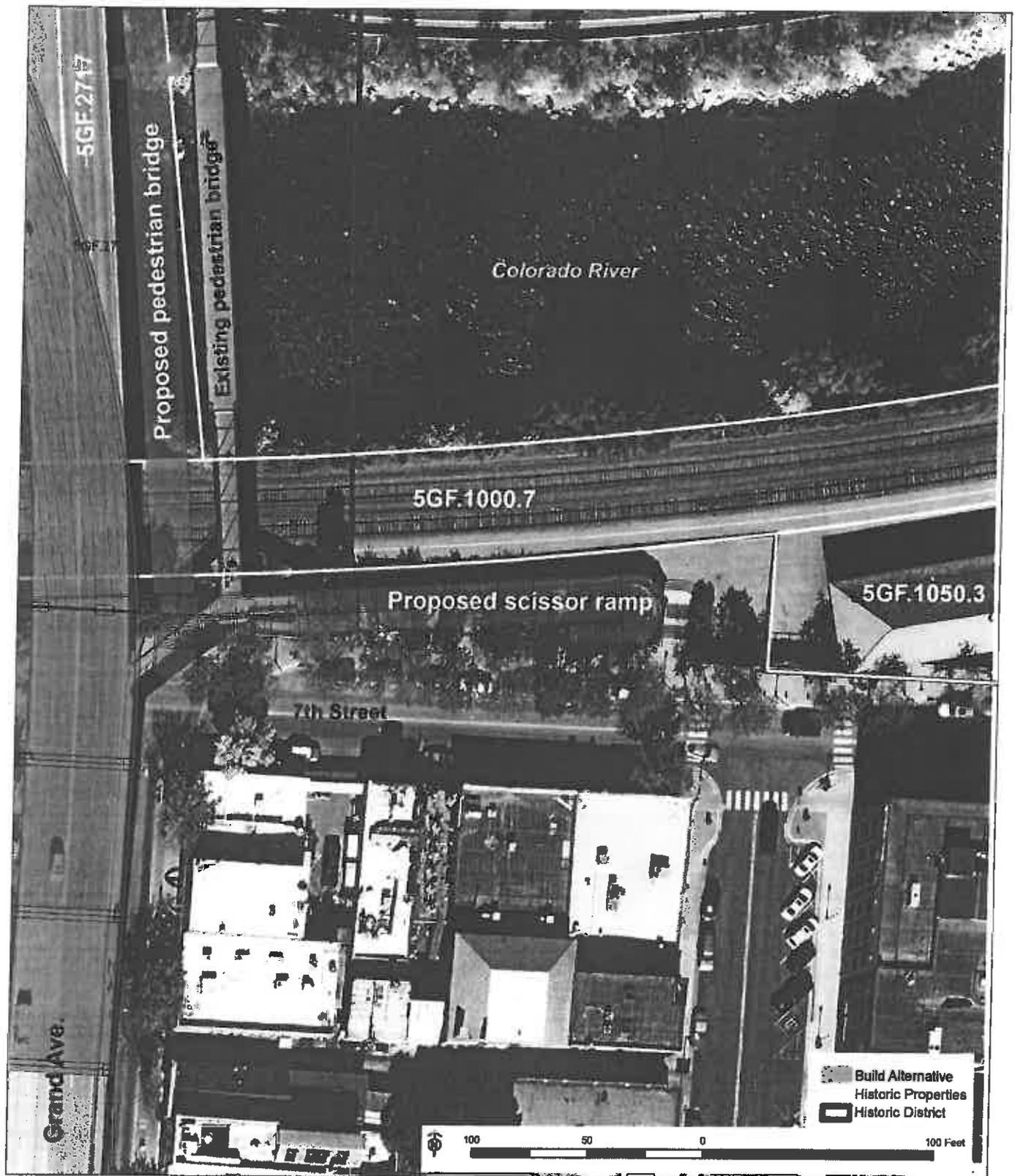
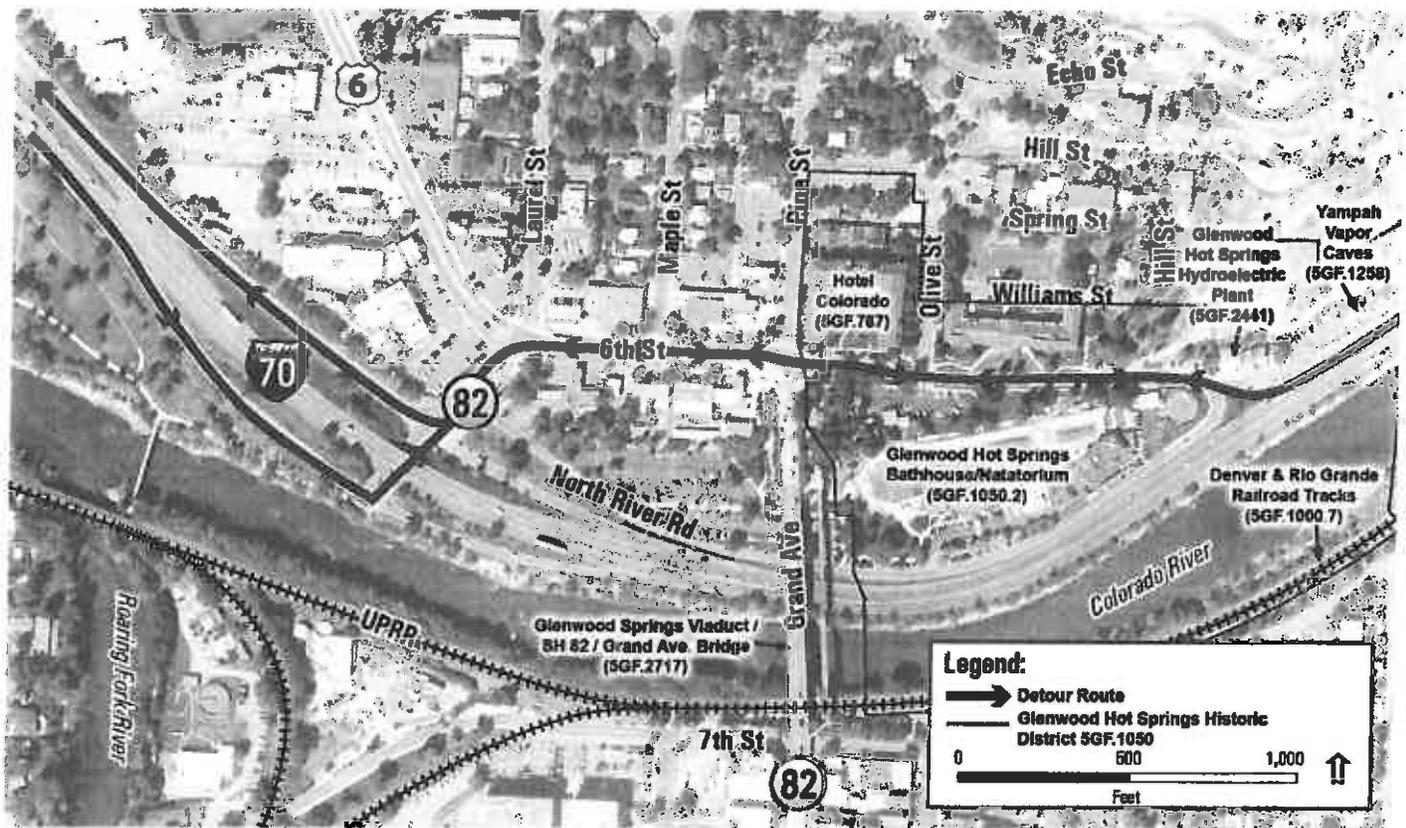


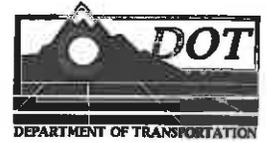
Figure 6: Temporary Detour near the Yampah Vapor Caves



# STATE OF COLORADO

## DEPARTMENT OF TRANSPORTATION

Environmental Programs Branch  
4201 East Arkansas Avenue  
Shumate Building  
Denver, Colorado 80222  
(303) 757-9281



August 5, 2013

Ms. Gretchen Ricehill  
City of Glenwood Springs  
Historic Preservation Commission  
101 8<sup>th</sup> Street  
Glenwood Springs, CO 81601

**SUBJECT:** Determinations of Eligibility and Effects, Notification of Section 4(f) *DeMinimis* and Section 4(f) Exception for Temporary Occupancy Criteria, Project FBR 0821-094, Grand Avenue Bridge Replacement, Glenwood Springs

Dear Ms. Ricehill:

This letter and associated materials constitute a request for comments on Determinations of Eligibility and Effects for the project referenced above, as well as acknowledgement of Section 4(f) criteria. The City of Glenwood Springs and Colorado Department of Transportation (CDOT) propose to replace the State Highway 82/Grand Avenue bridge spanning the Colorado River. The project is being documented as an Environmental Assessment (EA) by the Federal Highway Administration (FHWA) and CDOT.

CDOT is submitting this to you in compliance with Section 106 of the National Historic Preservation Act, which requires federal agencies to evaluate the effects of their undertakings on historic properties. The Glenwood Springs Historic Preservation Commission, a Certified Local Government, has been identified as a potential consulting party for this project. For more information about Section 106 and how you can participate as a consulting party, please visit the Advisory Council on Historic Preservation's web site, which contains the Citizen's Guide to Section 106 Review at <http://www.achp.gov/docs/CitizenGuide.pdf>.

### **PROJECT DESCRIPTION**

The Build Alternative would replace the SH 82/Grand Avenue bridge over the Colorado River with a five-span bridge with haunched or constant depth girders. The south end of the new bridge would start just north of the 8<sup>th</sup> Street/Grand Avenue intersection, continue on the existing alignment as it crosses 7<sup>th</sup> Street and the Union Pacific Railroad, curve west just past the railroad as it crosses the Colorado River, then touch down on the north side of the river near the 6<sup>th</sup> and Laurel Streets intersection, connecting to I-70 Exit 116 (Figure 1).

The existing pedestrian bridge located immediately east of the highway bridge would also be replaced. Three pedestrian bridge types are currently being considered: arch bridge, symmetric cable supported bridge, and asymmetric cable supported bridge. A staircase would lead pedestrians from the south side of the pedestrian bridge to downtown Glenwood Springs. To meet requirements of the Americans with Disability Act, the bicyclist/pedestrian connection on the south side would include either an elevator system and/or a scissor ramp connecting to 7<sup>th</sup> Street on the east side of the bridge (see Figure 5).

The Build Alternative would reconfigure the signalized intersection where the new highway bridge would touch down north of the river at 6<sup>th</sup> and Laurel Streets. A grade-separated pedestrian connection is

proposed in a box culvert under the relocated intersection, connecting the sidewalks on the northeast and southwest sides. The sidewalk connection on the north side of the culvert comes out at grade and rises up to 6<sup>th</sup> Street. The Build Alternative also includes several improvements to existing facilities adjacent to the alignment and bridges as part of the construction detour route that would be used during the approximately 30- to 60-day bridge closure. Although constructed for the detour, several of these improvements would be permanent because they would also provide long-term transportation benefits, while several changes would only be in place during the detour period. The permanent improvements would be minor surface improvements and would occur within the existing road right-of-way. Permanent improvements include the following: extending the I-70 westbound acceleration and deceleration lanes at Exit 116; extending the eastbound off-ramp deceleration lane at Exit 114; modifying the Exit 114 roundabouts to better accommodate large trucks; and restriping at several intersections along Midland Avenue and 27<sup>th</sup> St. The temporary changes include modifying the Midland/27<sup>th</sup> roundabout and removing traffic calming on Midland Avenue at 10<sup>th</sup> and 13<sup>th</sup> Streets. A temporary detour near the Yampah Vapor Caves is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 for bridge demolition/construction (see Figure 6 illustrating detour route). The detour would be built entirely within CDOT or City right-of-way. The temporary changes would be removed and the original features restored or reconstructed at the end of the project.

**AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) includes the parcels adjacent to 6<sup>th</sup> Street, Grand Avenue, and the area within the Hot Springs Historic District. The limits of the APE on each of these streets are as follows:

- **Grand Avenue** from 6<sup>th</sup> Street on the north to 9<sup>th</sup> Street on the south
- **6<sup>th</sup> St.** from North River Drive on the west to its end point at the Yampah Vapor Caves adjacent to I-70 on the east

Direct impacts from the project will affect some parcels on these streets. Indirect effects will include temporary construction impacts that are unlikely to extend past those properties that front along these streets. The APE includes the parcels that have structures built at least 45 years ago that are adjacent to these streets. For more information about the APE please refer to page 4 of the enclosed report.

**ELIGIBILITY DETERMINATIONS**

As summarized in the following table, a total of 37 properties were newly surveyed or documented on re-visitation forms. Survey was conducted between December 2011 and August 2012 by Gail Keeley of Hermsen Consultants. Site forms are included herewith along with the survey report. For more detailed information regarding eligibility determinations, please review pp. 21-34 of the report.

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
<b>6<sup>th</sup> St. (listed west to east)</b>			
5GF.4726	101 6 <sup>th</sup> St.	Subway	NE
5GF.4718	105 6 <sup>th</sup> St.	Kum & Go	NE
5GF.4721	106 6 <sup>th</sup> St.	Shell	NE
5GF.4722	112 6 <sup>th</sup> St.	Dairy Kreme / Glenwood Activities	NE
5GF.4723	114 6 <sup>th</sup> St.	Sioux Villa Curio	NE
5GF.4719	115 6 <sup>th</sup> St.	Eastwood Motor Company	NE
5GF.3298	205 6 <sup>th</sup> St.	Lincicome Auto / Mountain Sports Outlet	NE
5GF.4724	208-214 6 <sup>th</sup> St.;	Flower Mart / Gear Exchange / Liquors/ One	NE

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
	631 Grand Ave.	Love / Bridge Bldg.	
5GF.4725	216 6 <sup>th</sup> St.	Polanka Polish Rest. / Natural Medicals	NE
5GF.767	526 Pine St.	Hotel Colorado	E; NRHP; CHD
5GF.1050	401 N. River St.	Glenwood Hot Springs Historic District	E
5GF.1050.2	401 N River St.	Hot Springs Bath House / Natatorium	OE; CHD
5GF.2441	601 6 <sup>th</sup> St.	Glenwood Springs Hydroelectric Plant	E; SRHP; NRHP; CHD
5GF.1258	709 6 <sup>th</sup> St.	Yampah Hot Springs Vapor Caves	NE; CHD
<b>Bridge and Railroad</b>			
5GF.2717	SH 82 Bridge over Colorado River	Glenwood Springs Viaduct / SH 82 / Grand Ave.	OE
5GF.1000.7	South edge of Colorado River	Denver & Rio Grande Railroad Tracks	E; CHD
5GF.1050.3	413 7 <sup>th</sup> St.	Denver & Rio Grande Railroad Station	E; CHD
<b>Grand Ave. (listed north to south)</b>			
5GF.4727	701 Grand Ave.	Rib City	NE
5GF.4731	702 Grand Ave.	Riviera Lounge / Peppo Nino Restaurant	NE
5GF.4732	710 Grand Ave.	Fins Grille and Raw Bar	NE
5GF.4728	711 Grand Ave.	Immortal Images Tattoo	NE
5GF.1035	714 Grand Ave.	Mirror Bar / Glenwood Shoe Service / King Barber Shop	NE
5GF.1015	715 Grand Ave.	Silver Club Bldg.	E
5GF.1034	716 Grand Ave.	Green Natural Solutions	NE
5GF.1016	717 Grand Ave.	Palace Hotel / Elizabeth Dean Boutique	E
5GF.1017	719 Grand Ave.	Parkison Bldg / The Candlestick Maker	E
5GF.4733	720 Grand Ave.	King Mall	NE
5GF.1018	721 Grand Ave.	Florindo's Italian Cuisine	NE
5GF.1033	722-724 Grand Ave.	Springs Restaurant / Doc Holliday Saloon	E
5GF.4729	723 Grand Ave.	The Book Train	NE
5GF.1019	725-727 Grand Ave.	Dougan Block / Sacred Grounds	E
5GF.1032	726 Grand Ave.	Ore Sample Room/ Narcissus Hair Studio	E
5GF.1020	729 Grand Ave.	Daily Bread Café and Bakery	NE
5GF.1021	731 Grand Ave.	Kamm-Dever Bldg.	ONE
5GF.4734	732 Grand Ave.	Bullock's Department Store / Summit Canyon Mountaineering	NE
5GF.1022	801 Grand Ave.	Citizens National Bank	E; NRHP
5GF.1023	805 Grand Ave.	Thorson Building	NE

E; NRHP = Eligible; listed on the NRHP

E; SRHP = Eligible; listed on the State Register of Historic Places

NE = Not Eligible for the NRHP

OE = Officially Eligible for the NRHP

ONE = Officially Not Eligible for the NRHP

CHD = Contributing Historic District

### **EFFECTS DETERMINATIONS**

Fourteen eligible or listed properties and twenty-three non-eligible properties were identified. Detailed effects determinations are included for the fourteen eligible or listed properties; no effects determinations are presented for the twenty-three properties determined not eligible. Because those properties were determined not eligible, the project results in a finding of *no historic properties affected*.

**Hotel Colorado (5GF767, 526 Pine Street):** There will be no direct impact to this property as a result of the Build Alternative. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west. A temporary detour near the Yampah Vapor Caves is planned to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 ( Figure 6). The detour, to be built entirely within CDOT or City right-of-way, would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near this property. The temporary and indirect impacts will not impair the ability of the property to convey the significance that made it eligible for inclusion on the NRHP. Therefore, CDOT has determined that the project results in *no adverse effect* to 5GF767.

**Glenwood Hot Springs Historic District (5GF1050):** There will be no direct or permanent impacts to any of the NRHP-eligible properties within the historic district. Temporary effects from construction activities anticipated include construction equipment operation, demolition of the existing road and pedestrian bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. A temporary detour near the Yampah Vapor Caves (5GF1258) (a contributing element to the district) is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70. The detour would be built entirely within CDOT or City right-of-way. The detour would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street within the Glenwood Hot Springs Historic District.

Views from the Hot Springs Pool toward the bridges would change with construction of the new highway and pedestrian bridges. The new pedestrian bridge would be similar in height to the existing bridge, but the bridge deck would be 16 feet wide (versus the existing width of between 10 and 14 feet), and its touchdown location on the north side of the river would be similar to that of the existing pedestrian bridge. Views of the highway bridge would be reduced because the new bridge would curve away to the west as it crosses the river. The new pedestrian bridge would continue to partially obscure views of the new highway bridge. As noted above, three bridge types are being considered for the pedestrian bridge. The super structures associated with these bridge types would be taller than the existing structure, making the presence of the pedestrian bridge more noticeable and visually prominent than the existing pedestrian bridge. The noise analysis conducted for this project indicates that the Build Alternative would result in lower noise levels at the Hot Springs than the No Action Alternative, as discussed below under the Glenwood Hot Springs Bathhouse/Natatorium.

There will be temporary effects of increased dust, noise, and vibration during construction, nighttime traffic during periodic use of the 6<sup>th</sup> Street detour, and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics that qualify the district for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that the proposed plan will result in *no adverse effect*.

**Glenwood Hot Springs Bathhouse/Natatorium (5GF1050.2, 401 North River Street):** There will be no direct impact to this property. According to the noise analysis, the existing noise level at this property is 67.4 dBA, which is 1.4 dBA above FHWA's Noise Abatement Criteria of 66.0 dBA for this land use type. The 2035 noise level is projected to be 69.3 dBA under the No Action Alternative and 66.2 dBA

under the Build Alternative. Therefore, the Build Alternative would result in lower noise levels (by 3.1 dBA) at this property. Temporary effects from construction activities would include construction equipment operation, demolition of the pedestrian bridge and bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. There also will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west and a new pedestrian bridge. The temporary detour near the Yampah Vapor Caves described above will also pass near this property, and would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street. The temporary and indirect impacts will not alter the characteristics that qualify 5GF1050.2 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Hydroelectric Plant (5GF2441, 601 6th St.):** There will be no direct impact to this property. Temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west are anticipated. The temporary detour near the Yampah Vapor Caves would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near the plant. The temporary and indirect impacts will not alter the characteristics that qualify 5GF2441 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Viaduct/SH 82/Grand Avenue Bridge (5GF2717):** This eligible property will be removed and replaced with a new structure. CDOT has determined that this change will result in an *adverse effect*. A discussion regarding why the bridge is being replaced as well as a review of alternatives that avoid the bridge is included as Attachment 1. The information included in the attachment will be used in the Section 4(f) Evaluation for this resource.

**Denver & Rio Grande Railroad Tracks (5GF1000.7):** The new Grand Avenue bridge will be a maximum of approximately 62 feet wide, which is 22 feet wider than the existing structure. The railroad presently crosses under the bridge, which will not change other than the new bridge will be 1.0 to 1.5 ft. higher above the tracks and 22 feet wider than the existing bridge at the railroad crossing. There would be temporary effects within the railroad right-of-way (the historic boundary) during construction of the highway and pedestrian bridges, including construction equipment operation, demolition of the existing highway and pedestrian bridge and bridge piers, girder erection over the railroad, temporary falsework construction, regrading and temporary shoring construction, construction of adjacent bridge piers and retaining walls, and bridge deck construction over the railroad.

Additionally, a temporary grade crossing would be constructed to provide construction access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. The crossing would be built of railroad approved materials consisting of aggregate, wood, and pavement. The crossing would be located adjacent to the temporary construction impact area at a specific location to be determined in consultation with the railroad. It is anticipated that the crossing would be used at pre-scheduled times during a majority of the construction period, specifically to construct and remove cofferdams, remove existing bridge materials and construct the new bridge. Upon construction completion, the temporary rail grade crossing would be removed and the railroad right-of-way returned to its current condition and appearance. The proposed change to the width of the overhead bridge only affects a small portion of the overall resource and will not change the characteristics that make the D&RG eligible to the NRHP. The railroad will still be able to convey its significance and continue operations with the slightly wider bridge crossing overhead. Therefore, CDOT has determined that this wider crossing will result in *no adverse effect*.

**Denver & Rio Grande Railroad Station (5GF1050.3, 413 7th St.):** There will be no direct impact to this property. A scissor ramp would be built on the east side of the new pedestrian bridge to provide a connection from the pedestrian bridge to 7<sup>th</sup> Street (Figure 5), replacing the existing stair access and kiosk and resulting in minor visual changes from the station. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Silver Club Building (5GF1015, 715 Grand Ave.):** The proposed bridge will be between 8.5 and 12.5 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 21 ft. from the west edge of the bridge. The profile of the new structure will be approximately 3-4 ft. higher in front of the building than the existing configuration. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Palace Hotel (5GF1016, 717 Grand Ave.):** The proposed bridge will be an average of 11 ft. closer to this building. Currently it is about 30 ft. from the edge of the bridge, whereas it will be an average of 19 ft. from the west edge of the new bridge. The profile of the new bridge will be approximately 3-4 ft. higher in front of this building than the existing bridge. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Parkison Building (5GF1017, 719 Grand Ave.):** The proposed bridge will be 12 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 18 ft. from the west edge of the bridge. The profile of the new bridge will be approximately 3-4 feet higher in front of the building than the existing bridge. Figures 2-4 show an illustration of existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Springs Restaurant/Doc Holliday Tavern (5GF1033, 722-724 Grand Ave.):** With construction of the proposed bridge, Grand Ave. will be 7 ft. closer to this building, which is presently about 25.5 feet from the edge of Grand Ave.; after the bridge is built, that distance will be 18 ft. The profile of the new bridge will be approximately two feet higher in front of this building than the existing bridge (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Dougan Block / Sacred Grounds Coffeehouse/Dancing Bear Trading Post (5GF1019, 725-727 Grand Ave.):** With construction of the proposed bridge, Grand Avenue will be about 2 ft. closer to this building; the existing bridge lands just to the north. Grand Avenue is at-grade with the curb line about 22 ft. from the middle of the building. The new bridge approach will be on retaining walls in front of the building, with the west curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 feet higher in front of this building than the existing roadway, and a 1.5-2-foot bridge rail will be added due to the elevation change; therefore the building front will be approximately 18 feet from the back of the bridge rail (Figures 2-4). According to the noise analysis conducted for the project, the existing noise level at this property is 70.2 dBA, which is 0.8 dBA below FHWA's Noise Abatement Criteria of 71.0 dBA for this land use type. The 2035 noise levels at the property would be 72.3 dBA under the No Action Alternative and 72.8 dBA under the Build Alternative. Both alternatives would exceed the NAC; however, this slight increase would be imperceptible to the human ear. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Ore Sampling Room/Narcissus Hair Studio (5GF1032, 726 Grand Ave.):** With construction of the proposed bridge, Grand Avenue traffic will be about 2 ft. closer to this building. The existing bridge lands north of the building and Grand Avenue is at-grade, with the lanes about 22 feet away. The existing curb is about 8 feet. away, and there is on-street parking and an access lane for Wing Street in front of the building. After the bridge is constructed, the Wing Street access and parking will be removed and the bridge approach will be on retaining walls in front of the building, with the east curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 ft. higher in front of the building than the existing roadway, and a 1.5-2 ft. bridge rail will be added due to the elevation change so the building front will be approximately 18 ft. from the back of the bridge rail (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* to 5GF1032.

**Citizens National Bank Building (5GF1022, 801 Grand Ave.):** There will be no direct impact to this property. The new bridge will touch down approximately one-half block to the north of the building. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. However, the temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no historic properties affected*.

#### **SECTION 4(F) AND DE MINIMIS**

##### **Background**

In addition to Section 106 of the NHPA, FHWA must comply with Section 4(f), which is codified at both 49 U.S.C § 303 and 23 U.S.C. § 138. Congress amended Section 4(f) when it enacted the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (Public Law 109-59, enacted August 10, 2005) (“SAFETEA-LU”). Section 6009 of SAFETEA-LU added a new subsection to Section 4(f), which authorizes FHWA to approve a project that uses Section 4(f) lands that are part of a historic property without preparation of an Avoidance Analysis, if it makes a finding that such uses would have “*de minimis*” impacts upon the Section 4(f) resource, with the concurrence of the SHPO.

On December 12, 2005, the Federal Highway Administration issued its “Guidance for Determining *De Minimis* Impacts to Section 4(f) Resources” which indicates that a finding of *de minimis* can be made when the Section 106 process results in a *no adverse effect* or *no historic properties affected* determination, when the SHPO is informed of the FHWA’s intent to make a *de minimis* impact finding based on their written concurrence in the Section 106 determination, and when FHWA has considered the views of any Section 106 consulting parties participating in the Section 106 process. This new provision of Section 4(f) and the associated guidance are in part the basis of this letter, and of FHWA’s determination and notification of *de minimis* impacts to the Glenwood Springs Historic Preservation Commission with respect to the proposed project. At this time we are notifying the Section 106 consulting parties per section 6009(b)(2)(C). On March 12, 2008, FHWA issued a Final Rule on Section 4(f), which clarifies and implements the procedures for determining a *de minimis* impact. In addition the Final Rule moves the Section 4(f) regulation to 23 CFR 774.

##### **Notification of Section 4(f) DeMinimis Determination**

The project has been determined to have *no adverse effect* to 5GF767, 5GF1050, 5GF1050.2, 5GF1050.3, 5GF2441, and 5GF1000.7. Based on the findings outlined above, FHWA may make a *de minimis* finding for the Section 4(f) requirements for these properties.

These materials have been sent to the State Historic Preservation Officer (SHPO) for review.

Ms. Ricehill  
August 5, 2013  
Page 8

As a Certified Local Government, we welcome your comments on this project. Should you choose to respond, we request your comments within 30 days of receipt of these materials. If we do not hear from you in this time frame, we will assume you do not plan to comment. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Historian, Lisa Schoch, at 303-512-4258.

If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

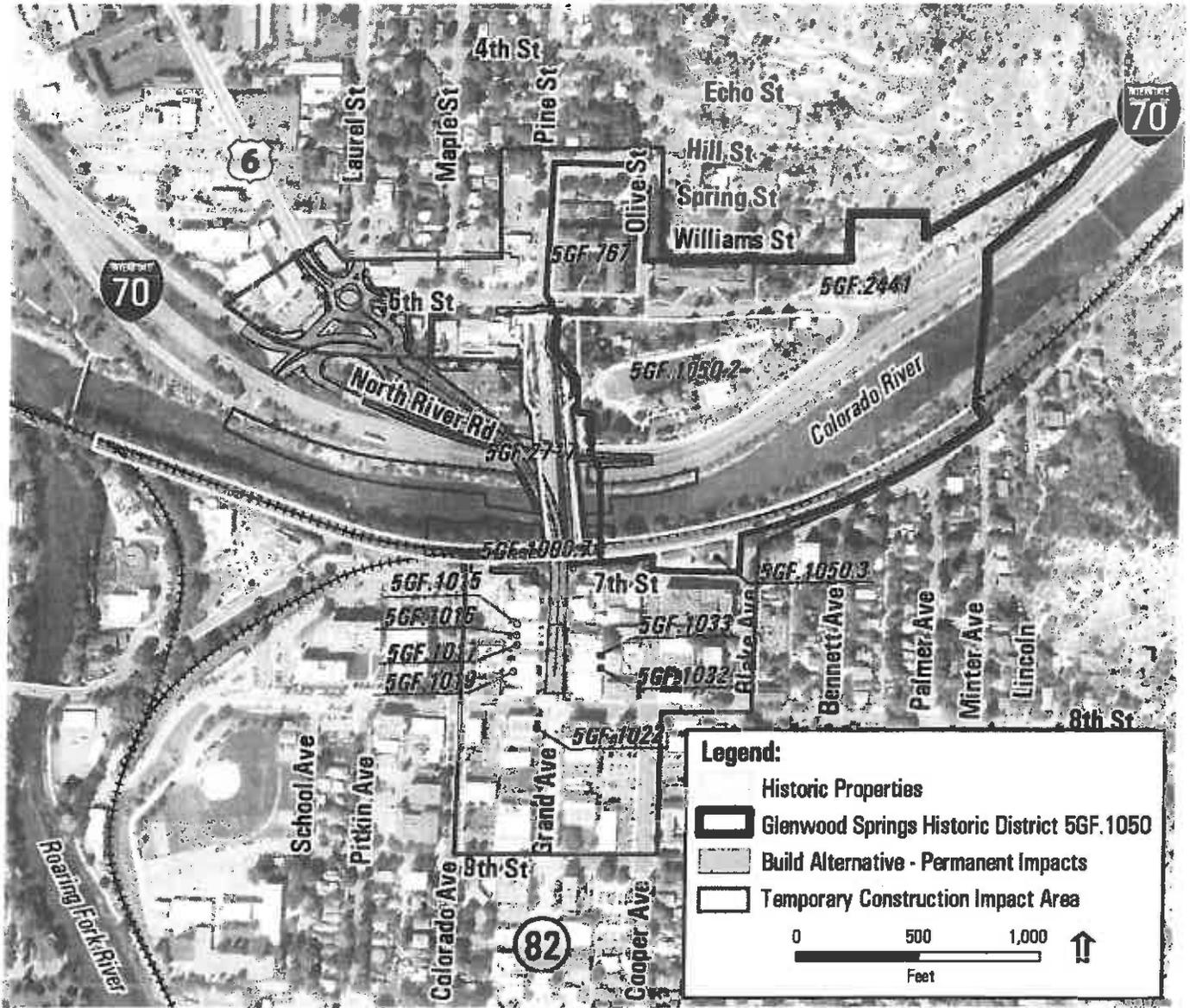
Very truly yours,


Jane Hann, Manager  
Environmental Programs Branch

Enclosures:      Survey Report  
                         Attachment 1

Figure 1: Build Alternative Impacts



**Figure 2: Existing Grand Avenue between 7<sup>th</sup> Street and 8<sup>th</sup> Street, Illustrates existing bridge/roadway width and distance from adjacent buildings**



**Figure 3: Build Alternative along Grand Avenue between 7<sup>th</sup> and 8<sup>th</sup> Streets, Illustrates Build Alternative and distance from adjacent buildings**



**Figure 4: Existing and Proposed Bridge Heights, Illustrates existing and proposed bridge height relative to buildings adjacent to Grand Avenue**

**Existing  
Grand  
Avenue at  
8<sup>th</sup> Street  
Looking  
North  
Toward  
River /  
Bridge**



**Build  
Alterna-  
tive from  
same view-  
point as  
above**



Figure 5: Proposed Scissor Ramp on East Side of Bridge

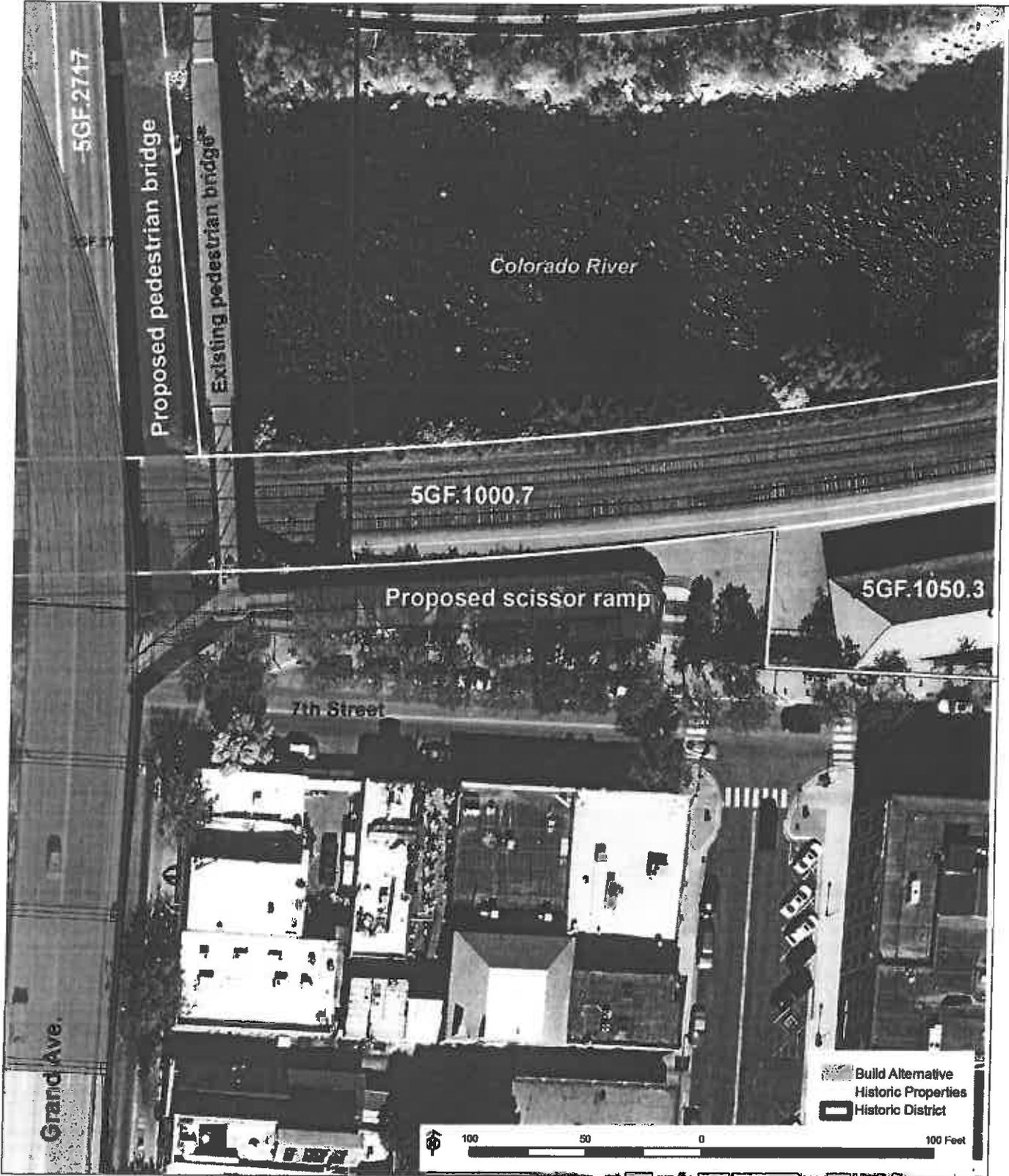
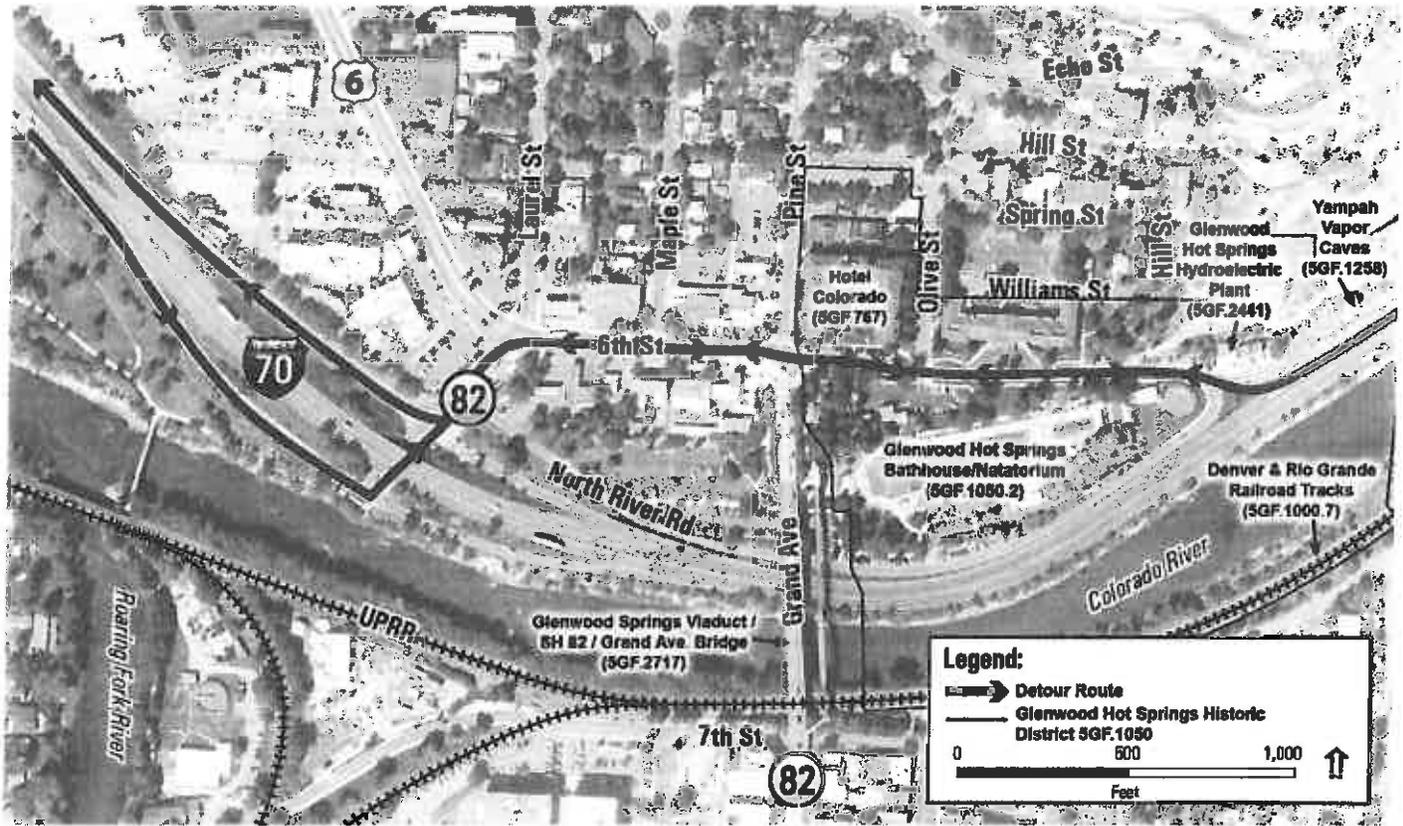


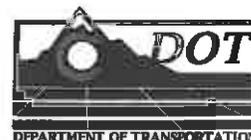
Figure 6: Temporary Detour near the Yampah Vapor Caves



# STATE OF COLORADO

## DEPARTMENT OF TRANSPORTATION

Environmental Programs Branch  
4201 East Arkansas Avenue  
Shumate Building  
Denver, Colorado 80222  
(303) 757-9281



August 9, 2013

Mr. John Martin  
Mr. Tom Jankovsky  
Board of County Commissioners  
108 8th Street, Suite 213  
Glenwood Springs, CO 81601

**SUBJECT:** Determinations of Eligibility and Effects and Notification of Section 4(f) *DeMinimis*,  
Project FBR 0821-094, Grand Avenue Bridge Replacement, Glenwood Springs

Dear Mr. Martin and Mr. Jankovsky

This letter and associated materials constitute a request for comments on Determinations of Eligibility and Effects for the project referenced above, as well as acknowledgement of Section 4(f) criteria. The City of Glenwood Springs and Colorado Department of Transportation (CDOT) propose to replace the State Highway 82/Grand Avenue bridge spanning the Colorado River. The project is being documented as an Environmental Assessment (EA) by the Federal Highway Administration (FHWA) and CDOT.

CDOT is submitting this to you in compliance with Section 106 of the National Historic Preservation Act, which requires federal agencies to evaluate the effects of their undertakings on historic properties. The Garfield Board of County Commissioners has been identified as a potential consulting party for this project. For more information about Section 106 and how you can participate as a consulting party, please visit the Advisory Council on Historic Preservation's web site, which contains the Citizen's Guide to Section 106 Review at <http://www.achp.gov/docs/CitizenGuide.pdf>.

### **PROJECT DESCRIPTION**

The Build Alternative would replace the SH 82/Grand Avenue bridge over the Colorado River with a five-span bridge with haunched or constant depth girders. The south end of the new bridge would start just north of the 8<sup>th</sup> Street/Grand Avenue intersection, continue on the existing alignment as it crosses 7<sup>th</sup> Street and the Union Pacific Railroad, curve west just past the railroad as it crosses the Colorado River, then touch down on the north side of the river near the 6<sup>th</sup> and Laurel Streets intersection, connecting to I-70 Exit 116 (Figure 1).

The existing pedestrian bridge located immediately east of the highway bridge would also be replaced. Three pedestrian bridge types are currently being considered: arch bridge, symmetric cable supported bridge, and asymmetric cable supported bridge. A staircase would lead pedestrians from the south side of the pedestrian bridge to downtown Glenwood Springs. To meet requirements of the Americans with Disability Act, the bicyclist/pedestrian connection on the south side would include either an elevator system and/or a scissor ramp connecting to 7<sup>th</sup> Street on the east side of the bridge (see Figure 5).

The Build Alternative would reconfigure the signalized intersection where the new highway bridge would touch down north of the river at 6<sup>th</sup> and Laurel Streets. A grade-separated pedestrian connection is proposed in a box culvert under the relocated intersection, connecting the sidewalks on the northeast and southwest sides. The sidewalk connection on the north side of the culvert comes out at grade and rises up

to 6<sup>th</sup> Street. The Build Alternative also includes several improvements to existing facilities adjacent to the alignment and bridges as part of the construction detour route that would be used during the approximately 30- to 60-day bridge closure. Although constructed for the detour, several of these improvements would be permanent because they would also provide long-term transportation benefits, while several changes would only be in place during the detour period. The permanent improvements would be minor surface improvements and would occur within the existing road right-of-way. Permanent improvements include the following: extending the I-70 westbound acceleration and deceleration lanes at Exit 116; extending the eastbound off-ramp deceleration lane at Exit 114; modifying the Exit 114 roundabouts to better accommodate large trucks; and restriping at several intersections along Midland Avenue and 27<sup>th</sup> St. The temporary changes include modifying the Midland/27<sup>th</sup> roundabout and removing traffic calming on Midland Avenue at 10<sup>th</sup> and 13<sup>th</sup> Streets. A temporary detour near the Yampah Vapor Caves is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 for bridge demolition/construction (see Figure 6 illustrating detour route). The detour would be built entirely within CDOT or City right-of-way. The temporary changes would be removed and the original features restored or reconstructed at the end of the project.

**AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) includes the parcels adjacent to 6<sup>th</sup> Street, Grand Avenue, and the area within the Hot Springs Historic District. The limits of the APE on each of these streets are as follows:

- **Grand Avenue** from 6<sup>th</sup> Street on the north to 9<sup>th</sup> Street on the south
- **6th St.** from North River Drive on the west to its end point at the Yampah Vapor Caves adjacent to I-70 on the east

Direct impacts from the project will affect some parcels on these streets. Indirect effects will include temporary construction impacts that are unlikely to extend past those properties that front along these streets. The APE includes the parcels that have structures built at least 45 years ago that are adjacent to these streets. For more information about the APE please refer to page 4 of the enclosed report.

**ELIGIBILITY DETERMINATIONS**

As summarized in the following table, a total of 37 properties were newly surveyed or documented on re-visitation forms. Survey was conducted between December 2011 and August 2012 by Gail Keeley of Hermsen Consultants. Site forms are included herewith along with the survey report. For more detailed information regarding eligibility determinations, please review pp. 21-34 of the report.

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
<b>6<sup>th</sup> St. (listed west to east)</b>			
5GF.4726	101 6 <sup>th</sup> St.	Subway	NE
5GF.4718	105 6 <sup>th</sup> St.	Kum & Go	NE
5GF.4721	106 6 <sup>th</sup> St.	Shell	NE
5GF.4722	112 6 <sup>th</sup> St.	Dairy Kreme / Glenwood Activities	NE
5GF.4723	114 6 <sup>th</sup> St.	Sioux Villa Curio	NE
5GF.4719	115 6 <sup>th</sup> St.	Eastwood Motor Company	NE
5GF.3298	205 6 <sup>th</sup> St.	Lincicome Auto / Mountain Sports Outlet	NE
5GF.4724	208-214 6 <sup>th</sup> St.; 631 Grand Ave.	Flower Mart / Gear Exchange / Liquors/ One Love / Bridge Bldg.	NE
5GF.4725	216 6 <sup>th</sup> St.	Polanka Polish Rest. / Natural Medicals	NE
5GF.767	526 Pine St.	Hotel Colorado	E; NRHP; CHD

**Surveyed Properties in the APE**

<b>ID Number</b>	<b>Address</b>	<b>Name / Description</b>	<b>Assessment</b>
5GF.1050	401 N. River St.	Glenwood Hot Springs Historic District	E
5GF.1050.2	401 N River St.	Hot Springs Bath House / Natatorium	OE; CHD
5GF.2441	601 6 <sup>th</sup> St.	Glenwood Springs Hydroelectric Plant	E; SRHP; NRHP; CHD
5GF.1258	709 6 <sup>th</sup> St.	Yampah Hot Springs Vapor Caves	NE; CHD
<b>Bridge and Railroad</b>			
5GF.2717	SH 82 Bridge over Colorado River	Glenwood Springs Viaduct / SH 82 / Grand Ave.	OE
5GF.1000.7	South edge of Colorado River	Denver & Rio Grande Railroad Tracks	E; CHD
5GF.1050.3	413 7 <sup>th</sup> St.	Denver & Rio Grande Railroad Station	E; CHD
<b>Grand Ave. (listed north to south)</b>			
5GF.4727	701 Grand Ave.	Rib City	NE
5GF.4731	702 Grand Ave.	Riviera Lounge / Peppo Nino Restaurant	NE
5GF.4732	710 Grand Ave.	Fins Grille and Raw Bar	NE
5GF.4728	711 Grand Ave.	Immortal Images Tattoo	NE
5GF.1035	714 Grand Ave.	Mirror Bar / Glenwood Shoe Service / King Barber Shop	NE
5GF.1015	715 Grand Ave.	Silver Club Bldg.	E
5GF.1034	716 Grand Ave.	Green Natural Solutions	NE
5GF.1016	717 Grand Ave.	Palace Hotel / Elizabeth Dean Boutique	E
5GF.1017	719 Grand Ave.	Parkison Bldg / The Candlestick Maker	E
5GF.4733	720 Grand Ave.	King Mall	NE
5GF.1018	721 Grand Ave.	Florindo's Italian Cuisine	NE
5GF.1033	722-724 Grand Ave.	Springs Restaurant / Doc Holliday Saloon	E
5GF.4729	723 Grand Ave.	The Book Train	NE
5GF.1019	725-727 Grand Ave.	Dougan Block / Sacred Grounds	E
5GF.1032	726 Grand Ave.	Ore Sample Room/ Narcissus Hair Studio	E
5GF.1020	729 Grand Ave.	Daily Bread Café and Bakery	NE
5GF.1021	731 Grand Ave.	Kamm-Dever Bldg.	ONE
5GF.4734	732 Grand Ave.	Bullock's Department Store / Summit Canyon Mountaineering	NE
5GF.1022	801 Grand Ave.	Citizens National Bank	E; NRHP
5GF.1023	805 Grand Ave.	Thorson Building	NE

E; NRHP = Eligible; listed on the NRHP  
 E; SRHP = Eligible; listed on the State Register of Historic Places  
 NE = Not Eligible for the NRHP  
 OE = Officially Eligible for the NRHP  
 ONE = Officially Not Eligible for the NRHP  
 CHD = Contributing Historic District

**EFFECTS DETERMINATIONS**

Fourteen eligible or listed properties and twenty-three non-eligible properties were identified. Detailed effects determinations are included for the fourteen eligible or listed properties; no effects determinations

are presented for the twenty-three properties determined not eligible. Because those properties were determined not eligible, the project results in a finding of *no historic properties affected*.

**Hotel Colorado (5GF767, 526 Pine Street):** There will be no direct impact to this property as a result of the Build Alternative. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west. A temporary detour near the Yampah Vapor Caves is planned to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70 (6). The detour, to be built entirely within CDOT or City right-of-way, would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near this property. The temporary and indirect impacts will not impair the ability of the property to convey the significance that made it eligible for inclusion on the NRHP. Therefore, CDOT has determined that the project results in *no adverse effect* to 5GF767.

**Glenwood Hot Springs Historic District (5GF1050):** There will be no direct or permanent impacts to any of the NRHP-eligible properties within the historic district. Temporary effects from construction activities anticipated include construction equipment operation, demolition of the existing road and pedestrian bridge piers; temporary falsework construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. A temporary detour near the Yampah Vapor Caves (5GF1258) (a contributing element to the district) is planned to allow I-70 traffic to bypass the bridge construction zone using 6<sup>th</sup> Street during periodic nighttime closures of I-70. The detour would be built entirely within CDOT or City right-of-way. The detour would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street within the Glenwood Hot Springs Historic District.

Views from the Hot Springs Pool toward the bridges would change with construction of the new highway and pedestrian bridges. The new pedestrian bridge would be similar in height to the existing bridge, but the bridge deck would be 16 feet wide (versus the existing width of between 10 and 14 feet), and its touchdown location on the north side of the river would be similar to that of the existing pedestrian bridge. Views of the highway bridge would be reduced because the new bridge would curve away to the west as it crosses the river. The new pedestrian bridge would continue to partially obscure views of the new highway bridge. As noted above, three bridge types are being considered for the pedestrian bridge. The super structures associated with these bridge types would be taller than the existing structure, making the presence of the pedestrian bridge more noticeable and visually prominent than the existing pedestrian bridge. The noise analysis conducted for this project indicates that the Build Alternative would result in lower noise levels at the Hot Springs than the No Action Alternative, as discussed below under the Glenwood Hot Springs Bathhouse/Natorium.

There will be temporary effects of increased dust, noise, and vibration during construction, nighttime traffic during periodic use of the 6<sup>th</sup> Street detour, and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics that qualify the district for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that the proposed plan will result in *no adverse effect*.

**Glenwood Hot Springs Bathhouse/Natorium (5GF1050.2, 401 North River Street):** There will be no direct impact to this property. According to the noise analysis, the existing noise level at this property is 67.4 dBA, which is 1.4 dBA above FHWA's Noise Abatement Criteria of 66.0 dBA for this land use type. The 2035 noise level is projected to be 69.3 dBA under the No Action Alternative and 66.2 dBA under the Build Alternative. Therefore, the Build Alternative would result in lower noise levels (by 3.1 dBA) at this property. Temporary effects from construction activities would include construction equipment operation, demolition of the pedestrian bridge and bridge piers; temporary falsework

construction; regrading and temporary shoring construction; construction of adjacent pedestrian bridge piers, abutments and retaining walls; and bridge deck construction. There also will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west and a new pedestrian bridge. The temporary detour near the Yampah Vapor Caves described above will also pass near this property, and would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street. The temporary and indirect impacts will not alter the characteristics that qualify 5GF1050.2 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Hydroelectric Plant (5GF2441, 601 6th St.):** There will be no direct impact to this property. Temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of removing the existing bridge and building a wider bridge that curves to the west are anticipated. The temporary detour near the Yampah Vapor Caves would result in temporary increases in nighttime traffic along 6<sup>th</sup> Street near the plant. The temporary and indirect impacts will not alter the characteristics that qualify 5GF2441 for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Glenwood Springs Viaduct/SH 82/Grand Avenue Bridge (5GF2717):** This eligible property will be removed and replaced with a new structure. CDOT has determined that this change will result in an *adverse effect*. A discussion regarding why the bridge is being replaced as well as a review of alternatives that avoid the bridge is included as Attachment 1. The information included in the attachment will be used in the Section 4(f) Evaluation for this resource.

**Denver & Rio Grande Railroad Tracks (5GF1000.7):** The new Grand Avenue bridge will be a maximum of approximately 62 feet wide, which is 22 feet wider than the existing structure. The railroad presently crosses under the bridge, which will not change other than the new bridge will be 1.0 to 1.5 ft. higher above the tracks and 22 feet wider than the existing bridge at the railroad crossing. There would be temporary effects within the railroad right-of-way (the historic boundary) during construction of the highway and pedestrian bridges, including construction equipment operation, demolition of the existing highway and pedestrian bridge and bridge piers, girder erection over the railroad, temporary falsework construction, regrading and temporary shoring construction, construction of adjacent bridge piers and retaining walls, and bridge deck construction over the railroad.

Additionally, a temporary grade crossing would be constructed to provide construction access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. The crossing would be built of railroad approved materials consisting of aggregate, wood, and pavement. The crossing would be located adjacent to the temporary construction impact area at a specific location to be determined in consultation with the railroad. It is anticipated that the crossing would be used at pre-scheduled times during a majority of the construction period, specifically to construct and remove cofferdams, remove existing bridge materials and construct the new bridge. Upon construction completion, the temporary rail grade crossing would be removed and the railroad right-of-way returned to its current condition and appearance. The proposed change to the width of the overhead bridge only affects a small portion of the overall resource and will not change the characteristics that make the D&RG eligible to the NRHP. The railroad will still be able to convey its significance and continue operations with the slightly wider bridge crossing overhead. Therefore, CDOT has determined that this wider crossing will result in *no adverse effect*.

**Denver & Rio Grande Railroad Station (5GF1050.3, 413 7th St.):** There will be no direct impact to this property. A scissor ramp would be built on the east side of the new pedestrian bridge to provide a

connection from the pedestrian bridge to 7<sup>th</sup> Street (Figure 5), replacing the existing stair access and kiosk and resulting in minor visual changes from the station. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. The temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no adverse effect*.

**Silver Club Building (5GF1015, 715 Grand Ave.):** The proposed bridge will be between 8.5 and 12.5 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 21 ft. from the west edge of the bridge. The profile of the new structure will be approximately 3-4 ft. higher in front of the building than the existing configuration. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Palace Hotel (5GF1016, 717 Grand Ave.):** The proposed bridge will be an average of 11 ft. closer to this building. Currently it is about 30 ft. from the edge of the bridge, whereas it will be an average of 19 ft. from the west edge of the new bridge. The profile of the new bridge will be approximately 3-4 ft. higher in front of this building than the existing bridge. Figures 2-4 illustrate existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Parkison Building (5GF1017, 719 Grand Ave.):** The proposed bridge will be 12 ft. closer to this building. Currently the building is about 30 ft. from the edge of the bridge; after construction it will be 18 ft. from the west edge of the bridge. The profile of the new bridge will be approximately 3-4 feet higher in front of the building than the existing bridge. Figures 2-4 show an illustration of existing and proposed bridge/roadway height and width. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Springs Restaurant/Doc Holliday Tavern (5GF1033, 722-724 Grand Ave.):** With construction of the proposed bridge, Grand Ave. will be 7 ft. closer to this building, which is presently about 25.5 feet from the edge of Grand Ave.; after the bridge is built, that distance will be 18 ft. The profile of the new bridge will be approximately two feet higher in front of this building than the existing bridge (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Dougan Block / Sacred Grounds Coffeeshouse/Dancing Bear Trading Post (5GF1019, 725-727 Grand Ave.):** With construction of the proposed bridge, Grand Avenue will be about 2 ft. closer to this building; the existing bridge lands just to the north. Grand Avenue is at-grade with the curb line about 22 ft. from the middle of the building. The new bridge approach will be on retaining walls in front of the building, with the west curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 feet higher in front of this building than the existing roadway, and a 1.5-2-foot bridge rail will be added due to the elevation change; therefore the building front will be approximately 18 feet from the back of the bridge rail (Figures 2-4). According to the noise analysis conducted for the project, the existing noise level at this property is 70.2 dBA, which is 0.8 dBA below FHWA's Noise Abatement Criteria of 71.0 dBA for this land use type. The 2035 noise levels at the property would be 72.3 dBA under the No Action Alternative and 72.8 dBA under the Build Alternative. Both alternatives would exceed the NAC; however, this slight increase would be imperceptible to the human ear. CDOT has determined that these changes will result in an *adverse effect* due to changes in the setting.

**Ore Sampling Room/Narcissus Hair Studio (5GF1032, 726 Grand Ave.):** With construction of the proposed bridge, Grand Avenue traffic will be about 2 ft. closer to this building. The existing bridge

lands north of the building and Grand Avenue is at-grade, with the lanes about 22 feet away. The existing curb is about 8 feet. away, and there is on-street parking and an access lane for Wing Street in front of the building. After the bridge is constructed, the Wing Street access and parking will be removed and the bridge approach will be on retaining walls in front of the building, with the east curb line about 20 ft. away. The profile of the new bridge will be approximately 1.5 ft. higher in front of the building than the existing roadway, and a 1.5-2 ft. bridge rail will be added due to the elevation change so the building front will be approximately 18 ft. from the back of the bridge rail (Figures 2-4). CDOT has determined that these changes will result in an *adverse effect* to 5GF1032.

**Citizens National Bank Building (5GF1022, 801 Grand Ave.):** There will be no direct impact to this property. The new bridge will touch down approximately one-half block to the north of the building. There will be temporary effects of increased dust, noise and vibration during construction and indirect effects from the visual impacts of a wider bridge structure. However, the temporary and indirect impacts will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. Therefore, CDOT has determined that this change will result in *no historic properties affected*.

## SECTION 4(F) AND DE MINIMIS

### Background

In addition to Section 106 of the NHPA, FHWA must comply with Section 4(f), which is codified at both 49 U.S.C § 303 and 23 U.S.C. § 138. Congress amended Section 4(f) when it enacted the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (Public Law 109-59, enacted August 10, 2005) (“SAFETEA-LU”). Section 6009 of SAFETEA-LU added a new subsection to Section 4(f), which authorizes FHWA to approve a project that uses Section 4(f) lands that are part of a historic property without preparation of an Avoidance Analysis, if it makes a finding that such uses would have “*de minimis*” impacts upon the Section 4(f) resource, with the concurrence of the SHPO.

On December 12, 2005, the Federal Highway Administration issued its “Guidance for Determining *De Minimis* Impacts to Section 4(f) Resources” which indicates that a finding of *de minimis* can be made when the Section 106 process results in a *no adverse effect* or *no historic properties affected* determination, when the SHPO is informed of the FHWA’s intent to make a *de minimis* impact finding based on their written concurrence in the Section 106 determination, and when FHWA has considered the views of any Section 106 consulting parties participating in the Section 106 process. This new provision of Section 4(f) and the associated guidance are in part the basis of this letter, and of FHWA’s determination and notification of *de minimis* impacts to the Garfield County Board of County Commissioners with respect to the proposed project. At this time we are notifying the Section 106 consulting parties per section 6009(b)(2)(C). On March 12, 2008, FHWA issued a Final Rule on Section 4(f), which clarifies and implements the procedures for determining a *de minimis* impact. In addition the Final Rule moves the Section 4(f) regulation to 23 CFR 774.

### Notification of Section 4(f) DeMinimis Determination

The project has been determined to have *no adverse effect* to 5GF767, 5GF1050, 5GF1050.2, 5GF1050.3, 5GF2441, and 5GF1000.7. Based on the findings outlined above, FHWA may make a *de minimis* finding for the Section 4(f) requirements for these properties.

These materials have been sent to the State Historic Preservation Officer (SHPO) for review.

Mr. Martin, Mr. Jankovsky  
August 9, 2013  
Page 8

As a county government, we welcome your comments on this project. Should you choose to respond, we request your comments within 30 days of receipt of these materials. If we do not hear from you in this time frame, we will assume you do not plan to comment. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Historian, Lisa Schoch, at 303-512-4258.

If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

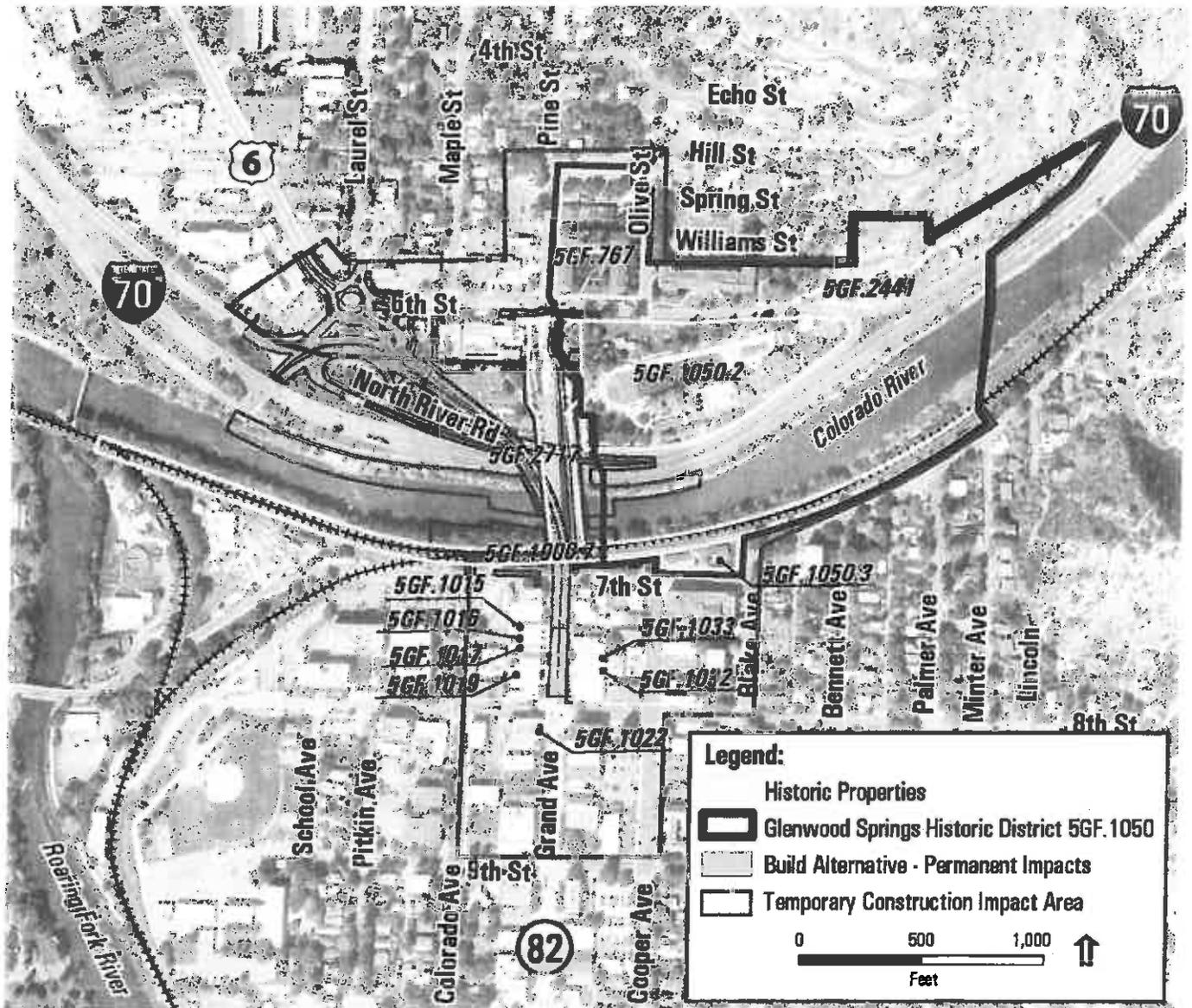
Very truly yours,



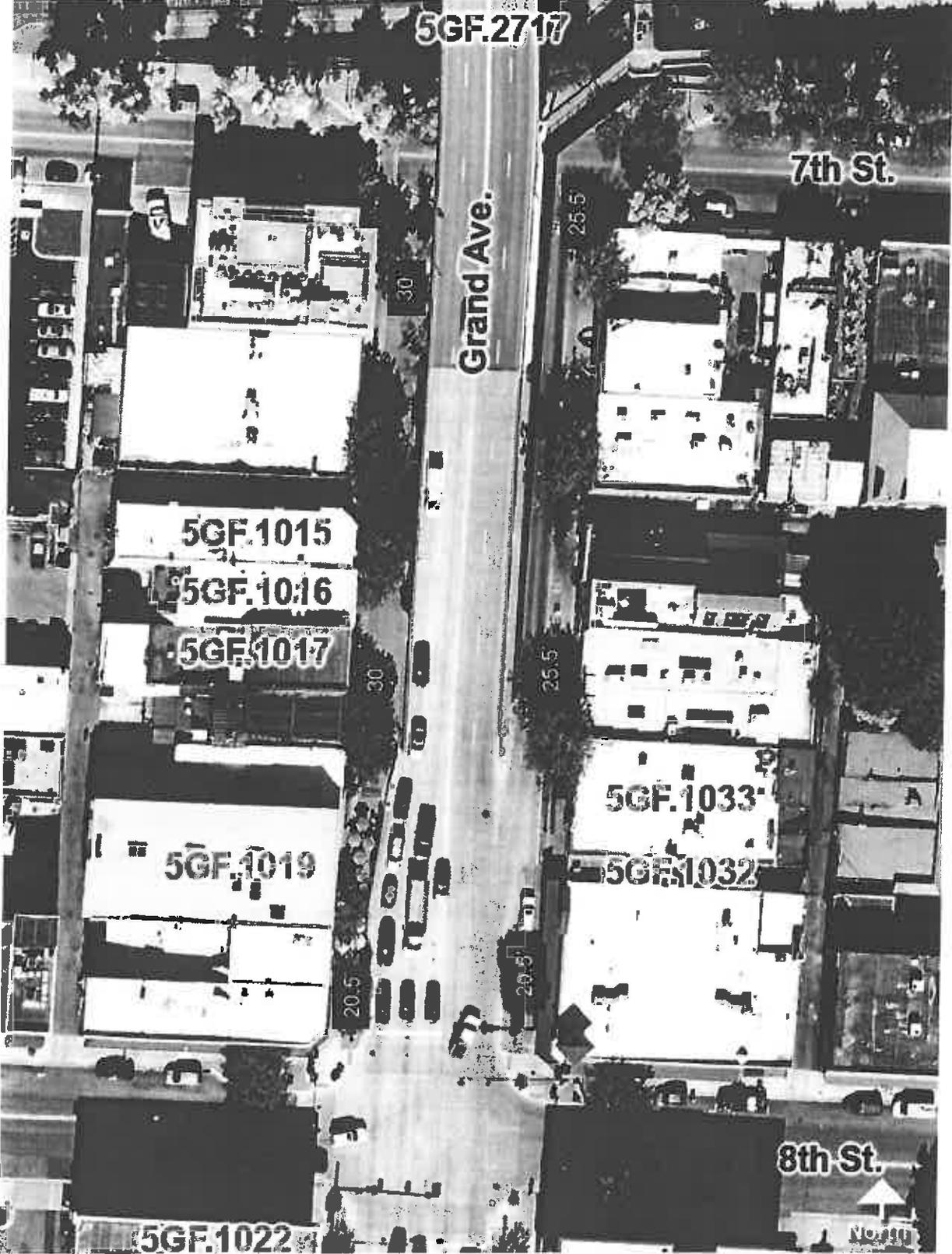
*For*  
Jane Hann, Manager  
Environmental Programs Branch

Enclosures:      Survey Report  
                         Attachment 1

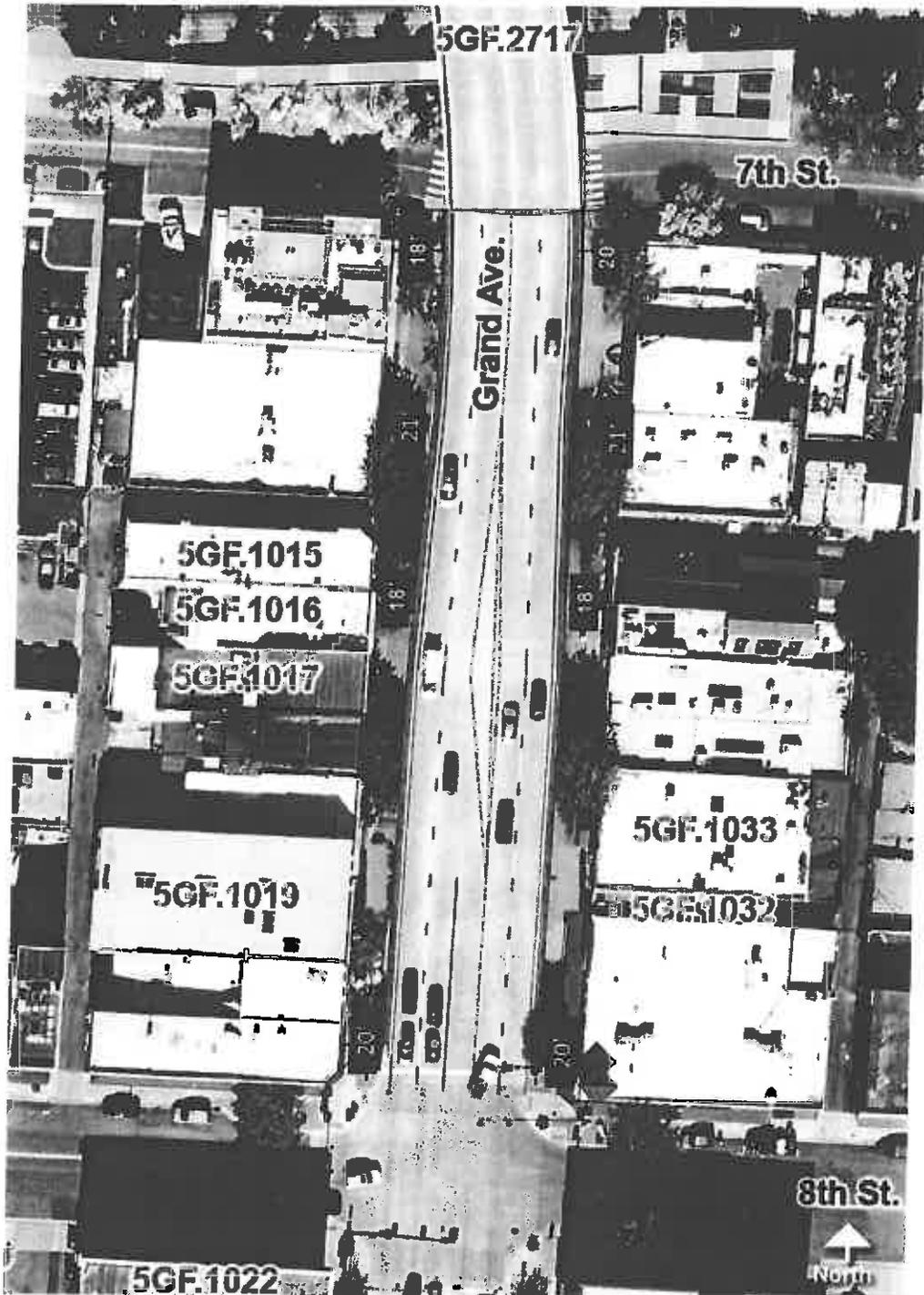
Figure 1: Build Alternative Impacts



**Figure 2: Existing Grand Avenue between 7<sup>th</sup> Street and 8<sup>th</sup> Street, Illustrates existing bridge/roadway width and distance from adjacent buildings**



**Figure 3: Build Alternative along Grand Avenue between 7<sup>th</sup> and 8<sup>th</sup> Streets, Illustrates Build Alternative and distance from adjacent buildings**



**Figure 4: Existing and Proposed Bridge Heights, Illustrates existing and proposed bridge height relative to buildings adjacent to Grand Avenue**

**Existing  
Grand  
Avenue at  
8<sup>th</sup> Street  
Looking  
North  
Toward  
River /  
Bridge**



**Build  
Alterna-  
tive from  
same view-  
point as  
above**



Figure 5: Proposed Scissor Ramp on East Side of Bridge

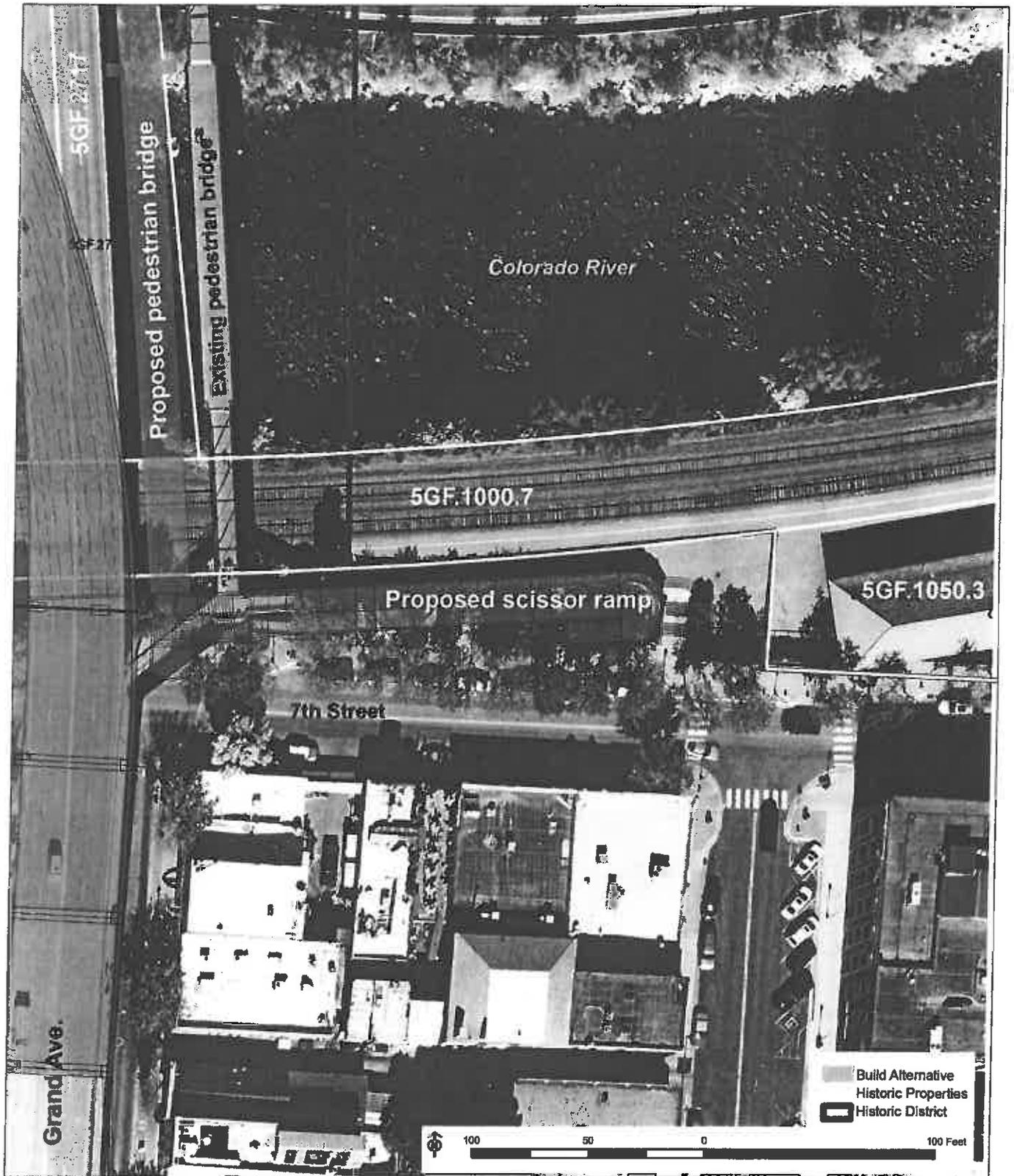
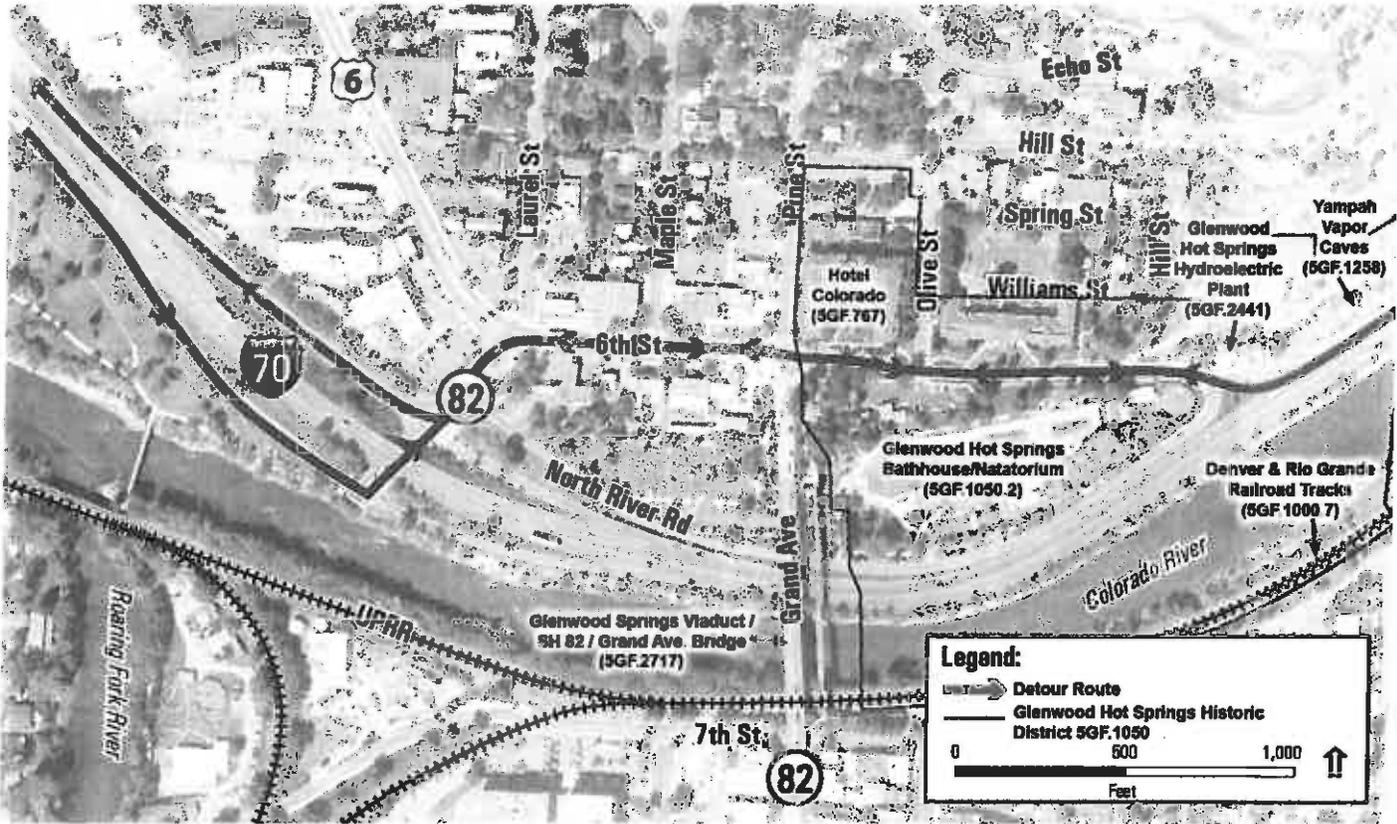


Figure 6: Temporary Detour near the Yampah Vapor Caves





August 14, 2013

Jane Hann  
Manager, Environmental Programs Branch  
Colorado Department of Transportation  
Environmental Programs Branch  
4201 East Arkansas Avenue  
Denver, CO 80222

Re: Determinations of Eligibility and Effects, Notification of Section 4(f) DeMinimis and Section 4(f) Exception for Temporary Occupancy Criteria, Project FBR 0821-094, Grand Avenue Bridge Replacement, Glenwood Springs, CO (CHS #60723)

Dear Ms. Hann,

Thank you for your correspondence dated and received on August 2, 2013 by our office regarding the review of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106).

After review of the provided information, we do not object to the proposed Area of Potential Effects (APE) for the project. After review of the provided survey information, we concur with the recommended finding of **not eligible** for the National Register of Historic Places for the properties listed below.

- 5GF.4726
- 5GF.4718
- 5GF.4721
- 5GF.4722
- 5GF.4723
- 5GF.4719
- 5GF.3298
- 5GF.4724
- 5GF.4725
- 5GF.4727
- 5GF.4731
- 5GF.4732
- 5GF.4728
- 5GF.1035
- 5GF.1034
- 5GF.4733
- 5GF.1018
- 5GF.4729
- 5GF.1020
- 5GF.1021
- 5GF.4734
- 5GF.1023

After review of the provided survey information, we concur with the recommended finding of **eligible** for the National Register of Historic Places for the properties listed below.

- 5GF.767
- 5GF.1050
- 5GF.1050.2
- 5GF.1050.3
- 5GF.1000.7
- 5GF.2441
- 5GF.1258

- 5GF.2717 Note: The survey form for 5GF.1050 recommends the removal of this resource and the adjacent pedestrian bridge from the Glenwood Hotel Springs Historic District. This form confirms that the Grand Ave Bridge is individually eligible, but does not comment in regards to the National Register eligibility of the pedestrian bridge. Was the pedestrian bridge evaluated for National Register eligibility?
- 5GF.1015
- 5GF.1016
- 5GF.1017
- 5GF.1019
- 5GF.1033
- 5GF.1032
- 5GF.1022

After review of the scope of work and assessment of adverse effect, we concur with the recommended findings of *no historic properties affected* [36 CFR 800.4(d)(1)] under Section 106 for the properties listed above as not eligible for the National Register of Historic Places and resource 5GF.1022.

We concur with the recommended finding of *no adverse effect* [36 CFR 800.5(b)] under Section 106 for the properties listed below.

- 5GF.767
- 5GF.1050
- 5GF.1050.2
- 5GF.2441
- 5GF.1000.7
- 5GF.1050.3

We concur with the recommended finding of *adverse effect* [36 CFR 800.5(a)(1)] under Section 106 for properties listed below. We have reviewed Attachment 1 and understand that measures to avoid or minimize the adverse effects were evaluated.

- 5GF.2717
- 5GF.1015
- 5GF.1016
- 5GF.1017
- 5GF.1033
- 5GF.1019
- 5GF.1032

In regards to the Section 4(f) application, we acknowledge that FHWA and CDOT may make a *de minimis* determination in respect to the requirements of Section 4(f). We also acknowledge that FHWA and CDOT may make a determination of Section 4(f) Temporary Occupancy Exception.

If unidentified archaeological resources are discovered during construction, work must be interrupted until the resources have been evaluated in terms of the National Register criteria, 36 CFR 60.4, in consultation with this office.

We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings.

Please note that our compliance letter does not end the 30-day review period provided to other consulting parties.

If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Manager, at (303) 866-4678.

Sincerely,

A handwritten signature in blue ink, appearing to read "Ed Nichols". The signature is written in a cursive style with a horizontal line extending from the end.

Edward C. Nichols  
State Historic Preservation Officer



August 15, 2013

Ms. Jane Hann, Manager  
Environmental Programs Branch  
Colorado Department of Transportation  
4201 E Arkansas Avenue  
Denver, Colorado 80222

RE: Section 106, National Historic Preservation Act - Determinations of Eligibility and Effects - Project FBR 0821-094 – Grand Avenue Bridge Replacement – Glenwood Springs

Dear Ms. Hann:

Thank you for submitting the Historic Resources Survey Report and letter regarding CDOT's determinations of eligibility and effects for the Grand Avenue bridge replacement project. I have reviewed the letter and supporting materials and offer the following comments on behalf of the Glenwood Springs Historic Preservation Commission:

1. The Historic Resources Survey Report includes Maps that show the Area of Potential Effect (APE) extending to 9<sup>th</sup> Street yet there is no survey documentation for any of the buildings on the east side of Grand Avenue from 8<sup>th</sup> to 9<sup>th</sup> Streets, and survey documentation for only two of the eight properties (nine buildings) on the west side of the 800 block of Grand Avenue. Since the 800 block of Grand Avenue is included in the APE, should all of the buildings have been surveyed and assessed for potential impact from this project? Additionally, there is no survey documentation for five buildings in the 200 and 300 blocks of 7<sup>th</sup> Street and one building on west 6<sup>th</sup> Street. These blocks were also included in the APE.
2. Where CDOT determined that the project would result in *no adverse effect* to listed and eligible buildings, was consideration given to the potential seismic impacts to structures in the vicinity of demolition and construction activities? We are especially concerned with any potential adverse physical impacts to the Denver & Rio Grande Railroad Station (5GF.1050.3), to the Hot Springs Bath House (5GF.1050.2), the Hotel Colorado (5GF.767), and the eligible commercial buildings on either side of Grand Avenue from 7<sup>th</sup> to 8<sup>th</sup> Streets (5GF – .1015, .1016, .1017, .1019, .1033 and .1032).
3. We understand that there is ongoing discussion of constructing either an elevator and/or a scissor ramp to provide an accessible route to the pedestrian bridge. CDOT found that construction of the scissor ramp would result in minor visual changes from the train station (page 6) but did not provide a determination of the potential visual

*City of Glenwood Springs Community Development Department  
101 W 8<sup>th</sup> Street*

*Glenwood Springs, Colorado 81601  
Tel: 970-384-6400  
FAX: 970-945-8582*

impact if an elevator was constructed. Additionally, should CDOT note the proximity of the scissor ramp's east wall to the west side of the station, and note the height of both the ramp and elevator shaft. Finally, should CDOT note the elevator location in figure 5, page 13?

4. Regarding the scissor ramp, we understand that due to the substantial grade change between 7<sup>th</sup> Street and to pedestrian bridge deck, the ramp may quite long. Should CDOT include an assessment of visual impacts of this ramp on potentially historic buildings fronting 7<sup>th</sup> Street (see also comment #1 regarding need for a survey of five 7<sup>th</sup> Street buildings).
5. We understand that a pedestrian bridge design has not been determined but that three basic designs are under consideration. It has been reported that the symmetrical or asymmetrical cable tower may be as high as 100 feet. At this point we do not agree with CDOT's determination of no direct impact to the Denver & Rio Grande Train Station (5GF.1050.3). Before making a "no adverse effect" determination which results in a *de minimis* impact finding, we believe that consideration needs to be given to the potential visual impact that a 100-foot tall tower might have on the views to the train station when looking from a southeast vantage point.
6. Figure 1, page 9 – please identify the red border in the legend as the APE.
7. Page 7- CDOT determined that the temporary and indirect impacts of the bridge project will not affect the Citizens National Bank building, a property that is listed on the National Register of Historic Places. This is a minor point but CDOT's determination was "no historic properties affected". Should the determination be "no adverse effect" since this is a historic property?

Attached is an address-by-address summary of the Historic Preservation Commission's comments about CDOT's findings regarding impacts to historic and eligible properties. This includes a listing of those properties that were not surveyed but that were included in the APE.

Please let me know if you have questions about these comments or need additional information. You can reach me at 970-384-6428.

Sincerely,



Gretchen E. Ricehill

Senior Planner

Community Development Dept.

*City of Glenwood Springs Community Development Department  
101 W 8<sup>th</sup> Street  
Glenwood Springs, Colorado 81601  
Tel: 970-384-6400  
FAX: 970-945-8582*

<b>ID Number</b>	<b>Address</b>	<b>Name</b>	<b>CDOT Determination</b>	<b>Glenwood Springs HPC Comment</b>
5GF.767	526 Pine	Hotel Colorado	No adverse effect	Consider impact of demolition/construction vibration
5GF.1050	Hot Springs historic district	Hot Springs historic district	No adverse effect	Agree with exceptions noted under 5GF.1050.2, 5GF.767
5GF.1050.2	401 N. River	Bathhouse & Natatorium	No adverse effect	Consider impact of demolition/construction vibration
5GF.2441	601 6 <sup>th</sup> St	Hydroelectric Plant	No adverse effect	Agree
5GF.1258	709 6 <sup>th</sup> St	Yampah Hot Springs Vapor Caves (as part of historic district)	No adverse effect	Agree
5GF.1000.7	South edge of Colorado River	D&RG / UP tracks	No adverse effect	Agree
5GF.1050.3	413 7 <sup>th</sup> St	D&RG train station	No adverse effect	Disagree. See comment #5, page 2 of this letter
5GF.1015	715 Grand Ave	Silver Club	No adverse effect	Consider impact of demolition/construction vibration
5GF.1016	717 Grand Ave	Palace Hotel	No adverse effect	Consider impact of demolition/construction vibration
5GF.1017	719 Grand Ave	Parkison Bldg	No adverse effect	Consider impact of demolition/construction vibration
5GF.1033	722-24 Grand Ave	Springs/Doc Holliday Saloon	No adverse effect	Consider impact of demolition/construction vibration
5GF.1019	725-27 Grand Ave	Dougan Block	No adverse effect	Consider impact of demolition/construction vibration
5GF.1032	726 Grand Ave	Ore Sample Room	No adverse effect	Consider impact of demolition/construction vibration
5GF.1022	801 Grand Ave	Citizens Bank	No adverse effect	Consider impact of demolition/construction vibration

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*Glenwood Springs, Colorado 81601*  
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Buildings within APE but not surveyed				
ID Number	Address	Name/Business		
	208 7 <sup>th</sup> St	(vacant)		
	210 7 <sup>th</sup> St	Ross Bail Bonds		
	308-10 7 <sup>th</sup>	Juicy Lucy's Steakhouse		Property consists of two buildings
5GF.2440	312 & 330 7 <sup>th</sup>	Odeon Theater/ FOE/Pullman Restaurant		Surveyed 1997
	802 Grand Ave	CMC/US Bank		Consists of 2 buildings
	809 Grand Ave			
5GF.1031	812/14 Grand Ave	Napier Bldg/JC Penney		Surveyed 1981
	813-17 Grand Ave			
	816 Grand Ave			
	818 Grand Ave			
	819-821 Grand Ave			
5GF.2786	820 Grand Ave	Glenwood Springs Furniture & Wallpaper Co. Bldg		Surveyed 1981
5GF.1030	822 Grand Ave	JC Schwartz- Howard & Torrey Bldg		Surveyed 1998
	823 Grand Ave	Melby Bldg		New construction
	825 Grand Ave	IOOF Bldg/Glenwood Drug		
	824-26 Grand Ave			
5GF.1028	828 Grand Ave	US Land Office		Surveyed 1981
5GF.1025	831 Grand Ave	Coal Office Bldg		Surveyed 1981; appears to consists of 2 buildings
	102 W 6 <sup>th</sup> St	Village Inn		Constructed 1975

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**COLORADO**

**Department of Transportation**

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

March 28, 2014

Mr. Edward Nichols  
State Historic Preservation Officer  
Colorado Historical Society  
1200 Broadway  
Denver, CO 80203

**SUBJECT:** Additional Information on Determinations of Eligibility and Effect, and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception, Project FBR 0821-094, SH 82/Grand Ave. Bridge Replacement EA, Glenwood Springs (CHS #60723)

Dear Mr. Nichols:

You initially reviewed eligibility and effect determinations for this project in August 2013. Since that time the scope of the project has changed to include an extension of 8<sup>th</sup> Street to the existing 8<sup>th</sup> Street bridge over the Roaring Fork River to provide a temporary traffic detour during the approximate 60-day period when the Grand Avenue bridge would be fully closed to traffic during construction. Construction of the 8<sup>th</sup> Street detour would temporarily remove via excavation portions of four railroad tracks that currently traverse the area where 8<sup>th</sup> Street would be extended. See the attached graphic entitled "Proposed 8<sup>th</sup> Street Construction Detour" for more information.

This submittal includes the following information:

1. Response regarding eligibility of the pedestrian bridge adjacent to Grand Avenue Bridge
2. Revised Area of Potential Effect (APE)
3. New Eligibility and Effect Determinations for the Denver & Rio Grande Railroad Aspen Branch (5GF1661) and the associated freight depot (5GF5021).
4. Easements and aerial easements for the project
5. Design changes at the proposed pedestrian bridge
6. Application of Section 4(f)

#### **PEDESTRIAN BRIDGE**

In your August 14, 2013 response letter, you requested additional information regarding the eligibility of the pedestrian bridge adjacent to the existing Grand Avenue Bridge. The pedestrian bridge was built in 1985 and was not evaluated for National Register eligibility because it does not meet the minimum 50-year age threshold for evaluation and is unlikely to be significant under Criteria Consideration G, for properties that have become significant within the past 50 years.

#### **REVISED AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) has been expanded to include an additional area on 8<sup>th</sup> Street, and includes entire parcels adjacent to 8<sup>th</sup> Street. A map reflecting the revised APE is attached.



The direct impacts from building the temporary detour would affect four to six parcels along 8<sup>th</sup> Street and nearby streets, with only one historic feature affected. Indirect effects would include temporary construction impacts such as noise and dust, which are not anticipated to extend past those properties that front the streets. The APE includes the parcels that have structures that were built at least 45 years ago adjacent to these streets, as shown on the attached APE map.

#### **ELIGIBILITY DETERMINATIONS**

The proposed 8<sup>th</sup> Street detour results in effects to an additional property that was not evaluated in our initial survey: a segment of the Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7) and its associated freight depot (5GF5021). Site forms for these properties are attached, as is a historic context for railroad development in the Glenwood Springs/Aspen area, which was prepared because that topic was not specifically addressed in the initial Historic Resources Survey Report.

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** This railroad line was built in 1887 by the Denver & Rio Grande Railroad, and was the first rail line to reach the lucrative silver mining operations in Aspen. The entire Aspen Branch of the D&RG was determined eligible for inclusion on the National Register of Historic Places (NRHP) in 1988. In 2005, the 41-mile resource was determined to retain its eligibility even though the rails and railbed of much of the entire Aspen Branch line was removed for conversion to a recreational trail. The Aspen Branch of the Denver & Rio Grande Railroad is significant under Criterion A for its key role in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and for its association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The existence of portions of the tracks, ties, and grade in this segment supports the eligibility of the entire resource.

**5GF5021, Freight Depot:** The freight depot is significant for its association with the D&RG Aspen Branch. The depot was built by the D&RG to support its rail operations and is eligible under Criterion A for the reasons stated above for the railroad. The original freight depot on this site was removed and replaced with the current structure after 1947. That depot has been further modified over the years and is not eligible under Criterion C. The freight depot was documented separately but is an associated feature of the overall railroad, and is included in the boundary for the railroad.

#### **EFFECT DETERMINATIONS**

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour. The detour route for regional traffic begins at Exit 114 on I-70 and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River, then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary “square about” for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad that currently extends between the two sections of 8<sup>th</sup> Street.

Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific Railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train.



This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for two to four years.

To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. This would require temporary removal and storage of approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements, noted as 13 and 15 in the attached table of easement acquisitions, will be required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:

- West Wye Track – approximately 150 feet
- West Track of East Wye – approximately 165 feet
- Middle Track of East Wye – approximately 175 feet
- East Track of East Wye – approximately 175 feet

There are two options for the restoration of the railroad connection:

#### **Option 1**

The City of Glenwood Springs approached the CDOT study team about entering into a partnership regarding its planned 8<sup>th</sup> Street Extension project. If the City gains approval and funding, their project would involve building a new bridge at the railroad crossing that would restore the railroad connection. In anticipation of the City's project, CDOT would leave the 8<sup>th</sup> Street detour in place.

8th Street in downtown Glenwood Springs currently terminates just west of School Street. The City's 8th Street project would connect the 8th Street Bridge over the Roaring Fork River along a new alignment under the UPRR railroad tracks. This would involve lowering 8th Street between its intersection at Pitkin Avenue and the 8th Street Bridge, a distance of approximately 900 feet. The City has developed some alternatives for their 8<sup>th</sup> Street Extension project but has not selected an option, so it is unclear how the railroad connection will be restored. If the City's project moves forward, CDOT will disconnect this railroad segment (5GF1661.7) and not restore it as part of this undertaking. Under this option, the project results in an *adverse effect* to the overall Denver & Ro Grande—Aspen Branch because the railroad connection will not be restored and the integrity of the segment in the project area would be diminished. Although the freight depot (5GF5021) will not be directly affected by disconnection of the overall railroad, construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks farther to the west. These grading activities would not directly affect the freight depot. However, because the depot is considered part of that property, it is included in the overall adverse effect to the railroad.

CDOT proposes Level II documentation and interpretative mitigation to resolve the adverse effect for this resource. CDOT will continue to coordinate with your office during the development of both the CDOT and City projects. Assuming Option 1 is implemented, CDOT plans to proceed with consultation to resolve the adverse effect to the railroad, including notification of the Advisory Council on Historic Preservation and development of mitigation through a Memorandum of Agreement (MOA).



## Option 2

If the City's project is *not* funded, CDOT will restore the railroad connection. The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition are anticipated to last between four and five months.

After completion of the project, the railbed will be restored and the overall railroad would still portray the key role it played in the settlement and commercial growth of Glenwood Springs and Colorado, as well as its association with settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor—all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to 5GF1661.7 under this option.

**5GF5021, Freight Depot:** No direct impacts would occur to the freight depot. Construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks further to the west, but that would have no direct effect to the depot. Construction and operation of the detour, and returning that route to preconstruction conditions is anticipated to last four to five months. Temporary effects would include dust and noise from construction activities. Because no direct impacts would occur, the freight depot would still convey its association with the D&RG. For this reason, the proposed project would result in *no adverse effect* to 5GF5021.

## TEMPORARY AND PERMANENT AERIAL EASEMENTS/UPDATED EFFECTS DETERMINATIONS

Since we previously consulted, more information is available about the types of easements needed for the project. Attached is a table and map showing the locations of proposed temporary easements and a permanent aerial easement. Most of the parcels requiring easements are non-historic resources, including properties 1, 2, 3, 4, 6, 7 and 16. The other properties are evaluated below:

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August submittal; the entire railroad is eligible and the segment retains integrity. CDOT will acquire one permanent aerial easement totaling about 0.01 acre from the railroad to widen the Grand Avenue Bridge. The aerial easement is noted on the attached graphic as a small dark blue sliver west of the existing bridge, south of the river, and located between the yellow build alternative and the cross-hatched orange shading for a temporary easement. This easement would not affect property access, signage, or the property owner's current use of the property. CDOT will also acquire four temporary easement locations from the railroad, noted as 11 and 12 on the attached table and graphic. These four easements are combined so 11 equals 0.15 acre and 12 is 0.04 acre. These easements will be used for construction of a temporary grade crossing as noted in our August submittal. The crossing will be constructed to provide access to the north side of the railroad for demolition of the existing bridge and construction of the new



bridge. We consulted on this previously and you concurred with the finding of *no adverse effect*. The temporary grade crossing and associated easements will not alter the qualities of significance of the railroad and the finding of no adverse effect is still appropriate. Note that easements 9 and 10 are on railroad property, but are not within the boundary of the historic railroad.

**Glenwood Hot Springs Historic District (5GF1050):** The project will require a temporary easement totaling 0.14 acres on the legal hot springs property but this is outside the boundary of the historic hot springs district. This easement is noted as 8 on the attached table and graphic.

#### **DESIGN CHANGE/UPDATED EFFECT DETERMINATIONS**

**Denver & Rio Grande Railroad Station (5GF1050.3):** When we consulted in August 2013, elevator and/or scissor ramp configurations were being considered for the new pedestrian bridge access. Since that time, the design has been modified; a scissor ramp is no longer being considered and dual elevators are now proposed. The elevators would be located at the south end of the pedestrian bridge, with the stairway located on the east side of the elevator bank.

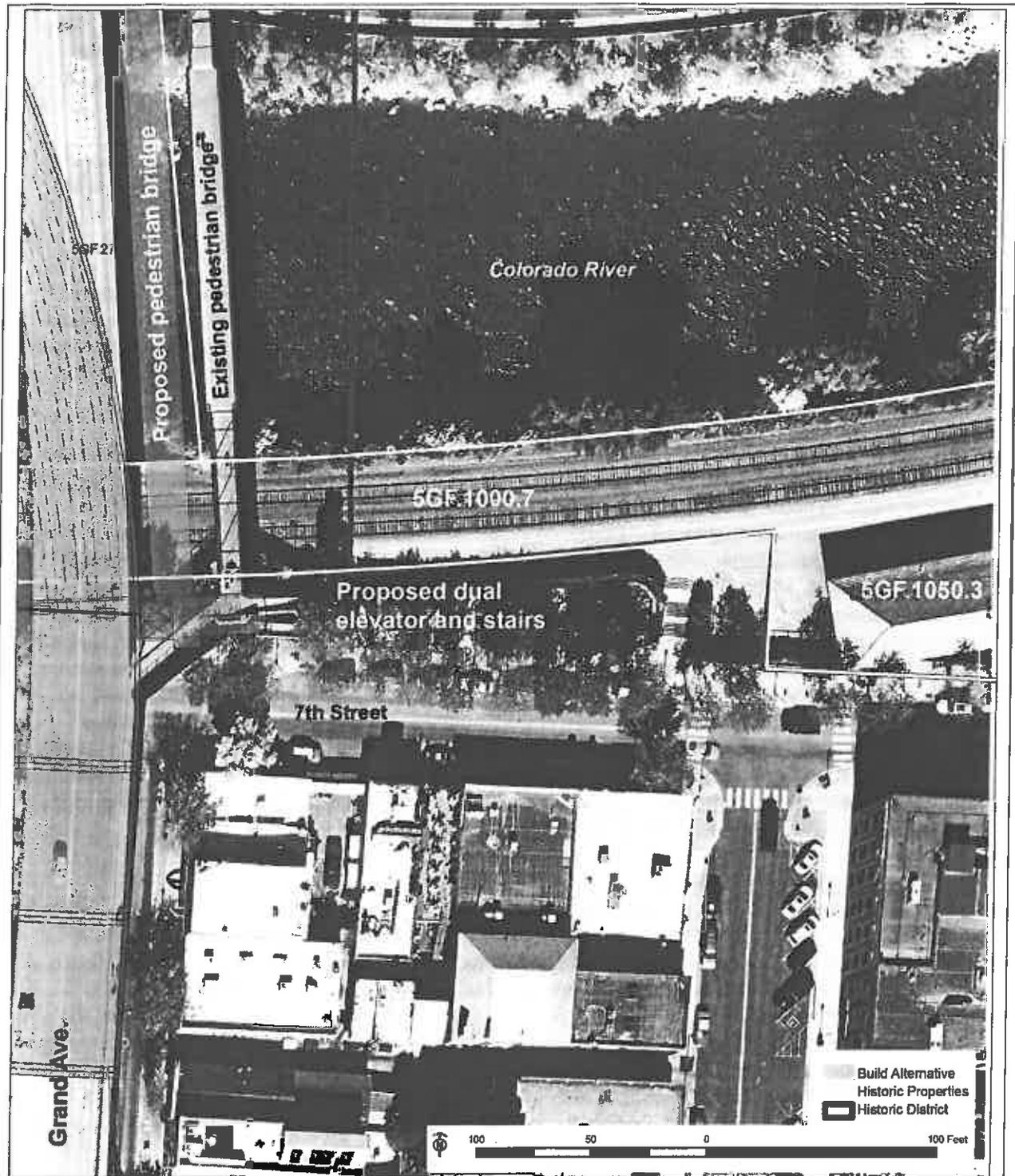
Depending on the roof design, the elevator height would extend approximately 15 to 20 feet above the pedestrian bridge deck. The scissor ramp would have been located approximately 68 feet from the train station, whereas the dual elevators/stairway would be located approximately 250 feet from the train station. This increased distance results in reduced visual changes from the pedestrian bridge access to the train station. The dual elevators/stairway are also anticipated to result in minimal visual changes to buildings and viewers along 7<sup>th</sup> Street. The aerial map below shows the layout of the elevators and stairway in relation to 7<sup>th</sup> Street and the train station. The rendering on the page following shows the elevators and stairway as viewed from 7<sup>th</sup> Street looking west and also depicts a clock tower design for the elevator bank that is currently being considered. However, design of the elevator bank and stairway will continue to be refined during the final design process in coordination your office, the consulting parties and the City. The dual elevator will be visible from the railroad station and will constitute a change to the setting of the area near the south end of the proposed new highway and pedestrian bridges, but the visual change is minimal in the area of the railroad station and will not alter the qualities that make that property significant. For these reasons, CDOT has determined that the dual elevators at the proposed pedestrian bridge will result in *no adverse effect* to 5GF1050.3.

#### **APPLICATION OF SECTION 4(F)**

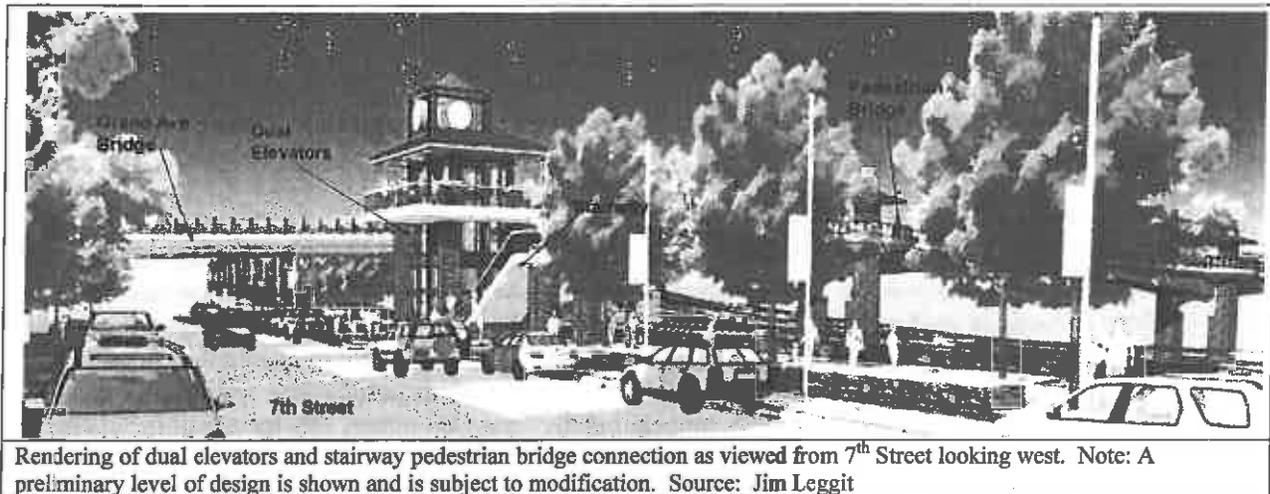
**Section 4(f) Net Benefit:** If the railroad connection is not restored by CDOT and is ultimately reconnected as part of a separate project by the City of Glenwood Springs (Option 1 above), FHWA and CDOT will apply the "Section 4(f) Evaluation and Approval for Transportation Projects That Have a Net Benefit to a Section 4(f) Property" for the Denver & Rio Grande Aspen Branch Railroad (5GF1661). The guidelines for application of the Net Benefits Programmatic Section 4(f) Evaluation require that FHWA obtain written agreement from your office regarding the assessment of effects; the proposed measures to minimize harm; the mitigation necessary to preserve, rehabilitate, and enhance those features and values of the Section 4(f) property; and that such measures will result in a net benefit to the Section 4(f) property. CDOT has implemented the following measures to minimize harm to the railroad at the detour location:

- 1) Use of a 2-to-1 cut slope to minimize grading limits;





Layout of dual elevators and stairway pedestrian bridge connection. Note: A preliminary level of design is shown and is subject to modification. Source: AMEC



- 2) Use of a minimal roadway section width of 24 feet, with an overall width of 29 feet with curb and gutter;
- 3) Use of the City of Glenwood Springs' maximum 8% roadway grade to reduce excavation limits.

CDOT proposes Level II documentation and interpretive signage or additional creative interpretive mitigation to resolve the adverse effect to the railroad. Archival documentation will have an overall net benefit because it will provide a permanent record of the resource. In addition, the proposed interpretive signage or other creative mitigation will contribute to a greater public understanding of the railroad system and railroad-related features, such as depot structures. An MOA will be developed in consultation with your office and the consulting parties to refine the terms of the mitigation. At this time we request your written agreement that this project results in a net benefit to the Denver & Rio Grande Aspen Branch railroad (5GF1661).

**Section 4(f) Temporary Occupancy Exception:** If CDOT restores the historic railroad connection as part of the 8<sup>th</sup> Street detour (Option 2 above), FHWA and CDOT will apply the temporary occupancy exception as outlined in 23 CFR 774.13(d) with regard to the D&RG Aspen Branch (5GF1661.7) and the Freight Depot (5GF5021). Criterion 5 of this exception requires documented agreement of the official with jurisdiction (SHPO) regarding the application of this exception. The five criteria for temporary occupancy follow:

1. **Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.**

The extension of 8<sup>th</sup> Street and the associated detour will be in operation during the 60-day closure of the Grand Avenue bridge. When the bridge is re-opened the temporary construction detour will be removed and the railroad bed and tracks restored. Overall detour construction, operation and restoring the detour area to its pre-construction condition will last between four and five months, less than the time required for the overall Grand Avenue bridge construction project.



There will be no change in ownership to any of the railroad property, and the temporary detour will be in place less than the time needed for construction of the project.

**2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal.**

The project involves the extension of 8<sup>th</sup> Street as a temporary detour while the Grand Avenue bridge is being replaced. The work will involve construction of the temporary detour that will extend across four segments of the Denver & Rio Grande railroad. This will involve removal of between 150 and 175 feet of track for each of the four segments of track, and excavation of the railbed to accommodate construction of a temporary paved two-lane road. Once the bridge replacement is completed, the detour will be removed and the railbed and tracks restored. This will involve backfill of the removed portion of the railbed and reinstalling the removed tracks. The nature of the change to the railroad property will be minimal and temporary and the railroad will be restored to its functionality.

**3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis.**

There will be no permanent adverse physical impacts to 5GF1661.7 or the associated freight depot (5GF5021). Segments of the four railroad lines within the railroad boundary will be removed temporarily to accommodate the temporary detour, but the railbed and tracks will be restored once the detour is no longer needed. This temporary effect will not interfere with the features and attributes of the railroad property, which is significant under National Register Criterion A for its role in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and for its association with the settlement of the Roaring Fork Valley and 19<sup>th</sup> century development of Aspen's mining industry.

A finding of *no adverse effect* under Section 106 has been made for this property.

**4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project.**

The project involves the extension of 8<sup>th</sup> Street as a temporary detour while the Grand Avenue bridge is being replaced. The work will involve construction of the temporary detour that will extend across four segments of the Denver & Rio Grande railroad. This will involve removal of between 150 and 175 feet of track for each of the four segments of track, and excavation of the railbed to accommodate construction of a temporary paved two-lane road. Once the bridge replacement is completed, the detour will be removed and the railbed and tracks restored. This will involve backfill of the removed portion of the railbed and reinstalling the removed tracks. The tracks will be installed in their previous alignment and the connection restored.

The land within the railroad's historic boundary will be returned to a condition as good as that which existed prior to construction.



**5. There must be documented agreement of the official with jurisdiction over the Section 4(f) resource regarding the above conditions.**

Based on the information provided herein, we request your agreement that this project meets the conditions of temporary occupancy as outlined above.

We request your review of the revised APE, concurrence with the Determination of Eligibility and Effect, and agreement with the application of the Net Benefit as well as with the application of the Temporary Occupancy exception. This information has also been forwarded to the following consulting parties for review: Glenwood Springs Historic Preservation Commission, Frontier Historical Society, and Colorado Preservation Inc. We will notify you of any responses received from these groups.

Thank you for your attention to this matter. If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

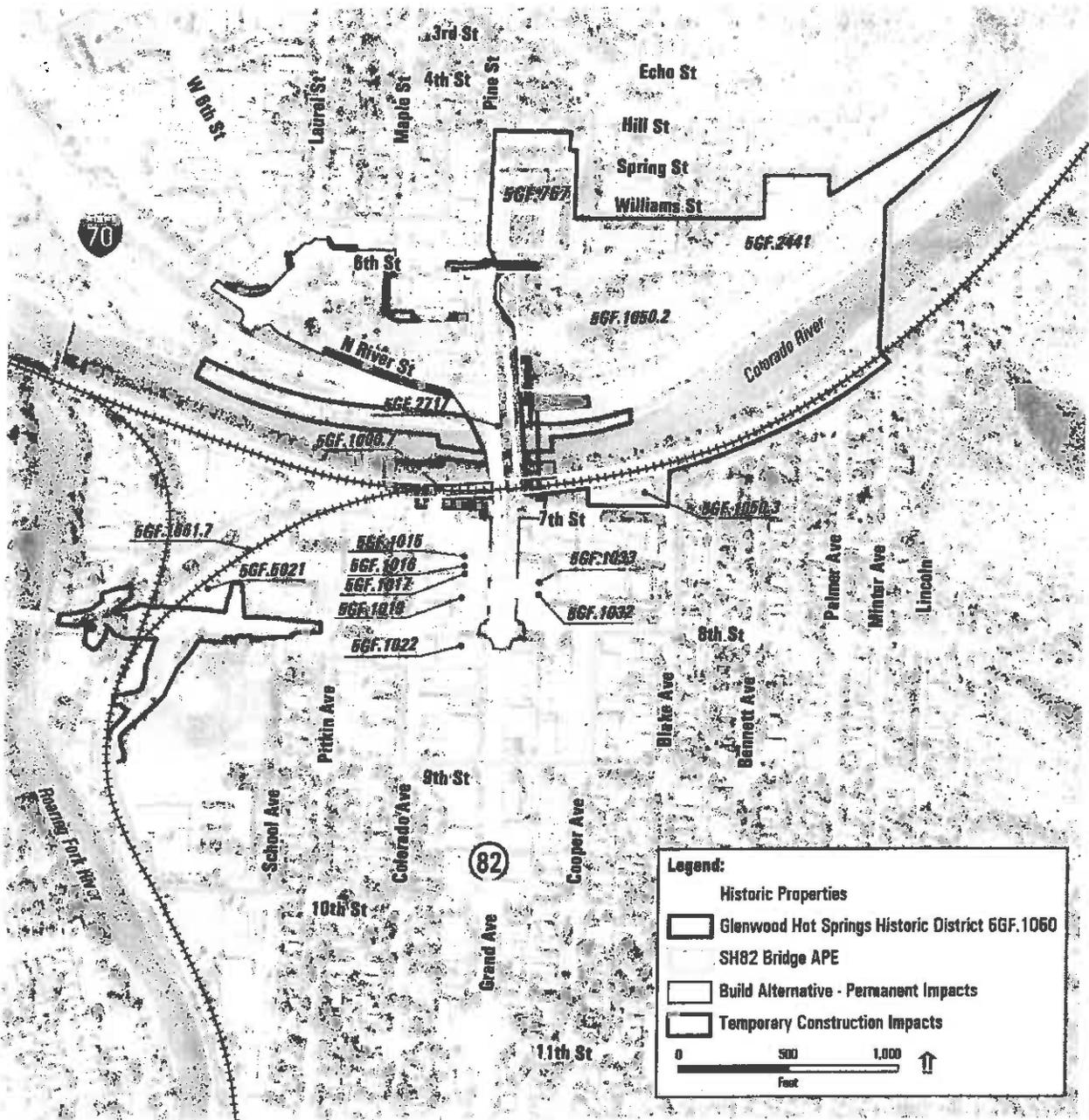
Very truly yours,

  
Jane Hann, Manager  
Environmental Programs Branch

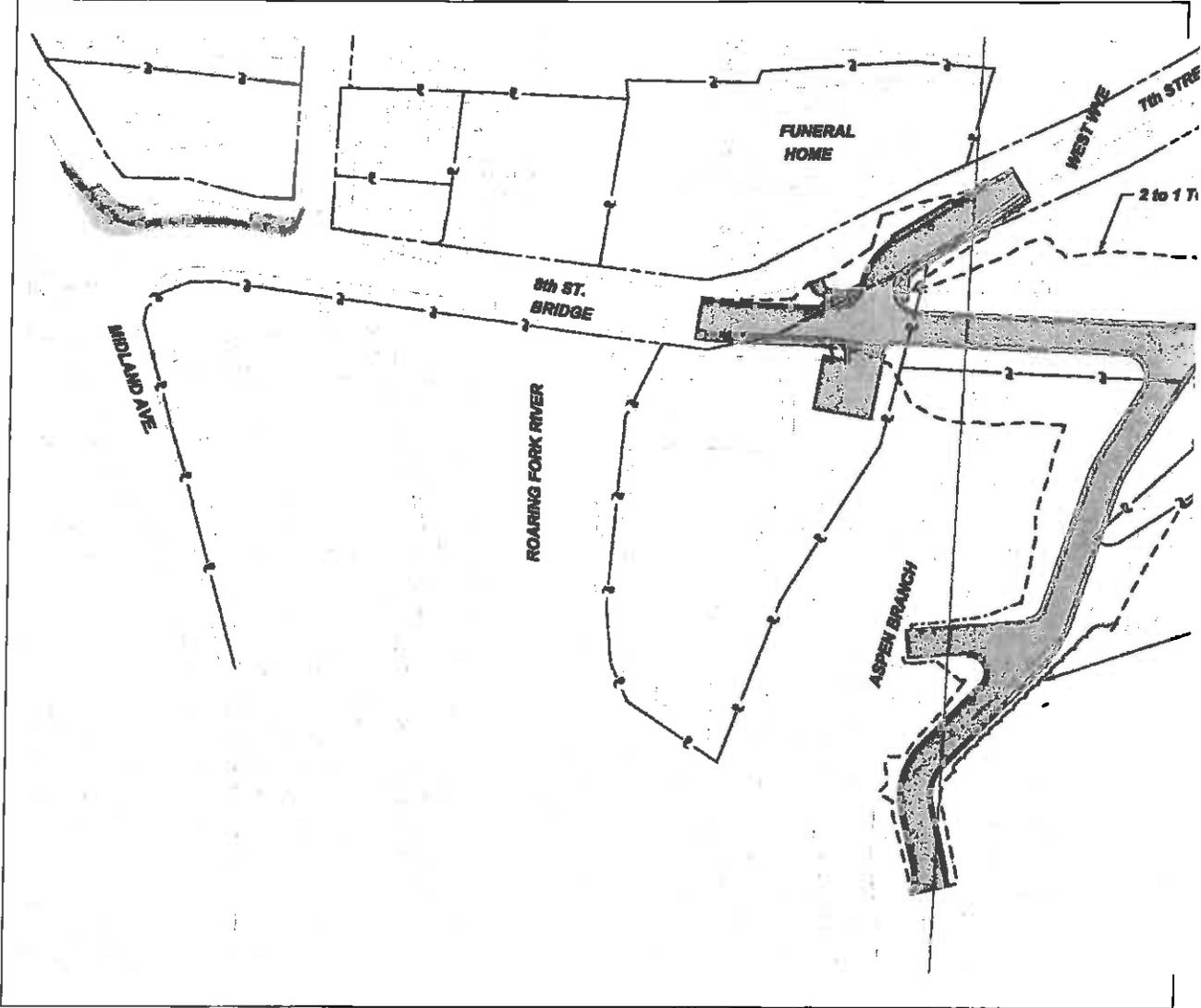
Enclosures: Revised APE map  
Site Forms  
Historic context  
Easement Graphic and Table







**Proposed 8<sup>th</sup> Street Construction Detour**





## **COLORADO**

**Department of Transportation**

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

April 2, 2014

Mr. John Martin  
Mr. Tom Jankovsky  
Board of County Commissioners  
108 8th Street, Suite 213  
Glenwood Springs, CO 81601

**SUBJECT: Additional Information on Determinations of Eligibility and Effect, Project FBR 0821-094, SH 82/Grand Ave. Bridge Replacement EA, Glenwood Springs**

Dear Mr. Martin and Mr. Jankovsky:

CDOT initially submitted eligibility and effect determinations to you for this project in August 2013 and invited you to participate as a Section 106 consulting party for the project. We did not receive a response from your organization at that time. Since our August submittal, the scope of the project has changed to include an extension of 8<sup>th</sup> Street to the existing 8<sup>th</sup> Street bridge over the Roaring Fork River as a temporary traffic detour during the approximate 60-day period when the Grand Avenue (SH 82) bridge would be fully closed to traffic during construction. Construction of the 8<sup>th</sup> Street detour would temporarily remove and require excavation of portions of the railroad tracks and railbed that currently traverse the area where 8<sup>th</sup> Street would be extended. See the attached graphic entitled "Proposed 8<sup>th</sup> Street Construction Detour" for more information.

This submittal includes the following information:

1. Revised Area of Potential Effect (APE)
2. New Eligibility and Effect Determinations for the Denver & Rio Grande Railroad Aspen Branch (5GF1661) and the associated freight depot (5GF5021).
3. Easements and aerial easements for the project
4. Design changes/updated effects information

### **REVISED AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) was updated to address the proposed 8<sup>th</sup> Street detour. The APE boundary has been expanded to include an additional area on 8<sup>th</sup> Street, and includes entire parcels adjacent to 8<sup>th</sup> Street. A map reflecting the revised APE is attached.

The direct impacts from building the temporary detour would affect four to six parcels along 8<sup>th</sup> Street and nearby streets, with only one historic feature affected. Indirect effects would include temporary construction impacts such as noise and dust, which are not anticipated to extend past those properties that front the streets. The APE includes the parcels that have structures that were built at least 45 years ago adjacent to these streets, as shown on the attached APE map.

### **ELIGIBILITY DETERMINATIONS**

The proposed 8<sup>th</sup> Street detour results in effects to an additional property that was not evaluated in our initial survey: a segment of the Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7) and its associated freight depot (5GF5021). Site forms for these properties are attached, as is a historic context for railroad development in the Glenwood Springs/Aspen area, which was prepared because that topic was not specifically addressed in the initial Historic Resources Survey Report.

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** This railroad line was built in 1887 by the Denver & Rio Grande Railroad, and was the first rail line to reach the lucrative silver mining operations in Aspen. The entire Aspen Branch of the D&RG was determined eligible for inclusion on the National Register of Historic Places (NRHP) in 1988. In 2005, the 41-mile resource was determined to retain its eligibility even though the rails and railbed of much of the entire Aspen Branch line was removed for conversion to a recreational trail. The Aspen Branch of the Denver & Rio Grande Railroad is significant under Criterion A for its key role in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and for its association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The existence of portions of the tracks, ties, and grade in this segment supports the eligibility of the entire resource.

**5GF5021, Freight Depot:** The freight depot is significant for its association with the D&RG Aspen Branch. The depot was built by the D&RG to support its rail operations and is eligible under Criterion A for the reasons stated above for the railroad. The original freight depot on this site was removed and replaced with the current structure after 1947. That depot has been further modified over the years and is not eligible under Criterion C. The freight depot was documented separately but is an associated feature of the overall railroad, and is included in the boundary for the railroad.

### **EFFECT DETERMINATIONS**

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour. The detour route for regional traffic begins at Exit 114 on I-70 and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River, then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary “square about” for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad that currently extends between the two sections of 8<sup>th</sup> Street.

Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific Railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for two to four years.

To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. This would require temporary removal and storage of approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements, noted as 13 and 15 in the



attached table of easement acquisitions, will be required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:

- West Wye Track – approximately 150 feet
- West Track of East Wye – approximately 165 feet
- Middle Track of East Wye – approximately 175 feet
- East Track of East Wye – approximately 175 feet

There are two options for the restoration of the railroad connection:

### **Option 1**

The City of Glenwood Springs approached the CDOT study team about entering into a partnership regarding its planned 8<sup>th</sup> Street Extension project. If the City gains approval and funding, their project would involve building a new bridge at the railroad crossing that would restore the railroad connection. In anticipation of the City's project, CDOT would leave the 8<sup>th</sup> Street detour in place.

8th Street in downtown Glenwood Springs currently terminates just west of School Street. The City's 8th Street project would connect the 8th Street Bridge over the Roaring Fork River along a new alignment under the UPRR railroad tracks. This would involve lowering 8th Street between its intersection at Pitkin Avenue and the 8th Street Bridge, a distance of approximately 900 feet. The City has developed some alternatives for their 8<sup>th</sup> Street Extension project but has not selected an option, so it is unclear how the railroad connection will be restored. If the City's project moves forward, CDOT will disconnect this railroad segment (5GF1661.7) and not restore it as part of this undertaking. Under this option, the project results in an *adverse effect* to the overall Denver & Ro Grande—Aspen Branch because the railroad connection will not be restored and the integrity of the segment in the project area would be diminished. Although the freight depot (5GF5021) will not be directly affected by disconnection of the overall railroad, construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks farther to the west. These grading activities would not directly affect the freight depot. However, because the depot is considered part of that property, it is included in the overall adverse effect to the railroad.

CDOT proposes Level II documentation and interpretative mitigation to resolve the adverse effect for this resource. Assuming Option 1 is implemented, CDOT plans to proceed with consultation to resolve the adverse effect to the railroad, including notification of the Advisory Council on Historic Preservation and development of mitigation through a Memorandum of Agreement (MOA).

### **Option 2**

If the City's project is *not* funded, CDOT will restore the railroad connection. The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition are anticipated to last between four and five months.

After completion of the project, the railbed will be restored and the overall railroad would still portray the key role it played in the settlement and commercial growth of Glenwood Springs and Colorado, as well as its association with settlement of the Roaring Fork Valley and the 19th century development of Aspen's



mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor—all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to 5GF1661.7 under this option.

**5GF5021, Freight Depot:** No direct impacts would occur to the freight depot. Construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks further to the west, but that would have no direct effect to the depot. Construction and operation of the detour, and returning that route to preconstruction conditions is anticipated to last four to five months. Temporary effects would include dust and noise from construction activities. Because no direct impacts would occur, the freight depot would still convey its association with the D&RG. For this reason, the proposed project would result in *no adverse effect* to 5GF5021.

### **TEMPORARY AND PERMANENT AERIAL EASEMENTS/UPDATED EFFECTS DETERMINATIONS**

Since we previously consulted, more information is available about the types of easements needed for the project. Attached is a table and map showing the locations of proposed temporary easements and a permanent aerial easement. Most of the parcels requiring easements are non-historic resources, including properties 1, 2, 3, 4, 6, 7 and 16. The other properties are evaluated below:

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August submittal; the entire railroad is eligible and the segment retains integrity. CDOT will acquire one permanent aerial easement totaling about 0.01 acre from the railroad to widen the Grand Avenue Bridge. The aerial easement is noted on the attached graphic as a small dark blue sliver west of the existing bridge, south of the river, and located between the yellow build alternative and the cross-hatched orange shading for a temporary easement. This easement would not affect property access, signage, or the property owner's current use of the property. CDOT will also acquire four temporary easement locations from the railroad, noted as 11 and 12 on the attached table and graphic. These four easements are combined so 11 equals 0.15 acre and 12 is 0.04 acre. These easements will be used for construction of a temporary grade crossing as noted in our August submittal. The crossing will be constructed to provide access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. We consulted on this previously and you concurred with the finding of *no adverse effect*. The temporary grade crossing and associated easements will not alter the qualities of significance of the railroad and the finding of no adverse effect is still appropriate. Note that easements 9 and 10 are on railroad property, but are not within the boundary of the historic railroad.

**Glenwood Hot Springs Historic District (5GF1050):** The project will require a temporary easement totaling 0.14 acres on the legal hot springs property but this is outside the boundary of the historic hot springs district. This easement is noted as 8 on the attached table and graphic.

### **DESIGN CHANGE/UPDATED EFFECT DETERMINATIONS**

#### **Additional Information on Vibration Effects**

As noted in our August 2013 submittal, properties 5GF1050.2, 5F1050.3, and 5GF767 would experience temporary effects of increased dust, noise and vibration during construction, which was considered in the determinations of effect. CDOT is mindful of the sensitive nature of the historic resources in the vicinity



of the project. These properties were considered during the design process, and measures to avoid and minimize direct and indirect impacts to these resources were incorporated into the preliminary design of the project.

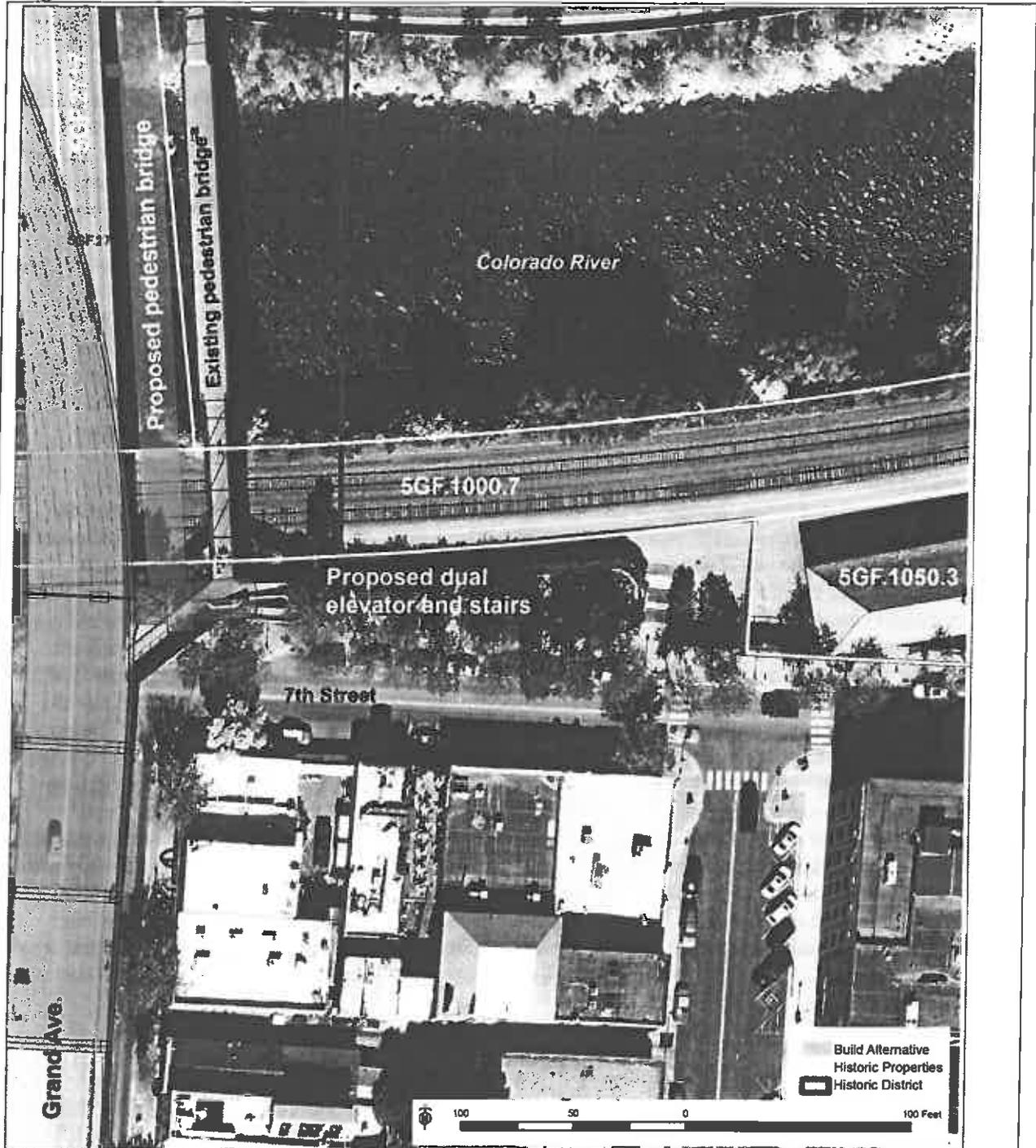
Pile driving would be required for construction of highway and pedestrian bridge piers. This construction activity would be the loudest of the construction operations and present the most potential for vibration impacts. However, at the current level of design, the exact areas where pile driving would occur and the technique(s) that would be employed have not been determined. The final design process will consider several factors, including geological/soil densities and composition, and proximity of historic resources to determine final pile driving locations and pile driving techniques that would be employed in order to protect sensitive properties from adverse vibratory effects. CDOT determined that the project results in a determination of *adverse effect* for properties 5GF1015, 5GF1016, 5GF1017, 5GF1019, 5GF1033 and 5GF1032 based on the new bridge construction. For properties 5GF1050.2, 5GF1050.3 and 5GF767, CDOT determined there would be *no adverse effect*, and based on current information on temporary increased noise, dust, and vibration, this finding is still valid. We will consult with you when there is more information regarding pile driving and vibration impacts but given that these are temporary construction impacts, it is not anticipated that the effect findings for these resources will change.

#### **Additional Information, Dual Elevators and Pedestrian Bridge Access**

Elevator and/or scissor ramp configurations were being considered for pedestrian bridge access at the time information was provided to you in August 2013. Since then, the design has been modified and dual elevators are now proposed. The dual elevators would be located at the south end of the pedestrian bridge, with the stairway located on the east side of the elevator bank as noted in Figure 1 below. The elevator height would extend approximately 15 to 20 feet above the pedestrian bridge deck, depending on the roof design. The dual elevators/stairway would be located approximately 250 feet from the train station. This increased distance results in reduced views of the pedestrian bridge access from the train station, and is anticipated to result in minimal visual changes toward the train station. In addition, the dual elevators/stairway is anticipated to result in minimal visual changes to buildings along 7<sup>th</sup> Street and to views from 7<sup>th</sup> Street. Figure 1 shows the layout of the elevators and stairway in relation to 7<sup>th</sup> Street and the train station. Figure 2 shows the elevators and stairway as viewed from 7<sup>th</sup> Street looking west, and depicts a clock tower design for the elevator bank. However, design of the elevator bank and stairway will continue to be refined during the final design process in coordination with consulting parties and the City. The dual elevators will result in some new elements in the viewshed to and from the train station but this will not alter the qualities that make the train station significant. The project will have some temporary effects of increased dust, noise and vibration, but overall the proposed dual elevator access will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. For these reasons, there is *no adverse effect* to the Denver & Rio Grande Railroad station (5GF1050.3).



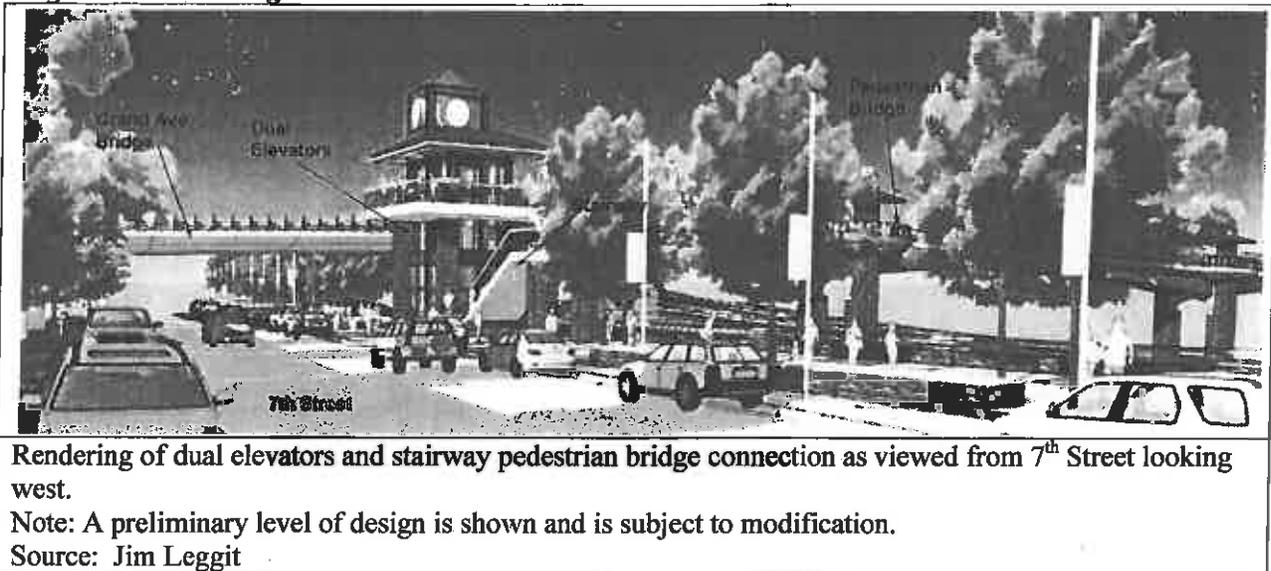
**Figure 1: Proposed Dual Elevator and Stairs**



Layout of dual elevators and stairway pedestrian bridge connection.  
Note: A preliminary level of design is shown and is subject to modification.  
Source: AMEC



**Figure 2: Rendering of Dual Elevator**



### **Proposed Pedestrian Bridge Design**

When we consulted with you in August 2013, we were considering symmetrical cable, asymmetrical cable and arch bridge types for the pedestrian bridge design. Based on stakeholder input and other design considerations, these types were eliminated. The preferred pedestrian bridge type consists of a five-span variable depth girder bridge.

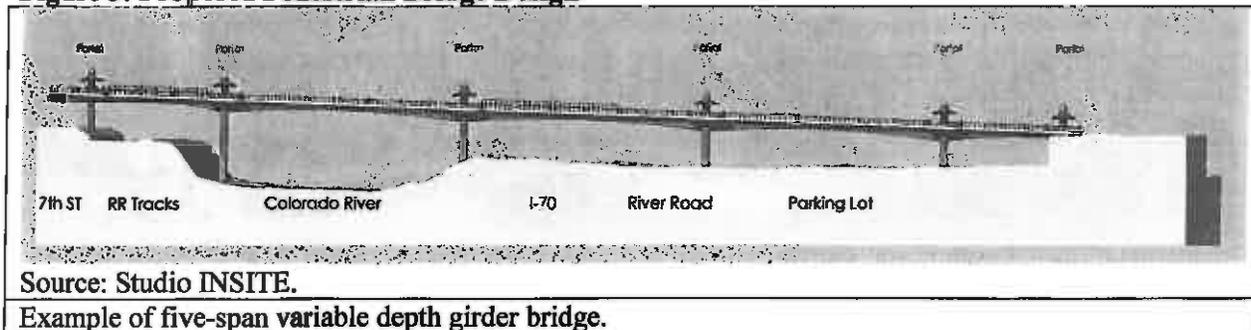
Under the current level of preliminary design, the bridge deck would be wider and flatter than the existing deck, and would have no overhead structure. It would cross the Colorado River on a straight alignment in a similar location as the existing bridge, with the northern touchdown point located slightly west of the existing touchdown point. It would include up to six piers, with no piers in the river. The bridge would feature pedestrian overlooks at bridge portals, either solid or see-through side railings, and lighting for safety purposes along the bridge and at bridge connections. This bridge type would have a lower profile than the other bridge options previously considered. For more information, see Figures 3 and 4 below. Note that these figures depict examples of aesthetic treatments under consideration, and the pier locations, size, materials and aesthetic treatments shown are not necessarily representative of how the final bridge design will appear. The historic and architectural context of the area will be considered when bridge materials and/or aesthetic bridge treatments are selected. Using the established Context Sensitive Solutions process, CDOT will continue to work with stakeholders to identify opportunities for aesthetic treatments in the design of the bridge, roadway, and sidewalk elements to reflect the materials and architectural style of Glenwood Springs' small town character and historic structures, as well as the visual and aesthetic goals and objectives provided in the I-70 Mountain Corridor Aesthetic.”

The proposed pedestrian bridge type would be more visually prominent than the *existing* pedestrian bridge, but will not be as visually prominent or intrusive on views of surrounding hillsides as the bridge designs previously under consideration. The pedestrian bridge type would result in a visual change to the overall setting of the area, and the dual elevators and stairway access at the southern end of the new pedestrian bridge would be partially visible from Grand Avenue. CDOT has determined that the proposed pedestrian bridge design will not have a direct effect on the train station—in other words the

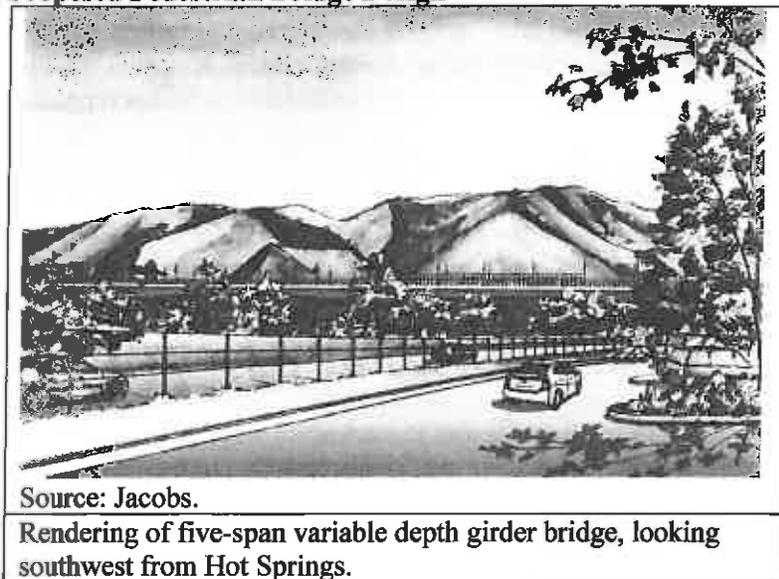


train station property will not be physically affected by the construction of the new pedestrian bridge. There are indirect effects based on visual changes in the views to and from the station. However, based on current design information CDOT has determined that the proposed pedestrian bridge will result in *no adverse effect* to the train station. We will consult with you as more aesthetic and design information is developed.

**Figure 3: Proposed Pedestrian Bridge Design**



**Figure 4: View of Proposed Pedestrian Bridge Design**



As a potential Section 106 consulting party, we welcome your comments on this information. Should you choose to respond we request that you do so within 30 days of receipt of these materials. If we do not hear from you in that time frame we will assume you do not plan to comment. If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).



Mr. Martin, Mr. Jankovsky  
April 2, 2014  
Page 9 of 12

Very truly yours,

  
for Jane Hann, Manager  
Environmental Programs Branch

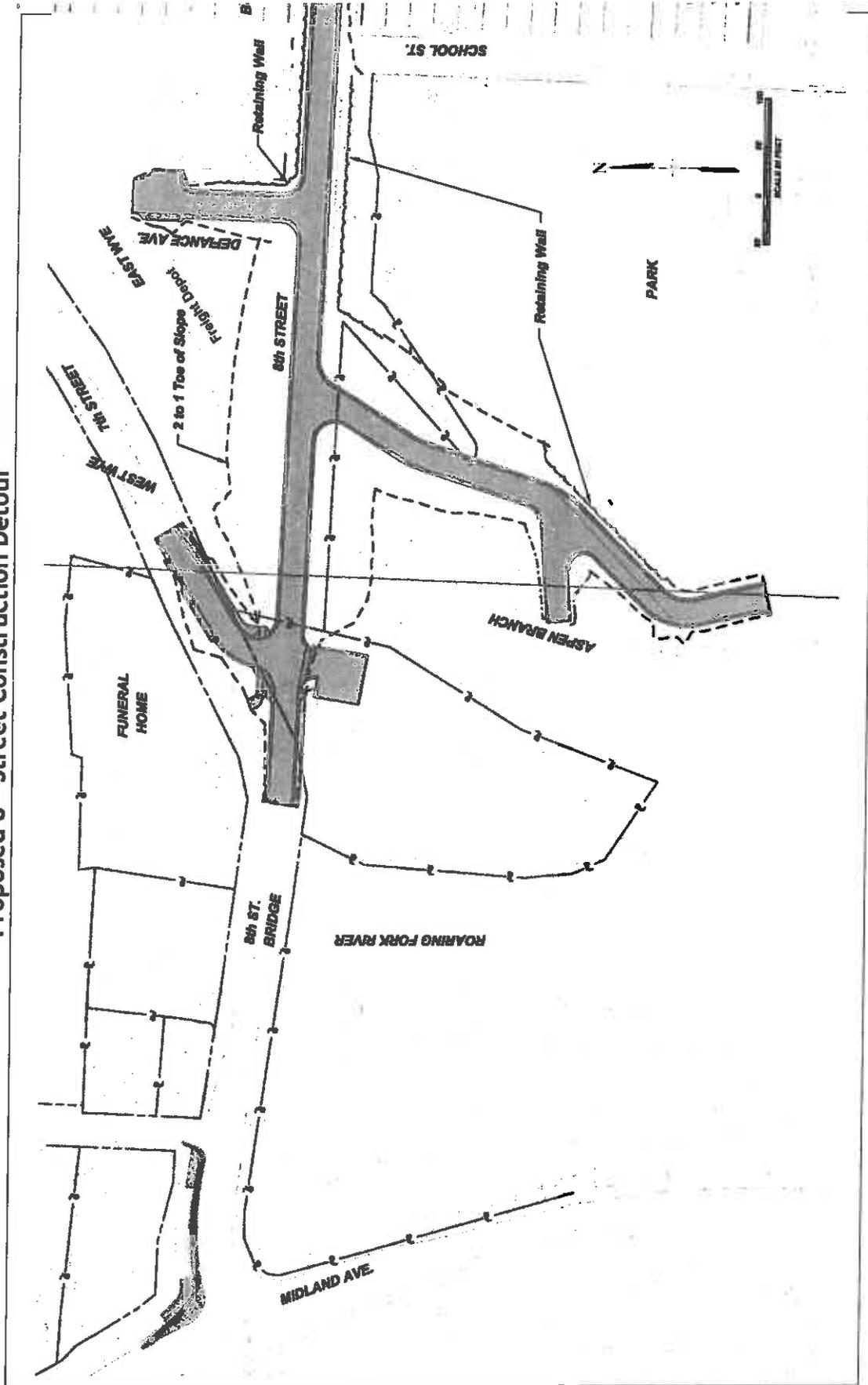
Enclosures:      Revised APE map  
                         Proposed 8<sup>th</sup> Street Construction Detour graphic  
                         Site Forms  
                         Historic context  
                         Easement Graphic and Table

cc:            Michael Vanderhoof, CDOT Region 3





### Proposed 8th Street Construction Detour







## COLORADO

Department of Transportation

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

April 2, 2014

Ms. Cindy Hines, Director  
Frontier Historical Society  
1001 Colorado Avenue  
Glenwood Springs, CO81601

**SUBJECT:** Additional Information on Determinations of Eligibility and Effect, Project FBR 0821-094,  
SH 82/Grand Ave. Bridge Replacement EA, Glenwood Springs

Dear Ms. Hines:

CDOT initially submitted eligibility and effect determinations to you for this project in August 2013 and invited you to participate as a Section 106 consulting party for the project. We did not receive a response from your organization at that time. Since our August submittal, the scope of the project has changed to include an extension of 8<sup>th</sup> Street to the existing 8<sup>th</sup> Street bridge over the Roaring Fork River as a temporary traffic detour during the approximate 60-day period when the Grand Avenue (SH 82) bridge would be fully closed to traffic during construction. Construction of the 8<sup>th</sup> Street detour would temporarily remove and require excavation of portions of the railroad tracks and railbed that currently traverse the area where 8<sup>th</sup> Street would be extended. See the attached graphic entitled "Proposed 8<sup>th</sup> Street Construction Detour" for more information.

This submittal includes the following information:

1. Revised Area of Potential Effect (APE)
2. New Eligibility and Effect Determinations for the Denver & Rio Grande Railroad Aspen Branch (5GF1661) and the associated freight depot (5GF5021).
3. Easements and aerial easements for the project
4. Design changes/updated effects information

### **REVISED AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) was updated to address the proposed 8<sup>th</sup> Street detour. The APE boundary has been expanded to include an additional area on 8<sup>th</sup> Street, and includes entire parcels adjacent to 8<sup>th</sup> Street. A map reflecting the revised APE is attached.

The direct impacts from building the temporary detour would affect four to six parcels along 8<sup>th</sup> Street and nearby streets, with only one historic feature affected. Indirect effects would include temporary construction impacts such as noise and dust, which are not anticipated to extend past those properties that front the streets. The APE includes the parcels that have structures that were built at least 45 years ago adjacent to these streets, as shown on the attached APE map.

### **ELIGIBILITY DETERMINATIONS**

The proposed 8<sup>th</sup> Street detour results in effects to an additional property that was not evaluated in our initial survey: a segment of the Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7) and its associated freight depot (5GF5021). Site forms for these properties are attached, as is a historic context for railroad development in the Glenwood Springs/Aspen area, which was prepared because that topic was not specifically addressed in the initial Historic Resources Survey Report.

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** This railroad line was built in 1887 by the Denver & Rio Grande Railroad, and was the first rail line to reach the lucrative silver mining operations in Aspen. The entire Aspen Branch of the D&RG was determined eligible for inclusion on the National Register of Historic Places (NRHP) in 1988. In 2005, the 41-mile resource was determined to retain its eligibility even though the rails and railbed of much of the entire Aspen Branch line was removed for conversion to a recreational trail. The Aspen Branch of the Denver & Rio Grande Railroad is significant under Criterion A for its key role in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and for its association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The existence of portions of the tracks, ties, and grade in this segment supports the eligibility of the entire resource.

**5GF5021, Freight Depot:** The freight depot is significant for its association with the D&RG Aspen Branch. The depot was built by the D&RG to support its rail operations and is eligible under Criterion A for the reasons stated above for the railroad. The original freight depot on this site was removed and replaced with the current structure after 1947. That depot has been further modified over the years and is not eligible under Criterion C. The freight depot was documented separately but is an associated feature of the overall railroad, and is included in the boundary for the railroad.

### **EFFECT DETERMINATIONS**

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour. The detour route for regional traffic begins at Exit 114 on I-70 and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River, then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary “square about” for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad that currently extends between the two sections of 8<sup>th</sup> Street.

Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific Railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for two to four years.

To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. This would require temporary removal and storage of approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements, noted as 13 and 15 in the



attached table of easement acquisitions, will be required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:

- West Wye Track – approximately 150 feet
- West Track of East Wye – approximately 165 feet
- Middle Track of East Wye – approximately 175 feet
- East Track of East Wye – approximately 175 feet

There are two options for the restoration of the railroad connection:

#### **Option 1**

The City of Glenwood Springs approached the CDOT study team about entering into a partnership regarding its planned 8<sup>th</sup> Street Extension project. If the City gains approval and funding, their project would involve building a new bridge at the railroad crossing that would restore the railroad connection. In anticipation of the City's project, CDOT would leave the 8<sup>th</sup> Street detour in place.

8th Street in downtown Glenwood Springs currently terminates just west of School Street. The City's 8th Street project would connect the 8th Street Bridge over the Roaring Fork River along a new alignment under the UPRR railroad tracks. This would involve lowering 8th Street between its intersection at Pitkin Avenue and the 8th Street Bridge, a distance of approximately 900 feet. The City has developed some alternatives for their 8<sup>th</sup> Street Extension project but has not selected an option, so it is unclear how the railroad connection will be restored. If the City's project moves forward, CDOT will disconnect this railroad segment (5GF1661.7) and not restore it as part of this undertaking. Under this option, the project results in an *adverse effect* to the overall Denver & Ro Grande—Aspen Branch because the railroad connection will not be restored and the integrity of the segment in the project area would be diminished. Although the freight depot (5GF5021) will not be directly affected by disconnection of the overall railroad, construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks farther to the west. These grading activities would not directly affect the freight depot. However, because the depot is considered part of that property, it is included in the overall adverse effect to the railroad.

CDOT proposes Level II documentation and interpretative mitigation to resolve the adverse effect for this resource. Assuming Option 1 is implemented, CDOT plans to proceed with consultation to resolve the adverse effect to the railroad, including notification of the Advisory Council on Historic Preservation and development of mitigation through a Memorandum of Agreement (MOA).

#### **Option 2**

If the City's project is *not* funded, CDOT will restore the railroad connection. The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition are anticipated to last between four and five months.

After completion of the project, the railbed will be restored and the overall railroad would still portray the key role it played in the settlement and commercial growth of Glenwood Springs and Colorado, as well as its association with settlement of the Roaring Fork Valley and the 19th century development of Aspen's



mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor—all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to 5GF1661.7 under this option.

**5GF5021, Freight Depot:** No direct impacts would occur to the freight depot. Construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks further to the west, but that would have no direct effect to the depot. Construction and operation of the detour, and returning that route to preconstruction conditions is anticipated to last four to five months. Temporary effects would include dust and noise from construction activities. Because no direct impacts would occur, the freight depot would still convey its association with the D&RG. For this reason, the proposed project would result in *no adverse effect* to 5GF5021.

### **TEMPORARY AND PERMANENT AERIAL EASEMENTS/UPDATED EFFECTS DETERMINATIONS**

Since we previously consulted, more information is available about the types of easements needed for the project. Attached is a table and map showing the locations of proposed temporary easements and a permanent aerial easement. Most of the parcels requiring easements are non-historic resources, including properties 1, 2, 3, 4, 6, 7 and 16. The other properties are evaluated below:

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August submittal; the entire railroad is eligible and the segment retains integrity. CDOT will acquire one permanent aerial easement totaling about 0.01 acre from the railroad to widen the Grand Avenue Bridge. The aerial easement is noted on the attached graphic as a small dark blue sliver west of the existing bridge, south of the river, and located between the yellow build alternative and the cross-hatched orange shading for a temporary easement. This easement would not affect property access, signage, or the property owner's current use of the property. CDOT will also acquire four temporary easement locations from the railroad, noted as 11 and 12 on the attached table and graphic. These four easements are combined so 11 equals 0.15 acre and 12 is 0.04 acre. These easements will be used for construction of a temporary grade crossing as noted in our August submittal. The crossing will be constructed to provide access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. We consulted on this previously and you concurred with the finding of *no adverse effect*. The temporary grade crossing and associated easements will not alter the qualities of significance of the railroad and the finding of no adverse effect is still appropriate. Note that easements 9 and 10 are on railroad property, but are not within the boundary of the historic railroad.

**Glenwood Hot Springs Historic District (5GF1050):** The project will require a temporary easement totaling 0.14 acres on the legal hot springs property but this is outside the boundary of the historic hot springs district. This easement is noted as 8 on the attached table and graphic.

### **DESIGN CHANGE/UPDATED EFFECT DETERMINATIONS**

#### **Additional Information on Vibration Effects**



As noted in our August 2013 submittal, properties 5GF1050.2, 5F1050.3, and 5GF767 would experience temporary effects of increased dust, noise and vibration during construction, which was considered in the determinations of effect. CDOT is mindful of the sensitive nature of the historic resources in the vicinity of the project. These properties were considered during the design process, and measures to avoid and minimize direct and indirect impacts to these resources were incorporated into the preliminary design of the project.

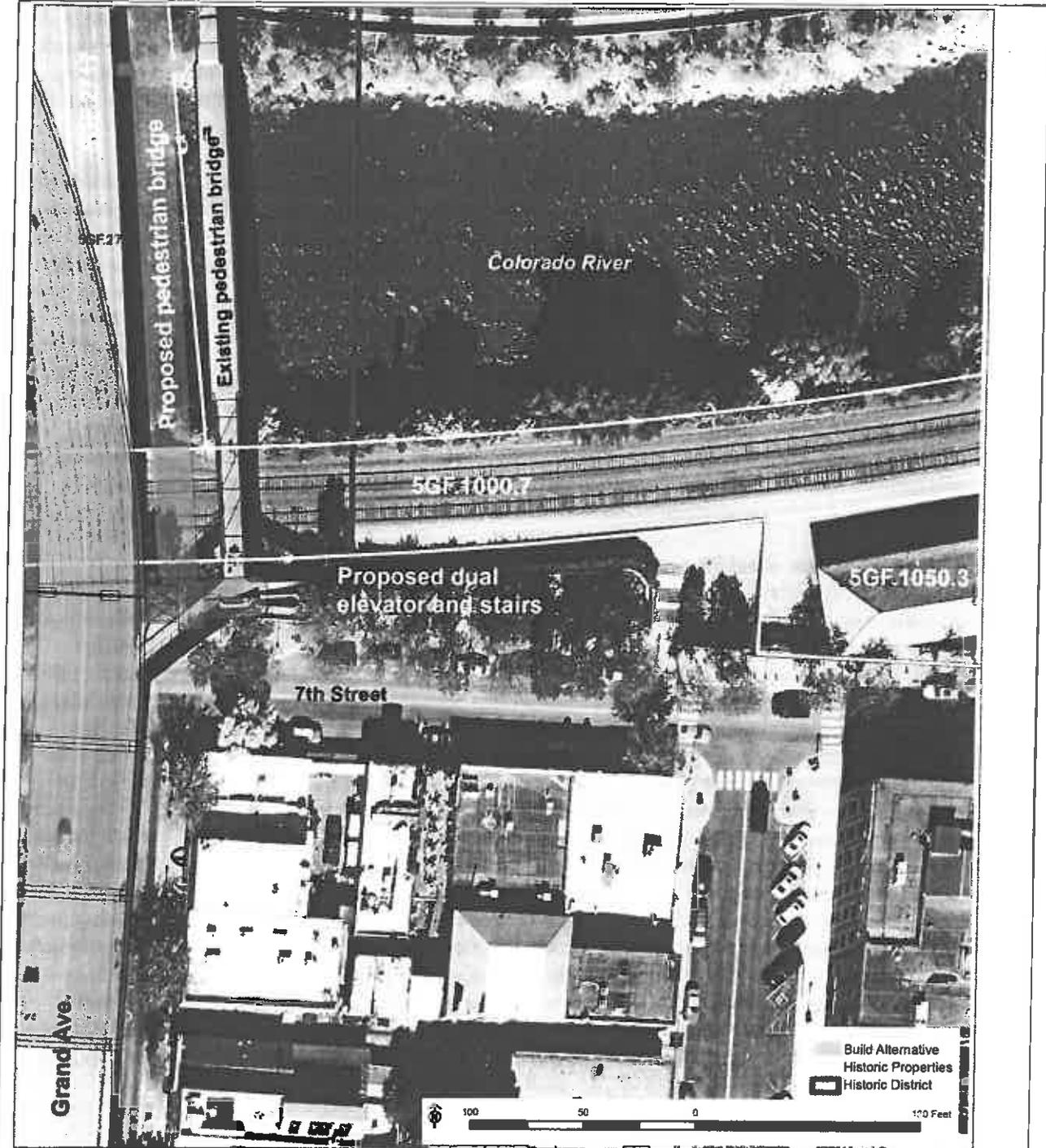
Pile driving would be required for construction of highway and pedestrian bridge piers. This construction activity would be the loudest of the construction operations and present the most potential for vibration impacts. However, at the current level of design, the exact areas where pile driving would occur and the technique(s) that would be employed have not been determined. The final design process will consider several factors, including geological/soil densities and composition, and proximity of historic resources to determine final pile driving locations and pile driving techniques that would be employed in order to protect sensitive properties from adverse vibratory effects. CDOT determined that the project results in a determination of *adverse effect* for properties 5GF1015, 5GF1016, 5GF1017, 5GF1019, 5GF1033 and 5GF1032 based on the new bridge construction. For properties 5GF1050.2, 5GF1050.3 and 5GF767, CDOT determined there would be *no adverse effect*, and based on current information on temporary increased noise, dust, and vibration, this finding is still valid. We will consult with you when there is more information regarding pile driving and vibration impacts but given that these are temporary construction impacts, it is not anticipated that the effect findings for these resources will change.

#### **Additional Information, Dual Elevators and Pedestrian Bridge Access**

Elevator and/or scissor ramp configurations were being considered for pedestrian bridge access at the time information was provided to you in August 2013. Since then, the design has been modified and dual elevators are now proposed. The dual elevators would be located at the south end of the pedestrian bridge, with the stairway located on the east side of the elevator bank as noted in Figure 1 below. The elevator height would extend approximately 15 to 20 feet above the pedestrian bridge deck, depending on the roof design. The dual elevators/stairway would be located approximately 250 feet from the train station. This increased distance results in reduced views of the pedestrian bridge access from the train station, and is anticipated to result in minimal visual changes toward the train station. In addition, the dual elevators/stairway is anticipated to result in minimal visual changes to buildings along 7<sup>th</sup> Street and to views from 7<sup>th</sup> Street. Figure 1 shows the layout of the elevators and stairway in relation to 7<sup>th</sup> Street and the train station. Figure 2 shows the elevators and stairway as viewed from 7<sup>th</sup> Street looking west, and depicts a clock tower design for the elevator bank. However, design of the elevator bank and stairway will continue to be refined during the final design process in coordination with consulting parties and the City. The dual elevators will result in some new elements in the viewshed to and from the train station but this will not alter the qualities that make the train station significant. The project will have some temporary effects of increased dust, noise and vibration, but overall the proposed dual elevator access will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. For these reasons, there is *no adverse effect* to the Denver & Rio Grande Railroad station (5GF1050.3).



**Figure 1: Proposed Dual Elevator and Stairs**



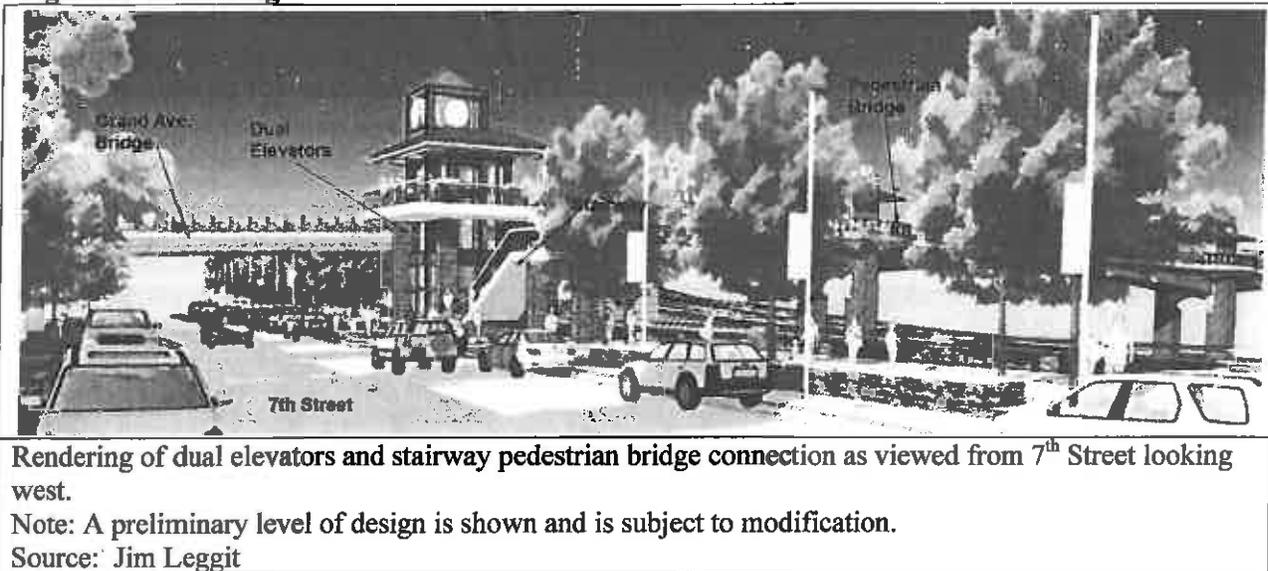
Layout of dual elevators and stairway pedestrian bridge connection.

Note: A preliminary level of design is shown and is subject to modification.

Source: AMEC



**Figure 2: Rendering of Dual Elevator**



### **Proposed Pedestrian Bridge Design**

When we consulted with you in August 2013, we were considering symmetrical cable, asymmetrical cable and arch bridge types for the pedestrian bridge design. Based on stakeholder input and other design considerations, these types were eliminated. The preferred pedestrian bridge type consists of a five-span variable depth girder bridge.

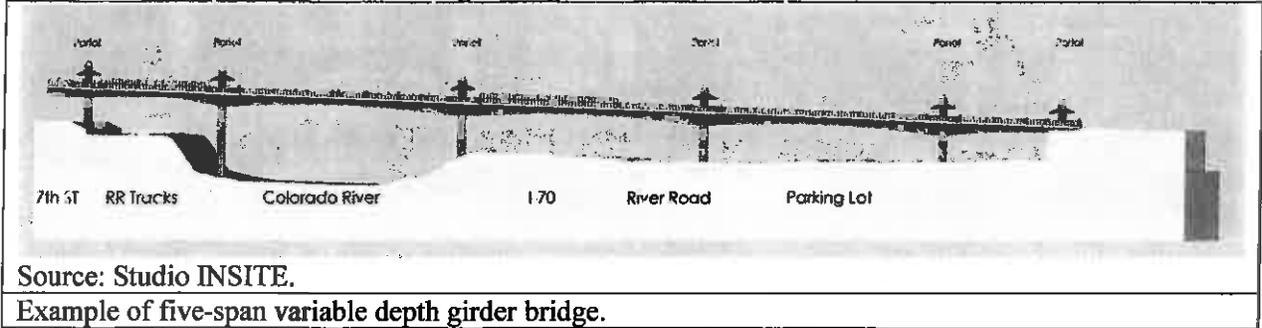
Under the current level of preliminary design, the bridge deck would be wider and flatter than the existing deck, and would have no overhead structure. It would cross the Colorado River on a straight alignment in a similar location as the existing bridge, with the northern touchdown point located slightly west of the existing touchdown point. It would include up to six piers, with no piers in the river. The bridge would feature pedestrian overlooks at bridge portals, either solid or see-through side railings, and lighting for safety purposes along the bridge and at bridge connections. This bridge type would have a lower profile than the other bridge options previously considered. For more information, see Figures 3 and 4 below. Note that these figures depict examples of aesthetic treatments under consideration, and the pier locations, size, materials and aesthetic treatments shown are not necessarily representative of how the final bridge design will appear. The historic and architectural context of the area will be considered when bridge materials and/or aesthetic bridge treatments are selected. Using the established Context Sensitive Solutions process, CDOT will continue to work with stakeholders to identify opportunities for aesthetic treatments in the design of the bridge, roadway, and sidewalk elements to reflect the materials and architectural style of Glenwood Springs' small town character and historic structures, as well as the visual and aesthetic goals and objectives provided in the I-70 Mountain Corridor Aesthetic."

The proposed pedestrian bridge type would be more visually prominent than the *existing* pedestrian bridge, but will not be as visually prominent or intrusive on views of surrounding hillsides as the bridge designs previously under consideration. The pedestrian bridge type would result in a visual change to the overall setting of the area, and the dual elevators and stairway access at the southern end of the new pedestrian bridge would be partially visible from Grand Avenue. CDOT has determined that the proposed pedestrian bridge design will not have a direct effect on the train station—in other words the

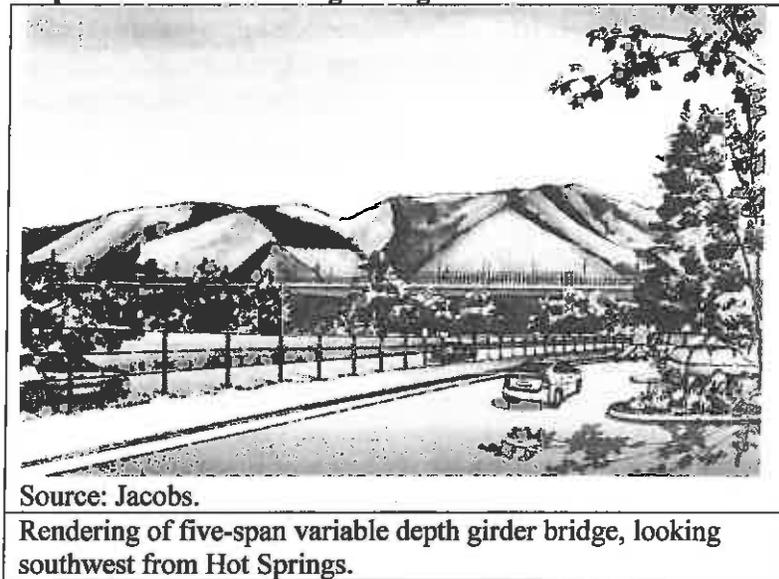


train station property will not be physically affected by the construction of the new pedestrian bridge. There are indirect effects based on visual changes in the views to and from the station. However, based on current design information CDOT has determined that the proposed pedestrian bridge will result in *no adverse effect* to the train station. We will consult with you as more aesthetic and design information is developed.

**Figure 3: Proposed Pedestrian Bridge Design**



**Figure 4: View of Proposed Pedestrian Bridge Design**

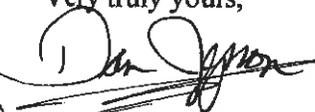


As a potential Section 106 consulting party, we welcome your comments on this information. Should you choose to respond we request that you do so within 30 days of receipt of these materials. If we do not hear from you in that time frame we will assume you do not plan to comment. If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).



Ms. Hines  
April 2, 2014  
Page 9 of 12

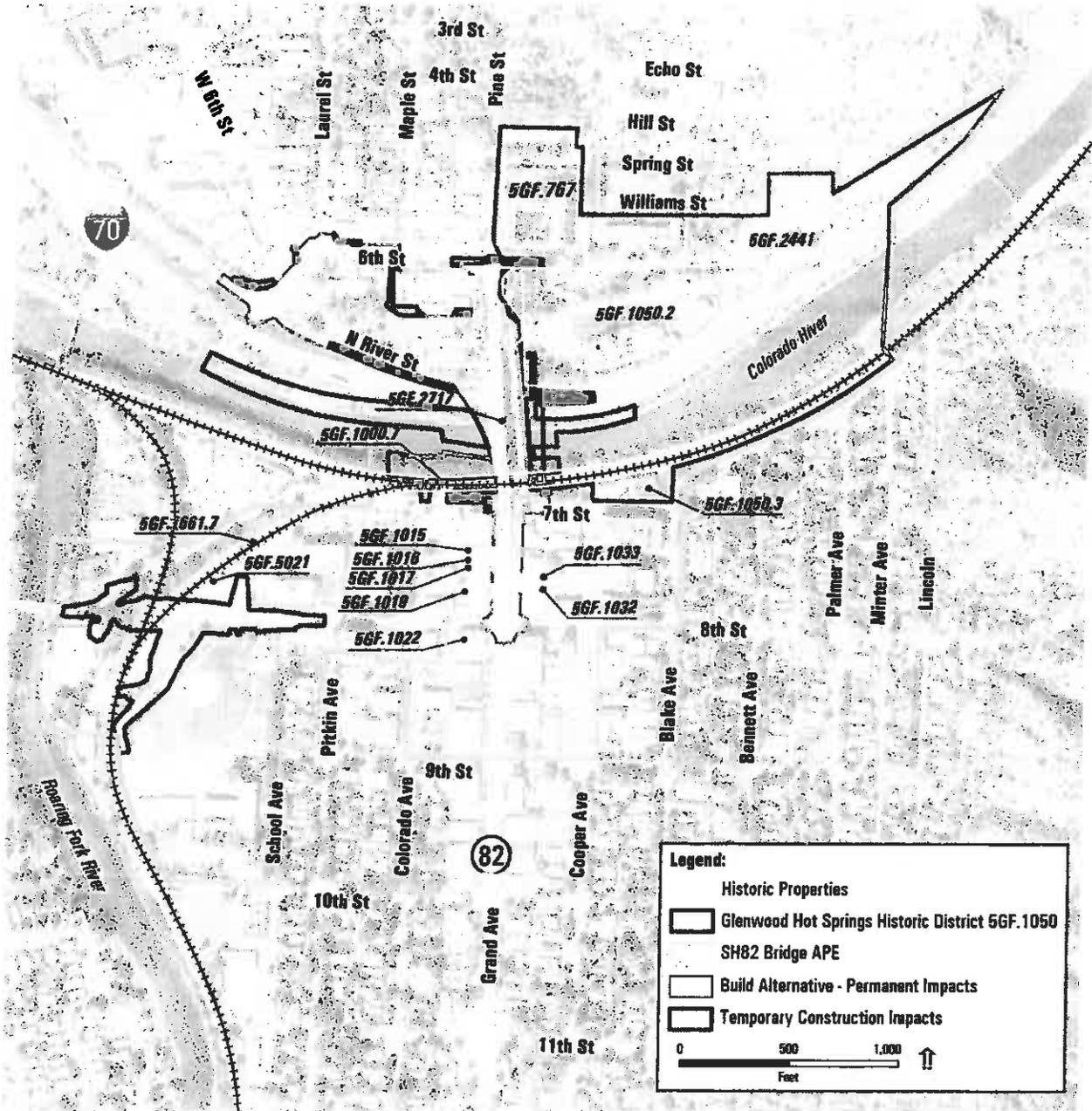
Very truly yours,

  
 Jane Hann, Manager  
Environmental Programs Branch

Enclosures:      Revised APE map  
                      Proposed 8<sup>th</sup> Street Construction Detour graphic  
                      Site Forms  
                      Historic context  
                      Easement Graphic and Table

cc:            Michael Vanderhoof, CDOT Region 3

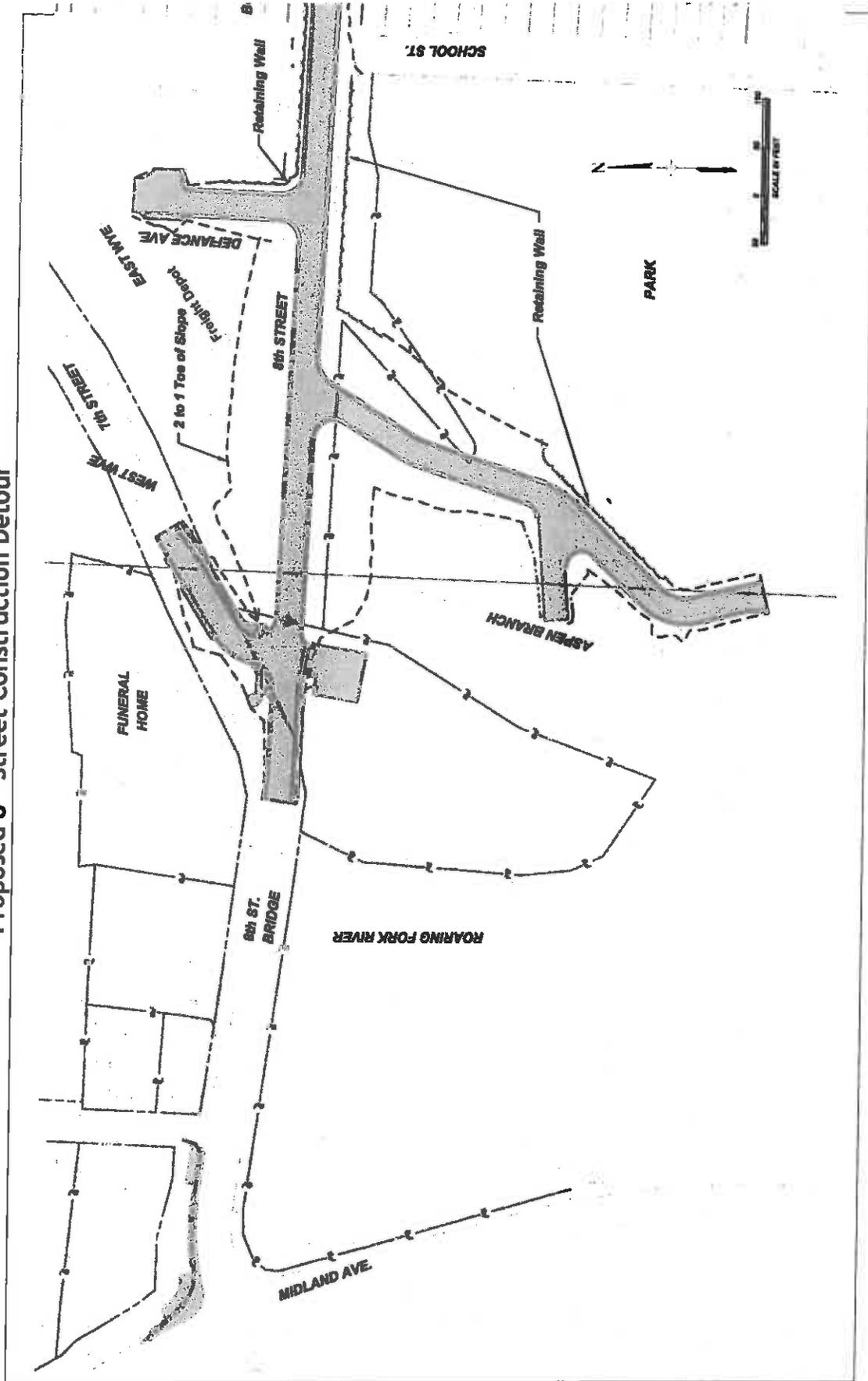




Revised Area of Potential Effects



### Proposed 8th Street Construction Detour







## COLORADO

Department of Transportation

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

April 2, 2014

Ms. Jane Daniels  
Colorado Preservation Incorporated  
1420 Ogden Street, Suite 103  
Denver, CO 80218

**SUBJECT: Additional Information on Determinations of Eligibility and Effect, Project FBR 0821-094, SH 82/Grand Ave. Bridge Replacement EA, Glenwood Springs**

Dear Ms. Daniels:

CDOT initially submitted eligibility and effect determinations to you for this project in August 2013 and invited you to participate as a Section 106 consulting party for the project. We did not receive a response from your organization at that time. Since our August submittal, the scope of the project has changed to include an extension of 8<sup>th</sup> Street to the existing 8<sup>th</sup> Street bridge over the Roaring Fork River as a temporary traffic detour during the approximate 60-day period when the Grand Avenue (SH 82) bridge would be fully closed to traffic during construction. Construction of the 8<sup>th</sup> Street detour would temporarily remove and require excavation of portions of the railroad tracks and railbed that currently traverse the area where 8<sup>th</sup> Street would be extended. See the attached graphic entitled "Proposed 8<sup>th</sup> Street Construction Detour" for more information.

This submittal includes the following information:

1. Revised Area of Potential Effect (APE)
2. New Eligibility and Effect Determinations for the Denver & Rio Grande Railroad Aspen Branch (5GF1661) and the associated freight depot (5GF5021).
3. Easements and aerial easements for the project
4. Design changes/updated effects information

### **REVISED AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) was updated to address the proposed 8<sup>th</sup> Street detour. The APE boundary has been expanded to include an additional area on 8<sup>th</sup> Street, and includes entire parcels adjacent to 8<sup>th</sup> Street. A map reflecting the revised APE is attached.

The direct impacts from building the temporary detour would affect four to six parcels along 8<sup>th</sup> Street and nearby streets, with only one historic feature affected. Indirect effects would include temporary construction impacts such as noise and dust, which are not anticipated to extend past those properties that front the streets. The APE includes the parcels that have structures that were built at least 45 years ago adjacent to these streets, as shown on the attached APE map.

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Since we previously consulted, more information is available about the types of easements needed for the project. Attached is a table and map showing the locations of proposed temporary easements and a permanent aerial easement. Most of the parcels requiring easements are non-historic resources, including properties 1, 2, 3, 4, 6, 7 and 16. The other properties are evaluated below:

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of the project. These properties were considered during the design process, and measures to avoid and minimize direct and indirect impacts to these resources were incorporated into the preliminary design of the project.

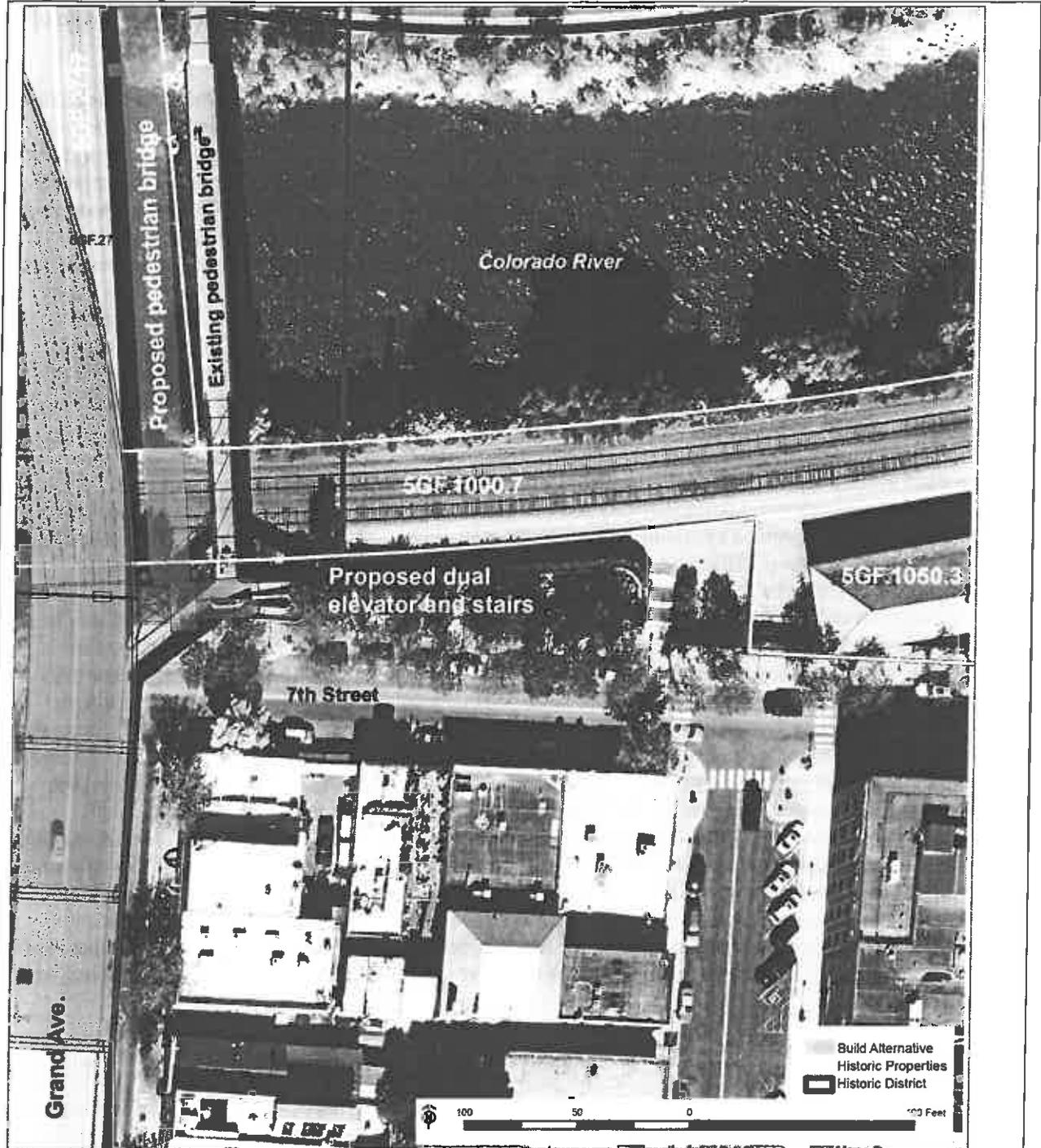
Pile driving would be required for construction of highway and pedestrian bridge piers. This construction activity would be the loudest of the construction operations and present the most potential for vibration impacts. However, at the current level of design, the exact areas where pile driving would occur and the technique(s) that would be employed have not been determined. The final design process will consider several factors, including geological/soil densities and composition, and proximity of historic resources to determine final pile driving locations and pile driving techniques that would be employed in order to protect sensitive properties from adverse vibratory effects. CDOT determined that the project results in a determination of *adverse effect* for properties 5GF1015, 5GF1016, 5GF1017, 5GF1019, 5GF1033 and 5GF1032 based on the new bridge construction. For properties 5GF1050.2, 5GF1050.3 and 5GF767, CDOT determined there would be *no adverse effect*, and based on current information on temporary increased noise, dust, and vibration, this finding is still valid. We will consult with you when there is more information regarding pile driving and vibration impacts but given that these are temporary construction impacts, it is not anticipated that the effect findings for these resources will change.

#### **Additional Information, Dual Elevators and Pedestrian Bridge Access**

Elevator and/or scissor ramp configurations were being considered for pedestrian bridge access at the time information was provided to you in August 2013. Since then, the design has been modified and dual elevators are now proposed. The dual elevators would be located at the south end of the pedestrian bridge, with the stairway located on the east side of the elevator bank as noted in Figure 1 below. The elevator height would extend approximately 15 to 20 feet above the pedestrian bridge deck, depending on the roof design. The dual elevators/stairway would be located approximately 250 feet from the train station. This increased distance results in reduced views of the pedestrian bridge access from the train station, and is anticipated to result in minimal visual changes toward the train station. In addition, the dual elevators/stairway is anticipated to result in minimal visual changes to buildings along 7<sup>th</sup> Street and to views from 7<sup>th</sup> Street. Figure 1 shows the layout of the elevators and stairway in relation to 7<sup>th</sup> Street and the train station. Figure 2 shows the elevators and stairway as viewed from 7<sup>th</sup> Street looking west, and depicts a clock tower design for the elevator bank. However, design of the elevator bank and stairway will continue to be refined during the final design process in coordination with consulting parties and the City. The dual elevators will result in some new elements in the viewshed to and from the train station but this will not alter the qualities that make the train station significant. The project will have some temporary effects of increased dust, noise and vibration, but overall the proposed dual elevator access will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. For these reasons, there is *no adverse effect* to the Denver & Rio Grande Railroad station (5GF1050.3).



**Figure 1: Proposed Dual Elevator and Stairs**



Layout of dual elevators and stairway pedestrian bridge connection.  
Note: A preliminary level of design is shown and is subject to modification.  
Source: AMEC



**Figure 2: Rendering of Dual Elevator**



Rendering of dual elevators and stairway pedestrian bridge connection as viewed from 7<sup>th</sup> Street looking west.

Note: A preliminary level of design is shown and is subject to modification.

Source: Jim Leggit

### **Proposed Pedestrian Bridge Design**

When we consulted with you in August 2013, we were considering symmetrical cable, asymmetrical cable and arch bridge types for the pedestrian bridge design. Based on stakeholder input and other design considerations, these types were eliminated. The preferred pedestrian bridge type consists of a five-span variable depth girder bridge.

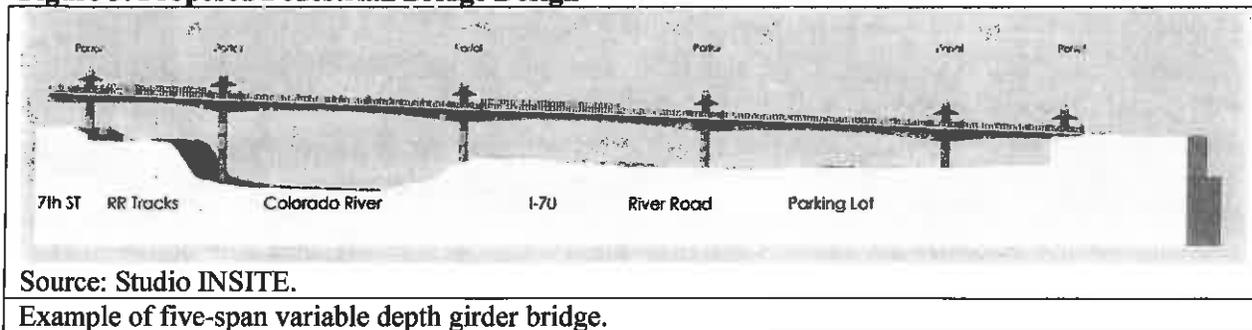
Under the current level of preliminary design, the bridge deck would be wider and flatter than the existing deck, and would have no overhead structure. It would cross the Colorado River on a straight alignment in a similar location as the existing bridge, with the northern touchdown point located slightly west of the existing touchdown point. It would include up to six piers, with no piers in the river. The bridge would feature pedestrian overlooks at bridge portals, either solid or see-through side railings, and lighting for safety purposes along the bridge and at bridge connections. This bridge type would have a lower profile than the other bridge options previously considered. For more information, see Figures 3 and 4 below. Note that these figures depict examples of aesthetic treatments under consideration, and the pier locations, size, materials and aesthetic treatments shown are not necessarily representative of how the final bridge design will appear. The historic and architectural context of the area will be considered when bridge materials and/or aesthetic bridge treatments are selected. Using the established Context Sensitive Solutions process, CDOT will continue to work with stakeholders to identify opportunities for aesthetic treatments in the design of the bridge, roadway, and sidewalk elements to reflect the materials and architectural style of Glenwood Springs' small town character and historic structures, as well as the visual and aesthetic goals and objectives provided in the I-70 Mountain Corridor Aesthetic."

The proposed pedestrian bridge type would be more visually prominent than the *existing* pedestrian bridge, but will not be as visually prominent or intrusive on views of surrounding hillsides as the bridge designs previously under consideration. The pedestrian bridge type would result in a visual change to the overall setting of the area, and the dual elevators and stairway access at the southern end of the new pedestrian bridge would be partially visible from Grand Avenue. CDOT has determined that the proposed pedestrian bridge design will not have a direct effect on the train station—in other words the

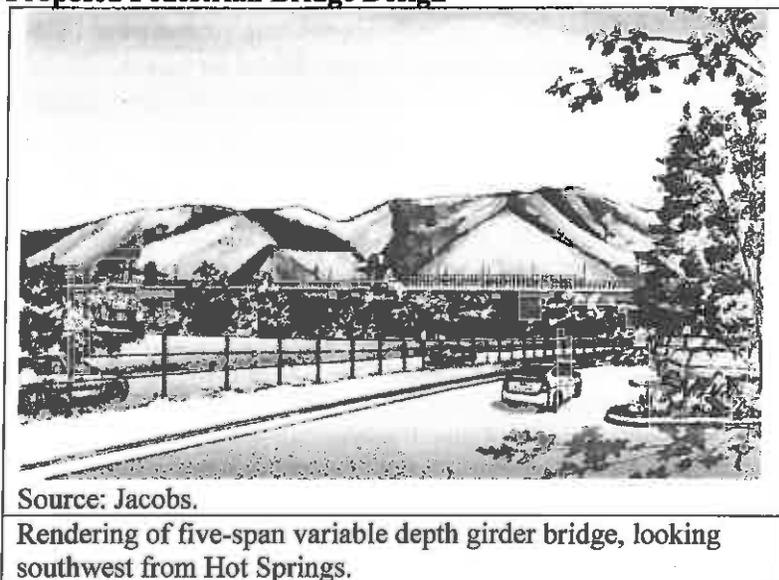


train station property will not be physically affected by the construction of the new pedestrian bridge. There are indirect effects based on visual changes in the views to and from the station. However, based on current design information CDOT has determined that the proposed pedestrian bridge will result in *no adverse effect* to the train station. We will consult with you as more aesthetic and design information is developed.

**Figure 3: Proposed Pedestrian Bridge Design**



**Figure 4: View of Proposed Pedestrian Bridge Design**



As a potential Section 106 consulting party, we welcome your comments on this information. Should you choose to respond we request that you do so within 30 days of receipt of these materials. If we do not hear from you in that time frame we will assume you do not plan to comment. If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Ms. Daniels  
April 2, 2014  
Page 9 of 12

Very truly yours,

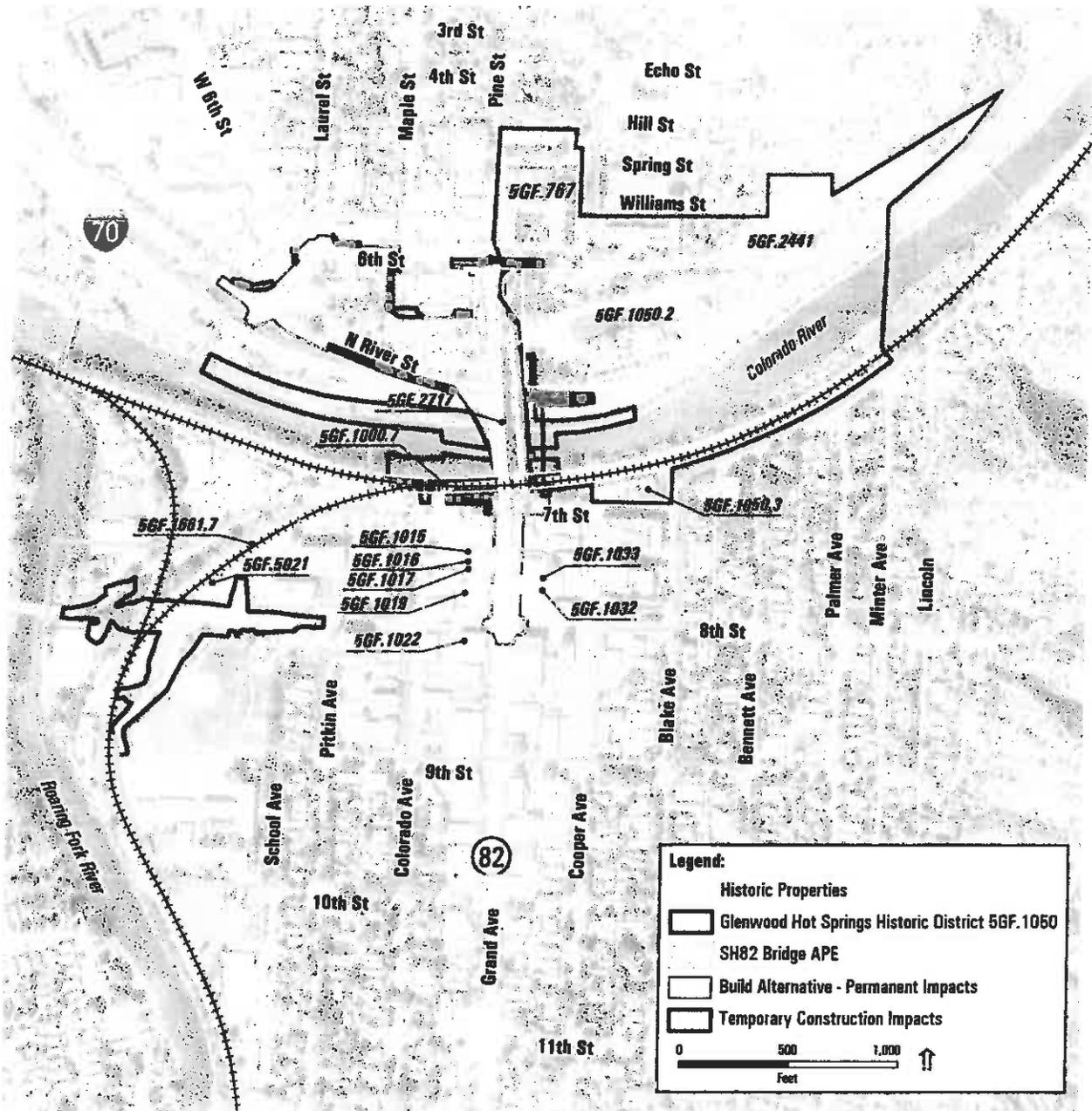


Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Revised APE map  
Proposed 8<sup>th</sup> Street Construction Detour graphic  
Site Forms  
Historic context  
Easement Graphic and Table

cc: Michael Vanderhoof, CDOT Region 3

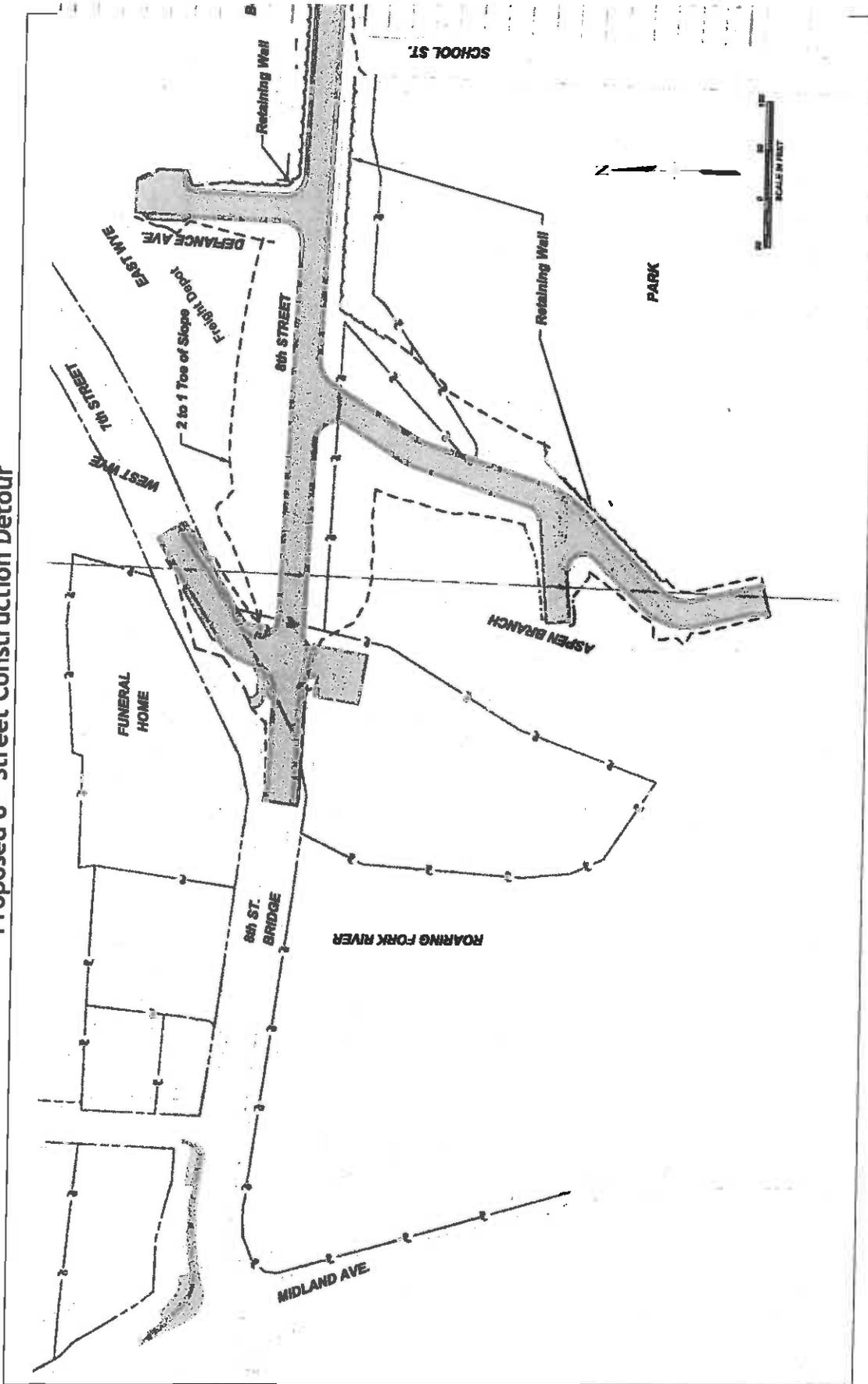




Revised Area of Potential Effects



### Proposed 8th Street Construction Detour







**COLORADO**

**Department of Transportation**

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

April 2, 2014

Ms. Gretchen Ricehill  
City of Glenwood Springs  
Historic Preservation Commission  
101 8<sup>th</sup> Street  
Glenwood Springs, CO 81601

**SUBJECT:** Additional Information on Determinations of Eligibility and Effect, Project FBR 0821-094, SH 82/Grand Ave. Bridge Replacement EA, Glenwood Springs

Dear Ms. Ricehill:

Your office initially reviewed eligibility and effect determinations for this project in August 2013. You requested additional information about the project, which we have provided in this submittal. In addition, the scope of the project has changed to include an extension of 8<sup>th</sup> Street to the existing 8<sup>th</sup> Street bridge over the Roaring Fork River as a temporary traffic detour during the approximate 60-day period when the Grand Avenue (SH 82) bridge would be fully closed to traffic during construction. Construction of the 8<sup>th</sup> Street detour would temporarily remove and require excavation of portions of the railroad tracks and railbed that currently traverse the area where 8<sup>th</sup> Street would be extended. See the attached graphic entitled "Proposed 8<sup>th</sup> Street Construction Detour" for more information.

This submittal includes the following information:

1. Responses to the Glenwood Springs Historic Preservation Commission August 15, 2013 letter
2. Revised Area of Potential Effect (APE)
3. New Eligibility and Effect Determinations for the Denver & Rio Grande Railroad Aspen Branch (5GF1661) and the associated freight depot (5GF5021).
4. Easements and aerial easements for the project

### **RESPONSES TO GLENWOOD SPRINGS HISTORIC PRESERVATION COMMISSION (GSHPC)**

**Response to GSHPC Comment #1:** Your letter requested clarification regarding why properties along Grand Avenue from 8<sup>th</sup> to 9<sup>th</sup> Streets and along the 200 and 300 blocks of 7<sup>th</sup> Street are included in the APE boundary but were not documented. Depending on the project scope and resource base, CDOT often includes properties within APE boundaries that are not documented for eligibility. Although the properties you noted are within the APE boundary, resources in those areas were not surveyed for NRHP eligibility and potential project effects either because they were not anticipated to experience direct or indirect effects due to their distance from proposed improvements, or because they are less than 45 years old. The properties along Grand Avenue from 8<sup>th</sup> to 9<sup>th</sup> Street were not documented because they extended just beyond the limit of the build alternative permanent impacts. On 7<sup>th</sup> Street, the property at 702 Grand Avenue (corner of Grand Avenue and 7<sup>th</sup>) was evaluated, but the properties fronting 7<sup>th</sup> Street to the east were not.

**Response to GSHPC Comment #2:** You requested more information regarding CDOT's evaluation of potential seismic impacts to properties in the vicinity of demolition and construction activities. As noted

in our August 2013 submittal, properties 5GF1050.2, 5F1050.3, and 5GF767 would experience temporary effects of increased dust, noise and vibration during construction, which was considered in the determinations of effect. CDOT is mindful of the sensitive nature of the historic resources in the vicinity of the project. These properties were considered during the design process, and measures to avoid and minimize direct and indirect impacts to these resources were incorporated into the preliminary design of the project.

Pile driving would be required for construction of highway and pedestrian bridge piers. This construction activity would be the loudest of the construction operations and present the most potential for vibration impacts. However, at the current level of design, the exact areas where pile driving would occur and the technique(s) that would be employed have not been determined. The final design process will consider several factors, including geological/soil densities and composition, and proximity of historic resources to determine final pile driving locations and pile driving techniques that would be employed in order to protect sensitive properties from adverse vibratory effects. CDOT determined that the project results in a determination of *adverse effect* for properties 5GF1015, 5GF1016, 5GF1017, 5GF1019, 5GF1033 and 5GF1032 based on the new bridge construction. For properties 5GF1050.2, 5GF1050.3 and 5GF767, CDOT determined there would be *no adverse effect*, and based on current information on temporary increased noise, dust, and vibration, this finding is still valid. We will consult with you when there is more information regarding pile driving and vibration impacts but given that these are temporary construction impacts, it is not anticipated that the effect findings for these resources will change.

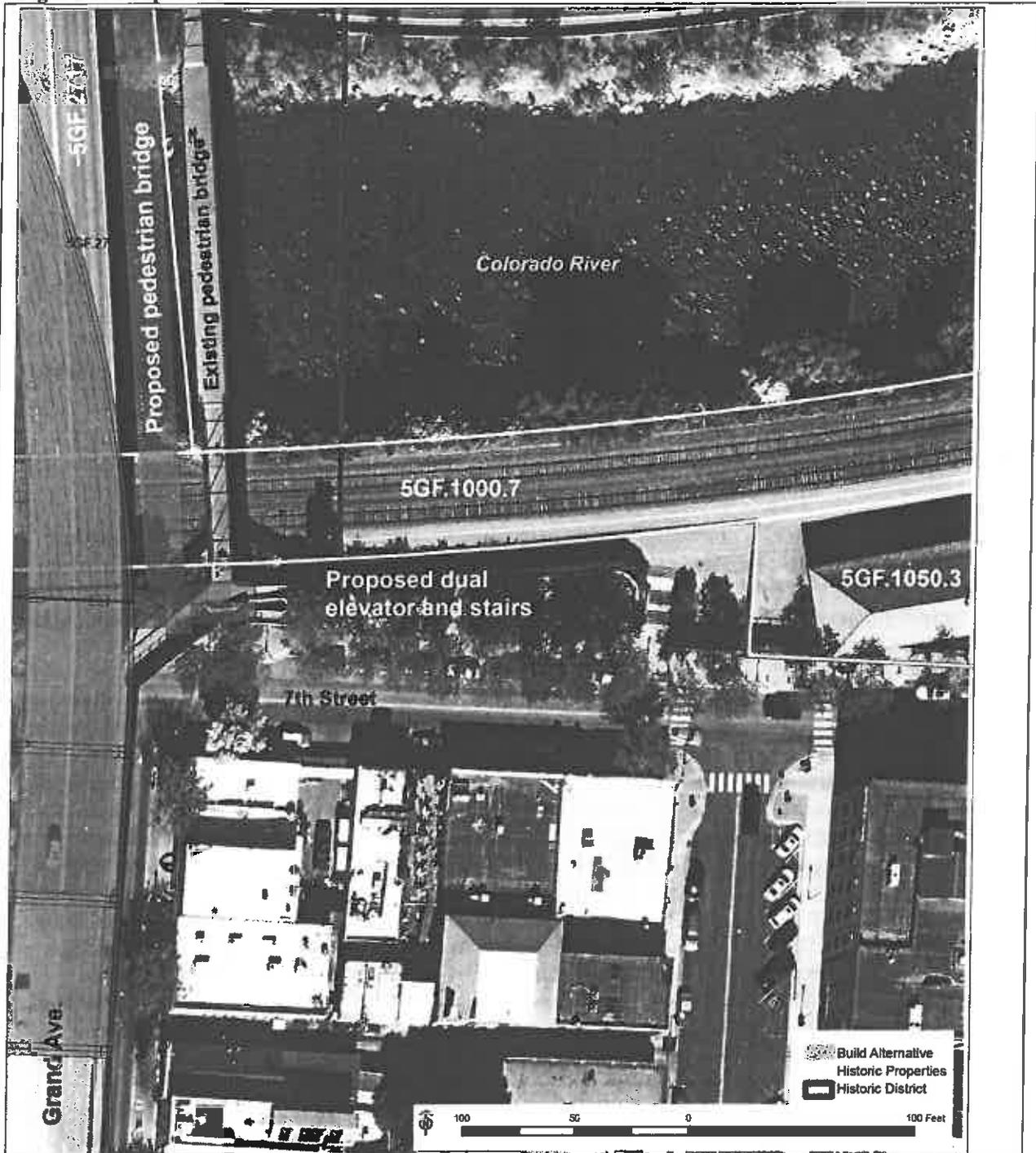
**Response to GSHPC Comment #3:** You requested more details regarding the visual effect of the scissor ramp on historic properties. Elevator and/or scissor ramp configurations were being considered for pedestrian bridge access at the time information was provided to you in August 2013. Since that time, the design has been modified and dual elevators are now proposed. The dual elevators would be located at the south end of the pedestrian bridge, with the stairway located on the east side of the elevator bank as noted in Figure 1 below. The elevator height would extend approximately 15 to 20 feet above the pedestrian bridge deck, depending on the roof design. The dual elevators/stairway would be located approximately 250 feet from the train station. This increased distance results in reduced views of the pedestrian bridge access from the train station, and is anticipated to result in minimal visual changes toward the train station. In addition, the dual elevators/stairway is anticipated to result in minimal visual changes to buildings along 7<sup>th</sup> Street and to views from 7<sup>th</sup> Street. Figure 1 shows the layout of the elevators and stairway in relation to 7<sup>th</sup> Street and the train station. Figure 2 shows the elevators and stairway as viewed from 7<sup>th</sup> Street looking west, and depicts a clock tower design for the elevator bank. However, design of the elevator bank and stairway will continue to be refined during the final design process in coordination with consulting parties and the City. The dual elevators will result in some new elements in the viewshed to and from the train station but this will not alter the qualities that make the train station significant. The project will have some temporary effects of increased dust, noise and vibration, but overall the proposed dual elevator access will not alter the characteristics of the property that qualify it for eligibility in a manner that would diminish its integrity. For these reasons, there is *no adverse effect* to the Denver & Rio Grande Railroad station (5GF1050.3).

**Response to GSHPC Comment #4:** Please refer to the response to Comment #3 above for more information about the proposed dual elevators.

**Response to GSHPC Comment #5:** You requested more information about the visual effect of a prominent pedestrian bridge design on the Denver & Rio Grande Railroad Station (5GF1050.3)



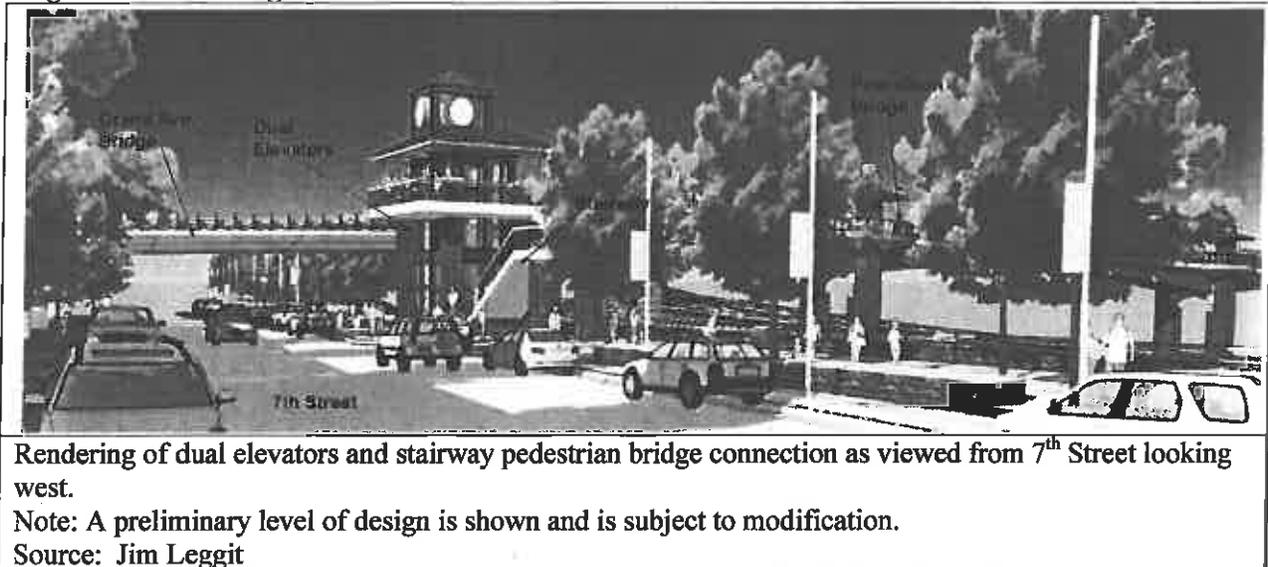
**Figure 1: Proposed Dual Elevator and Stairs**



Layout of dual elevators and stairway pedestrian bridge connection.  
Note: A preliminary level of design is shown and is subject to modification.  
Source: AMEC



**Figure 2: Rendering of Dual Elevator**

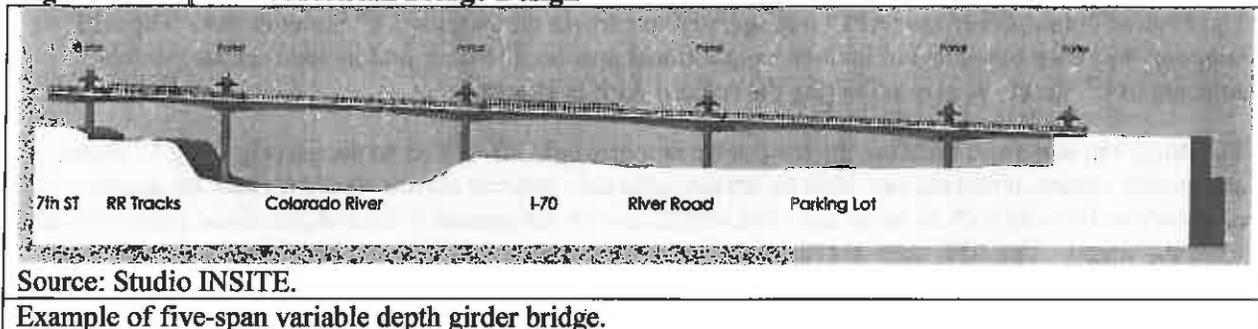


and disagreed with CDOT's determination of *no adverse effect* for this resource. When we consulted with you in August 2013, we were considering symmetrical cable, asymmetrical cable and arch bridge types for the pedestrian bridge design. Based on stakeholder input and other design considerations, these types were eliminated. The preferred pedestrian bridge type consists of a five-span variable depth girder bridge.

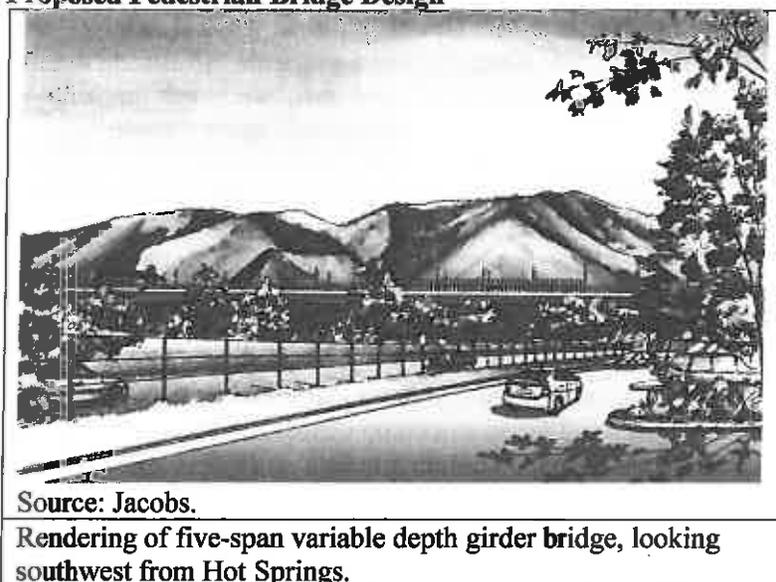
Under the current level of preliminary design, the bridge deck would be wider and flatter than the existing deck, and would have no overhead structure. It would cross the Colorado River on a straight alignment in a similar location as the existing bridge, with the northern touchdown point located slightly west of the existing touchdown point. It would include up to six piers, with no piers in the river. The bridge would feature pedestrian overlooks at bridge portals, either solid or see-through side railings, and lighting for safety purposes along the bridge and at bridge connections. This bridge type would have a lower profile than the other bridge options previously considered. For more information, see Figures 3 and 4 below. Note that these figures depict examples of aesthetic treatments under consideration, and the pier locations, size, materials and aesthetic treatments shown are not necessarily representative of how the final bridge design will appear. The historic and architectural context of the area will be considered when bridge materials and/or aesthetic bridge treatments are selected. Using the established Context Sensitive Solutions process, CDOT will continue to work with stakeholders to identify opportunities for aesthetic treatments in the design of the bridge, roadway, and sidewalk elements to reflect the materials and architectural style of Glenwood Springs' small town character and historic structures, as well as the visual and aesthetic goals and objectives provided in the I-70 Mountain Corridor Aesthetic."

The proposed pedestrian bridge type would be more visually prominent than the *existing* pedestrian bridge, but will not be as visually prominent or intrusive on views of surrounding hillsides as the bridge designs previously under consideration. The pedestrian bridge type would result in a visual change to the overall setting of the area, and the dual elevators and stairway access at the southern end of the new pedestrian bridge would be partially visible from Grand Avenue. CDOT has determined that the

**Figure 3: Proposed Pedestrian Bridge Design**



**Figure 4: View of Proposed Pedestrian Bridge Design**



proposed pedestrian bridge design will not have a direct effect on the train station—in other words the train station property will not be physically affected by the construction of the new pedestrian bridge. There are indirect effects based on visual changes in the views to and from the station. However, based on current design information CDOT has determined that the proposed pedestrian bridge will result in *no adverse effect* to the train station. We will consult with you as more aesthetic and design information is developed.

**Response to GSHPC Comment #6:** The omission of the APE in the legend has been noted and will be corrected.

**Response to GSHPC Comment #7:** You are correct that the most appropriate determination for the Citizens National Bank (5GF1022) should have been a finding of *no adverse effect*. This will be noted in the EA documentation.



### **REVISED AREA OF POTENTIAL EFFECTS**

The Area of Potential Effects (APE) was updated to address the proposed 8<sup>th</sup> Street detour. The APE boundary has been expanded to include an additional area on 8<sup>th</sup> Street, and includes entire parcels adjacent to 8<sup>th</sup> Street. A map reflecting the revised APE is attached.

The direct impacts from building the temporary detour would affect four to six parcels along 8<sup>th</sup> Street and nearby streets, with only one historic feature affected. Indirect effects would include temporary construction impacts such as noise and dust, which are not anticipated to extend past those properties that front the streets. The APE includes the parcels that have structures that were built at least 45 years ago adjacent to these streets, as shown on the attached APE map.

### **ELIGIBILITY DETERMINATIONS**

The proposed 8<sup>th</sup> Street detour results in effects to an additional property that was not evaluated in our initial survey: a segment of the Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7) and its associated freight depot (5GF5021). Site forms for these properties are attached, as is a historic context for railroad development in the Glenwood Springs/Aspen area, which was prepared because that topic was not specifically addressed in the initial Historic Resources Survey Report.

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** This railroad line was built in 1887 by the Denver & Rio Grande Railroad, and was the first rail line to reach the lucrative silver mining operations in Aspen. The entire Aspen Branch of the D&RG was determined eligible for inclusion on the National Register of Historic Places (NRHP) in 1988. In 2005, the 41-mile resource was determined to retain its eligibility even though the rails and railbed of much of the entire Aspen Branch line was removed for conversion to a recreational trail. The Aspen Branch of the Denver & Rio Grande Railroad is significant under Criterion A for its key role in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and for its association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The existence of portions of the tracks, ties, and grade in this segment supports the eligibility of the entire resource.

**5GF5021, Freight Depot:** The freight depot is significant for its association with the D&RG Aspen Branch. The depot was built by the D&RG to support its rail operations and is eligible under Criterion A for the reasons stated above for the railroad. The original freight depot on this site was removed and replaced with the current structure after 1947. That depot has been further modified over the years and is not eligible under Criterion C. The freight depot was documented separately but is an associated feature of the overall railroad, and is included in the boundary for the railroad.

### **EFFECT DETERMINATIONS**

**5GF1661.7, Denver & Rio Grande Railroad – Aspen Branch:** During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour. The detour route for regional traffic begins at Exit 114 on I-70 and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River, then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary “square about” for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad that currently extends between the two sections of 8<sup>th</sup> Street.



Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific Railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for two to four years.

To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. This would require temporary removal and storage of approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements, noted as 13 and 15 in the attached table of easement acquisitions, will be required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:

- West Wye Track – approximately 150 feet
- West Track of East Wye – approximately 165 feet
- Middle Track of East Wye – approximately 175 feet
- East Track of East Wye – approximately 175 feet

There are two options for the restoration of the railroad connection:

#### **Option 1**

The City of Glenwood Springs approached the CDOT study team about entering into a partnership regarding its planned 8<sup>th</sup> Street Extension project. If the City gains approval and funding, their project would involve building a new bridge at the railroad crossing that would restore the railroad connection. In anticipation of the City's project, CDOT would leave the 8<sup>th</sup> Street detour in place.

8th Street in downtown Glenwood Springs currently terminates just west of School Street. The City's 8th Street project would connect the 8th Street Bridge over the Roaring Fork River along a new alignment under the UPRR railroad tracks. This would involve lowering 8th Street between its intersection at Pitkin Avenue and the 8th Street Bridge, a distance of approximately 900 feet. The City has developed some alternatives for their 8<sup>th</sup> Street Extension project but has not selected an option, so it is unclear how the railroad connection will be restored. If the City's project moves forward, CDOT will disconnect this railroad segment (SGF1661.7) and not restore it as part of this undertaking. Under this option, the project results in an *adverse effect* to the overall Denver & Ro Grande—Aspen Branch because the railroad connection will not be restored and the integrity of the segment in the project area would be diminished. Although the freight depot (SGF5021) will not be directly affected by disconnection of the overall railroad, construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks farther to the west. These grading activities would not directly affect the freight depot. However, because the depot is considered part of that property, it is included in the overall adverse effect to the railroad.

CDOT proposes Level II documentation and interpretative mitigation to resolve the adverse effect for this resource. CDOT will continue to coordinate with you during the development of both the CDOT and City projects. Assuming Option 1 is implemented, CDOT plans to proceed with consultation to resolve the adverse effect to the railroad, including notification of the Advisory Council on Historic Preservation and development of mitigation through a Memorandum of Agreement (MOA).



## Option 2

If the City's project is *not* funded, CDOT will restore the railroad connection. The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition are anticipated to last between four and five months.

After completion of the project, the railbed will be restored and the overall railroad would still portray the key role it played in the settlement and commercial growth of Glenwood Springs and Colorado, as well as its association with settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor—all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to 5GF1661.7 under this option.

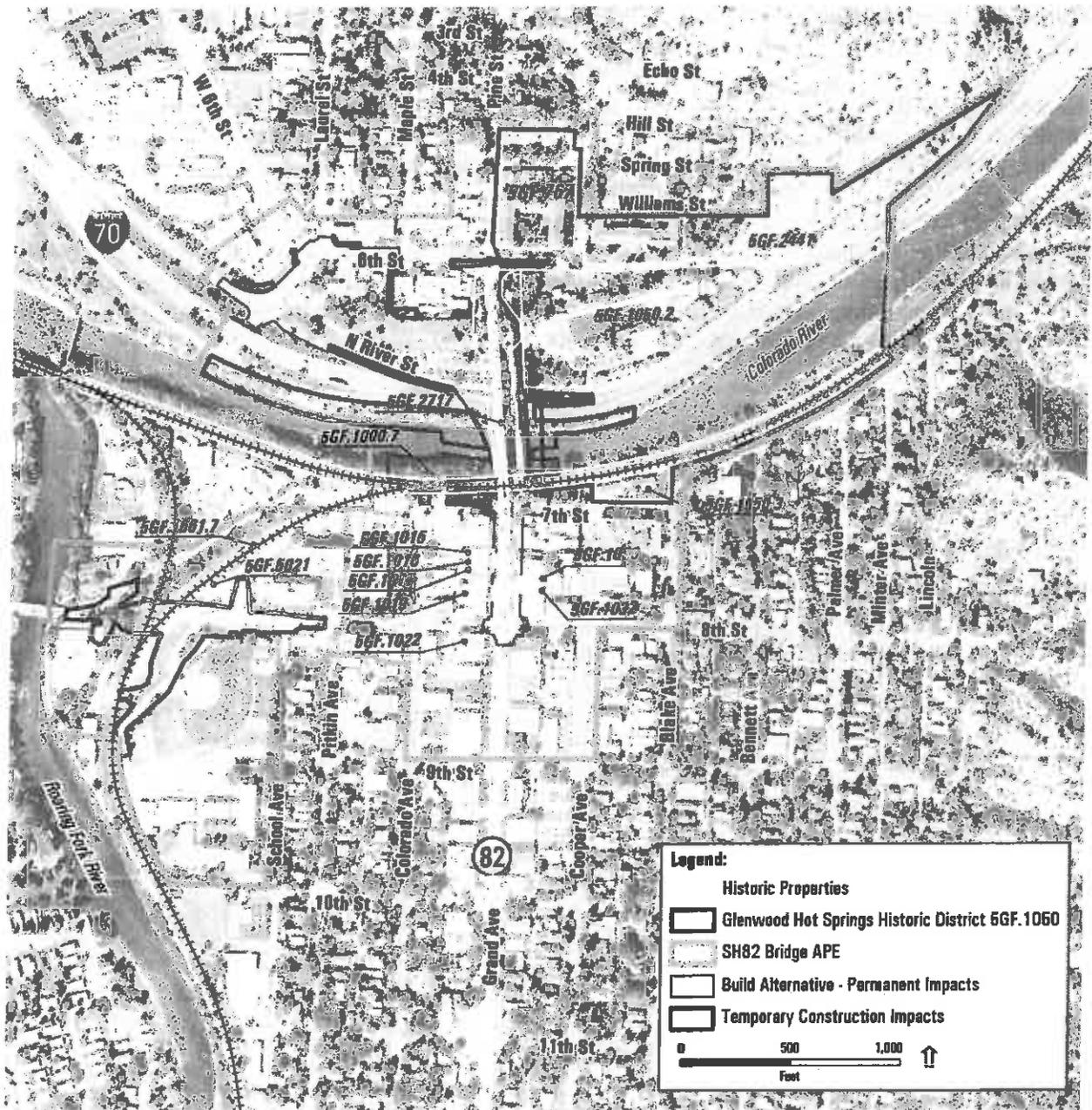
**5GF5021, Freight Depot:** No direct impacts would occur to the freight depot. Construction of the temporary detour on 8<sup>th</sup> Street would require grading to achieve appropriate grade for the road to pass under the railroad tracks further to the west, but that would have no direct effect to the depot. Construction and operation of the detour, and returning that route to preconstruction conditions is anticipated to last four to five months. Temporary effects would include dust and noise from construction activities. Because no direct impacts would occur, the freight depot would still convey its association with the D&RG. For this reason, the proposed project would result in *no adverse effect* to 5GF5021.

## **TEMPORARY AND PERMANENT AERIAL EASEMENTS/UPDATED EFFECTS DETERMINATIONS**

Since we previously consulted, more information is available about the types of easements needed for the project. Attached is a table and map showing the locations of proposed temporary easements and a permanent aerial easement. Most of the parcels requiring easements are non-historic resources, including properties 1, 2, 3, 4, 6, 7 and 16. The other properties are evaluated below:

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August submittal; the entire railroad is eligible and the segment retains integrity. CDOT will acquire one permanent aerial easement totaling about 0.01 acre from the railroad to widen the Grand Avenue Bridge. The aerial easement is noted on the attached graphic as a small dark blue sliver west of the existing bridge, south of the river, and located between the yellow build alternative and the cross-hatched orange shading for a temporary easement. This easement would not affect property access, signage, or the property owner's current use of the property. CDOT will also acquire four temporary easement locations from the railroad, noted as 11 and 12 on the attached table and graphic. These four easements are combined so 11 equals 0.15 acre and 12 is 0.04 acre. These easements will be used for construction of a temporary grade crossing as noted in our August submittal. The crossing will be constructed to provide access to the north side of the railroad for demolition of the existing bridge and construction of the new bridge. We consulted on this previously and you concurred with the finding of *no adverse effect*. The temporary grade crossing and associated easements will not alter the qualities of significance of the





Revised Area of Potential Effects



railroad and the finding of no adverse effect is still appropriate. Note that easements 9 and 10 are on railroad property, but are not within the boundary of the historic railroad.

**Glenwood Hot Springs Historic District (5GF1050):** The project will require a temporary easement totaling 0.14 acres on the legal hot springs property but this is outside the boundary of the historic hot springs district. This easement is noted as 8 on the attached table and graphic.

As a Section 106 consulting party, we welcome your comments on this information. Should you choose to respond we request that you do so within 30 days of receipt of these materials. If we do not hear from you in that time frame we will assume you do not plan to comment. If you have questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at (303) 512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,

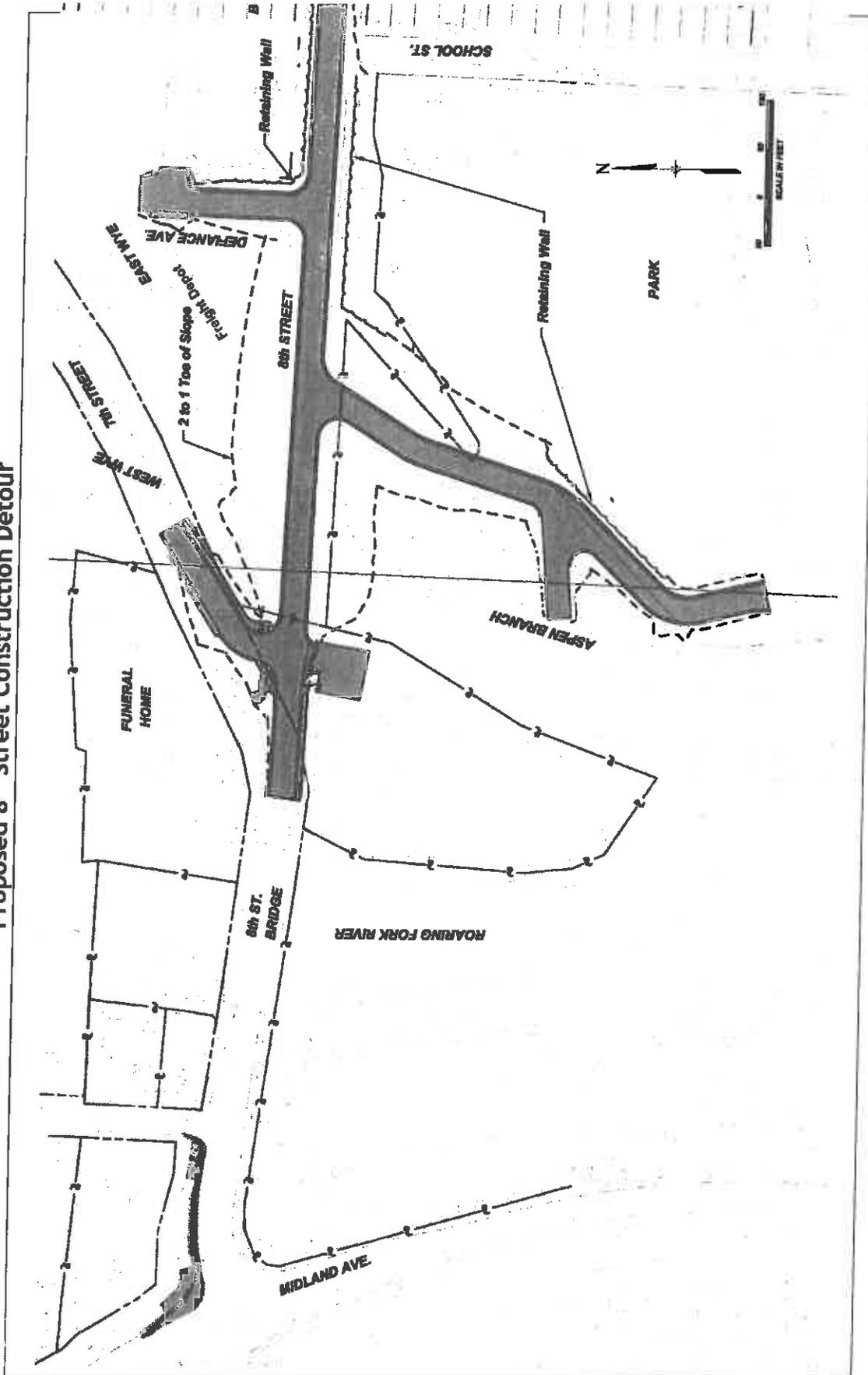
  
for Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Revised APE map  
Proposed 8<sup>th</sup> Street Construction Detour graphic  
Site Forms  
Historic context  
Easement Graphic and Table

cc: Michael Vanderhoof, CDOT Region 3  
Amy Pallante, Colorado SHPO



### Proposed 8th Street Construction Detour







April 24, 2014

Jane Hann  
Manager, Environmental Program Branch  
Colorado Department of Transportation  
Environmental Programs Branch  
4201 East Arkansas Avenue  
Denver, CO 80222

Re: Additional Information on Determinations of Eligibility and Effect, and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception, Project FBR 0821-094, SH 82/Grand Ave. Bridge Replacement (CHS #60723)

Dear Ms. Hann,

Thank you for your correspondence dated and received by email by our office on April 22, 2014 regarding the review of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106). After review of the submitted information, we do not object to the proposed Area of Potential Effects (APE) for the project.

After review of the survey information, we concur that the pedestrian bridge adjacent to the existing Grand Avenue Bridge is not eligible for the National Register of Historic Places. We also concur that segment 5GF.1661.7 retains integrity and supports the overall eligibility of resource 5GF.1661. We concur that resource 5GF.5021 is not individually eligible, but contributing to the overall eligibility of resource 5GF.1661.

After review of the scope of work and assessment of adverse effect, we concur with the recommended finding of *adverse effect* [36 CFR 800.5(a)(1)] under Section 106 to resource 5GF.1661, including segment 5GF.1661.7, for alternative Option 1. We concur with the recommended finding of *no adverse effect* [36 CFR 800.5(b)] under Section 106 to resource 5GF.1661, including segment 5GF.1661.7, for Option 2 providing the stipulations in your letter are completed.

We concur with the recommended finding of *no adverse effect* [36 CFR 800.5(b)] under Section 106 for resources 5GF.5021 and 5GF.1000, including segment 5GF.1000.7. At this time, we concur with the recommended finding of *no adverse effect* [36 CFR 800.5(b)] for 5GF.1050, including 5GF.1050.3, pending the review of the final design of the elevator bank and stairway.

Our office understands and agrees that CDOT and FHWA will apply the "Section 4(f) Evaluation and Approval for Transportation Projects that Have a Net Benefit to the Section 4(f) Property" for resource 5GF.1661 if the option to not restore the railroad connection is selected. Our office also agrees that the project meets the conditions of temporary occupancy as described in your letter.

If unidentified archaeological resources are discovered during construction, work must be interrupted until the resources have been evaluated in terms of the National Register criteria, 36 CRF 60.4, in consultation with this office. We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings.

Please note that our compliance letter does not end the 30-day review period provided to other consulting parties. If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Manager, at (303) 866-4678.

Sincerely,

A handwritten signature in black ink, appearing to read 'E.C. Nichols', written over a horizontal line.

Edward C. Nichols  
State Historic Preservation Officer

A handwritten mark or signature in black ink, consisting of a single, stylized character.



**COLORADO**  
Department of Transportation  
Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

August 27, 2014

Mr. Ed Nichols  
State Historic Preservation Officer  
Colorado Historical Society  
1200 Broadway  
Denver, CO 80203

**SUBJECT:** Additional Information, Section 106 Determinations of Eligibility and Effect and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception, CDOT Project FBR 0821-094, SH 82 / Grand Ave. Bridge Replacement Project Environmental Assessment, Glenwood Springs (CHS #60723)

Dear Mr. Nichols:

Your office initially reviewed eligibility and effect determinations for this project in August 2013 and reviewed additional information in March 2014. Since then, the scope of the project has changed to include some expanded and additional temporary construction easements on three historic properties identified in the Area of Potential Effects (APE). The APE outlined in our March 2014 correspondence has not changed as a result of these updates to easements. Effect determinations based on these changes to easements are included below. This submittal also includes some additional information regarding the existing pedestrian bridge and effects to one of the historic railroad segments.

**Pedestrian Bridge**

In our previous consultation efforts in August 2013 and March 2014, we indicated that the pedestrian bridge, located east of the existing highway bridge and built in 1985, will be replaced as part of this project. However, replacement of the pedestrian bridge may be considered as a design option under the Build Alternative. As such, it may or may not be replaced. This pedestrian bridge is not historic, but the design of the dual elevators associated with the new pedestrian bridge's southern access was assessed as part of the no adverse effect determination for the Denver & Rio Grande Railroad Station (5GF1050.3) as noted in our March 2014 consultation. If the pedestrian bridge is left in place, the dual elevator will not be part of the project scope and, therefore, the visual setting of the railroad station would not be affected. We will inform you once we know the final plan for the pedestrian bridge.

**UPDATED EFFECTS DETERMINATIONS**

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August 2013 submittal. The entire railroad is eligible and the segment retains integrity. In our March 2014 submittal, we indicated that CDOT would acquire one permanent aerial easement totaling about 0.01 acre from the railroad for the widened Grand Avenue Bridge and four areas of temporary easements totaling approximately 0.19 acre. The project scope now requires an additional 3,838 square-foot temporary aerial easement within the

historic property boundary east of the pedestrian bridge to accommodate overhead construction activities associated with replacement of the pedestrian bridge. This easement is separated into two areas on the attached figure (the 2,865 square-foot area and the 973 square-foot area) to make it clear which portion of the easement falls within the historic district boundary. The separate areas of this easement are outlined in black in the black cross-hatched area east of the existing pedestrian bridge. The other easements associated with the railroad and outlined in our previous consultation are still valid.

We consulted previously about the effect of the permanent aerial easement and temporary easements in March 2014 and you concurred with the finding of *no adverse effect*. The additional temporary aerial easement is a minor increase to what was initially expected and will not alter the qualities of significance of the railroad, and the finding of *no adverse effect* is still appropriate.

**Glenwood Hot Springs Historic District (5GF1050):** In our March 2014 submittal we consulted on an additional 0.14-acre temporary easement on the legal parcel of the hot springs property that was outside of the historic boundary. That easement has been expanded by approximately 2,370 square feet—this is outlined in black on the attached graphic. The area of the expanded easement is within the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) property north of I-70 but is outside of the historic district boundary. There are also two areas of enlarged easements near and in the north and south sides of the river that fall within the historic district boundary. On the north side of the river, the easement is approximately 14,795 square feet. It would serve as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70 and would include activities such as grading and operation of heavy machinery. On the south side of the river, the easement is approximately 2,625 square feet. It would serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with demolishing and building the new pedestrian bridge. This would include operation of heavy machinery. Also, an additional temporary aerial easement is required over the railroad (5GF1000.7), of which approximately 973 square feet falls within the historic district boundary. This easement would accommodate overhead construction activities associated with replacement of the pedestrian bridge. The additional temporary easements are outlined in black within the yellow-shaded area indicating the historic district on the attached graphic. Because these are temporary easements, there will be a temporary use of the land during construction but the qualities that make the district significant will not be affected by the easements and there will be *no adverse effect*.

**Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2):** There is an enlarged temporary easement north of I-70 that encroaches into the historic boundary of the Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2) but does not encroach into the overall Glenwood Hot Springs historic district (5GF1050). The easement would be approximately 2,370 square feet and is outlined in black on the attached graphic. There is a slight discrepancy between the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) and the overall historic district boundary (5GF1050). The bathhouse and natatorium (5GF1050.2) was found to be individually eligible in 1993 but a property boundary was not established at that time. In our August 2013 submittal, we identified the boundary of this property so that it encompassed the features outlined on the plan shape of the original site form. This boundary includes the natatorium, bathhouse, therapy pool, kiddie pool, cocktail spring, shed, and Yampa spring but excludes the pedestrian bridge. In our August 2013 consultation, we also revised the boundary of the overall Hot Springs historic district to include the Hotel Colorado, the Glenwood Springs Hydroelectric Plant, the Yampah

Hot Springs Vapor Caves, a segment of the Denver & Rio Grande railroad, and the Denver & Rio Grande Railroad Station but to exclude the pedestrian bridge. The boundary also includes all but a small area of the bathhouse and natatorium resource that is primarily used as a parking lot. This small area of the bathhouse and natatorium boundary that is outside the overall hot springs historic district will require a 2,730 square foot temporary easement as noted above. This is an easement that will be used temporarily during construction in an existing paved parking area that does not contain any of the major features of the bathhouse and natatorium property. This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work. The qualities that make this property significant will not be altered or diminished and there will be *no adverse effect*.

#### **Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7)**

In our March 28, 2014 consultation letter, we outlined two options for effects to the Denver & Rio Grande Railroad Aspen Branch segment 5GF1661.7 based on the 8<sup>th</sup> street detour. Per our March 2014 consultation, Option 1 involved the removal of a section of the railroad segment for an 8<sup>th</sup> street detour and not replacing the rails in anticipation of the City of Glenwood Springs 8<sup>th</sup> Street Extension project. Option 2 involved temporarily removing the railroad segment and restoring the connection once the CDOT project was completed. FHWA and CDOT met with Amy Pallante of your office to discuss these options and the possibility of using the Section 4(f) Net Benefit for Option 1.

Since then, Option 1 is no longer under consideration and instead CDOT is moving forward with a modified version of Option 2. For your convenience we have included below, and in italicized text, the description of Option 2 excerpted from our March 2014 letter. Your office concurred that Option 2 would result in *no adverse effect* to the railroad.

*During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour for the project. The detour route for regional traffic begins at Exit 114 on I-70, and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary "square about" for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad - Aspen Branch that currently extends between the two sections of 8<sup>th</sup> Street.*

*Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for an estimated two to four years.*

*To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. Under both original detour options, this would require temporary removal and storage of between approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements will be*

*required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:*

- *West Wye Track - approximately 150 feet*
- *West Track of East Wye - approximately 165 feet*
- *Middle Track of East Wye - approximately 175 feet*
- *East Track of East Wye - approximately 175 feet*

*The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the Grand Avenue bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks would be returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition is anticipated to last between five and six months*

*After completion of the project, the railbed will be restored and the overall railroad would still portray the key role that the railroad played in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and would continue to represent the railroad's association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in no adverse effect to this resource under this option.*

Under the modified Option 2, the work involves a shallow cut to the railroad that will not require the level of earthwork associated with the deep cut planned for the initial Option 2. The same number of tracks would be temporarily removed, but the length of track removed would be reduced, as summarized below:

- *West Wye Track - approximately 102 feet*
- *West Track of East Wye - approximately 73 feet*
- *Middle Track of East Wye - approximately 76 feet*
- *East Track of East Wye - approximately 70 feet*

Excavation of the existing rail bed/berm to accommodate construction of the temporary two-lane paved roadway would still occur, and a temporary easement would still be required within the railroad boundary, as shown in attached Figure 2.

As we noted previously for Option 2, the rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to this resource under this option.

#### **Section 4(f) Temporary Occupancy Exception**

in our August 2013 and March 2014 we consulted with you regarding the application of the Section 4(f) temporary occupancy exception as outlined in 23 CFR 774.13(d). In the August 2013 consultation, we applied the criteria to the Glenwood Hot Springs Historic District (5GF1050) and its contributing properties (5GF767, 5GF1000.7, 5GF1050.2, 5GF1050.3, 5GF2441, 5GF1258). In our March 2014 submittal we outlined some additional easements for the railroad and hot springs district but did not apply the temporary occupancy to those changes to the property. We are updating the application of temporary occupancy for these properties based on the updates to temporary easements outlined in our March 2014 correspondence and in this letter.

CDOT has determined that this project may meet the five criteria for the temporary occupancy exception as outlined in 23 CFR 774.13(d) with regard to 5GF1050, 5GF1050.2, 5GF2441, 5GF1000.7, and 5GF1050.3. A temporary occupancy evaluation for the Denver & Rio Grande Aspen Branch (5GF1661.7) and Freight Depot (5GF5021) was included in our March 2014 letter. Since there have been some changes to the effect to this resource due to the modified Option 2, we have re-evaluated the railroad in this submittal. Criterion 5 of this exception requires documented agreement of the official with jurisdiction (SHPO) regarding the application of this exception. The five criteria for temporary occupancy are listed below:

#### **Criteria for Temporary Occupancy:**

- 1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.**

The Glenwood Hot Springs Historic District (5GF1050) includes contributing features that are also individually eligible (5GF767, 5GF1000.7, 5GF1050.2, 5GF1050.3, 5GF2441, 5GF1258). Effects related to the district and its associated properties include temporary construction activities, a traffic detour along an existing roadway, visual changes related to construction of the new highway and pedestrian bridges, and temporary changes to the noise setting as detailed in the Section 106 findings for these properties. The historic district now involves an additional temporary easement of approximately 14,795 square feet along the north riverbank to serve as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70. It also involves an additional approximately 2,625 square foot temporary easement on the south side of the river that would serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with building the new pedestrian bridge. Further, it involves an approximate 973 square-foot temporary aerial easement to accommodate overhead construction activities associated with replacement of the pedestrian bridge.

With regard to the Denver & Rio Grande Railroad (5GF1000.7), which falls within the historic district boundary, the project requires a permanent aerial easement totaling approximately 0.01 acre and four temporary easements totaling approximately 0.19 acre that will be used for temporary grade crossings to access construction areas—these were noted in our March 2014 submittal. The railroad now also involves a new 3,838 square-foot temporary aerial easement east of the pedestrian bridge for overhead construction activities associated with demolition and construction of the new pedestrian bridge, including operation of heavy machinery. This easement is separated into two areas on the attached figure (the 2,865 square-foot area and the 973 square-foot area) to make it clear which portion of the easement falls within the historic district boundary.

As noted in our March 2014 submittal, a temporary easement of 0.14 acre would be required from the legal hot springs property, but would be outside the Glenwood Hot Spring Bathhouse and Natatorium boundary (5GF1050.2) and outside the historic district boundary (5GF1050). However, this temporary easement has been enlarged by approximately 2,730 square feet. The enlarged area falls within the historic boundary of the Glenwood Hot Spring Bathhouse and Natatorium property (5GF1050.2) but, due to a boundary discrepancy, is outside the larger Glenwood Hot Springs District boundary (5GF1050). This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work.

For the **Denver & Rio Grande Aspen Branch (5GF1661.7)** and **Freight Depot (5GF5021)**, the extension of 8<sup>th</sup> Street and the associated detour will be in operation during the closure of the Grand Avenue bridge. Excavation of the railroad bed and temporary removal of the railroad tracks and rails will take place during this time. Once the bridge is re-opened, the temporary construction detour will be removed and the railroad bed and tracks restored. Overall detour construction, operation, and restoring the detour area to its pre-construction condition will be less than the time required for the overall Grand Avenue bridge construction project.

There will be no change in ownership to the land within the hot springs, historic district, or railroad properties. With the exception of the permanent aerial easement over the railroad, the construction activities will result in temporary effects to these properties that will only last for the duration of the project.

2. **Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal.**

For the **Glenwood Hot Springs Historic District (5GF1050)** and associated properties (5GF767, 5GF1000.7, 5GF1050.2, 5GF1050.3, 5GF2441, 5GF1258), work includes temporary construction activity, a temporary detour on an existing roadway, and some visual and noise effects. Three temporary construction easements would be required within the Glenwood Hot Springs Historic District boundary. One would be required along the north bank of the river to lengthen the eastbound I-70 on-ramp. The second would be located within the south side of the river (construction pad built within the river) to accommodate demolition and construction of the pedestrian bridge. The third would be located over the railroad (5GF1050.7) to accommodate overhead construction activities associated with replacement of the pedestrian bridge.

For the **Denver & Rio Grande railroad (5GF1000.7)**, the work involves temporary construction easements for at-grade crossings within the railroad's historic boundary to provide access to construction areas. An additional temporary aerial easement within the railroad's historic boundary and east of the existing pedestrian bridge will facilitate overhead construction activities associated with demolition and construction of the pedestrian bridge.

For the **Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2)**, the work involves construction equipment operation, regrading and temporary shoring construction, construction of the adjacent pedestrian bridge piers, abutment and retaining wall construction, and bridge deck construction, all associated with the new highway and pedestrian bridges and outlined in our August 2013 consultation letter. As

noted in our March 2014 submittal, a temporary easement of 0.14 acre would be required from the legal hot springs property, but would be outside the Glenwood Hot Springs Bathhouse and Natatorium boundary (5GF1050.2) and outside the historic district boundary (5GF1050). This temporary easement has been enlarged by approximately 2,730 square feet. That enlarged area falls within the historic boundary of the Glenwood Hot Springs Bathhouse and Natatorium property (5GF1050.2) but, due to a boundary discrepancy, is outside the larger Glenwood Hot Springs District boundary (5GF1050). This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work.

With regard to the **Denver & Rio Grande Aspen Branch (5GF1661.7)** and **Freight Depot (5GF5021)**, the project involves the extension of 8<sup>th</sup> Street as a temporary detour while the Grand Avenue bridge is being replaced. The work will involve construction of the temporary detour that will extend across four segments of the Denver & Rio Grande railroad. This will involve removal of between 70 and 102 feet each of the four segments of track, and excavation of the railbed to accommodate construction of a temporary paved two-lane road. A temporary easement will be required of the railroad. Once the bridge replacement is completed, the detour will be removed and the railbed and tracks restored. This will involve backfill of the removed portion of the railbed and reinstalling the removed tracks. The nature of the change to the railroad property will be minimal and temporary and the railroad will be restored to its functionality.

The nature of any changes to the overall district, the Glenwood Hot Springs Bathhouse and Natatorium, and railroad properties will be minimal and temporary.

- 3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis.**

There will be no permanent adverse impacts to the **Glenwood Hot Springs Historic District (5GF1050)** or its associated properties. The effects to the district will be temporary and will not interfere with its features and attributes. The district is significant under NRHP Criteria A and C as a resort associated with the growth of Glenwood Springs and for its examples of Romanesque architecture. The contributing features have individual significance as well.

There will be no permanent adverse physical impacts to the **Denver & Rio Grande Railroad (5GF1000.7)**. Work that will occur within the railroad right-of-way (the historic boundary) includes temporary construction easements for at-grade crossings to provide access to construction areas. Also, a temporary aerial easement within the railroad's historic boundary east of the existing pedestrian bridge will be required to accommodate overhead construction activities associated with demolition and construction of the pedestrian bridge, which includes operation of heavy machinery. These elements will not interfere with the features and attributes that are part of the railroad and the district.

There will be no permanent adverse physical impacts to the **Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2)**. There are temporary construction impacts related to the construction of the highway bridge. An enlarged temporary construction easement would be required within the Glenwood Hot Springs Bathhouse and Natatorium

boundary, but this work will not interfere with the features and attributes of the bathhouse and natatorium property within the larger hot springs district.

Under the modified Option 2, there will be no permanent adverse physical impacts to the **Denver & Rio Grande Aspen Branch (5GF1661.7)** or the associated **Freight Depot (5GF5021)**. Segments of the four railroad lines within the railroad boundary will be removed temporarily to accommodate the temporary detour, but the railbed and tracks will be restored once the temporary detour is no longer needed. This temporary effect will not interfere with the features and attributes of the railroad property, which is significant under National Register Criterion A for its role in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and for its association with the settlement of the Roaring Fork Valley and 19<sup>th</sup> century development of Aspen's mining industry.

Findings of *no adverse effect* under Section 106 have been recommended for all of the railroad and the district and associated properties.

4. **The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project.**

The effects to the **Glenwood Springs Historic District (5GF1050)** and its contributing/individual eligible features include temporary construction activities and a temporary detour along 6<sup>th</sup> Street. As noted in previous consultation, there will be some changes to the views from the Historic District and associated Hot Springs pool based on the new pedestrian and highway bridges, but those structures already exist in the viewshed. The district involves a temporary easement along the north riverbank as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70. It also involves a temporary easement on the south side of the river to serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with building the new pedestrian bridge. It also involves a temporary aerial easement over the railroad (5GF1000.7) to accommodate overhead construction activities associated with replacement of the pedestrian bridge. For the overall district and its contributing/individually eligible features, the properties will be returned to a condition as good as that which existed prior to construction.

There will be work within the **Denver & Rio Grande Railroad (5GF1000.7)** right-of-way. Temporary easements will be required for at-grade crossings to provide access to construction areas associated with demolition and construction of the highway and pedestrian bridges. A permanent aerial easement will be required for the widened highway bridge. An additional temporary aerial easement east of the pedestrian bridge will be required for overhead construction activities associated with demolition and construction of the new pedestrian bridge, and include operation of heavy machinery. The land within the railroad's historic boundary will be returned to a condition as good as that which existed prior to construction, as noted in the Section 106 findings above.

There will be temporary construction activities within the boundary of the **Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2)** property. An enlarged temporary construction easement falls within the historic boundary of the Glenwood Hot Springs Bathhouse and Natatorium property but, due to a boundary discrepancy, is outside the larger Glenwood Hot Springs District boundary. This temporary easement is located in a paved parking lot would be used for temporary storage of materials and reconstruction of

the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work. Once the work is completed, this property will be returned to a condition as good as that which existed prior to construction.

For the Denver & Rio Grande Aspen Branch (5GF1661.7) and Freight Depot (5GF5021), the project involves the extension of 8<sup>th</sup> Street as a temporary detour while the Grand Avenue bridge is being replaced. The work will involve construction of the temporary detour that will extend across four segments of the Denver & Rio Grande railroad. This will involve removal of between 70 and 102 feet of track for each of the four segments of track, and excavation of the railbed to accommodate construction of a temporary paved two-lane road. Once the bridge replacement is completed, the detour will be removed and the railbed and tracks restored. This will involve backfill of the removed portion of the railbed and reinstalling the removed tracks. The tracks will be installed in their previous alignment and the connection restored. A temporary easement is needed for the work.

**5. There must be documented agreement of the official with jurisdiction over the Section 4(f) resource regarding the above conditions.**

Based on the information provided herein, we request your agreement that this project meets the conditions of temporary occupancy under 23 CFR 774.13(d) and that Section 4(f) does not apply.

We request your concurrence with the Determination of Eligibility and Effects and comments on the application of Section 4(f) Temporary Occupancy. Your response is necessary for the Federal Highway Administration's compliance with Section 106 of the National Historic Preservation Act (as amended) and with the Advisory Council on Historic Preservation's regulations. This information has also been forwarded to the consulting parties for review: Glenwood Springs Historic Preservation Commission, Frontier Historical Society, and Colorado Preservation Inc. We will notify you of any responses received from these groups.

Thank you for your attention to this matter. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at 303-512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,

*Vanessa Henderson*

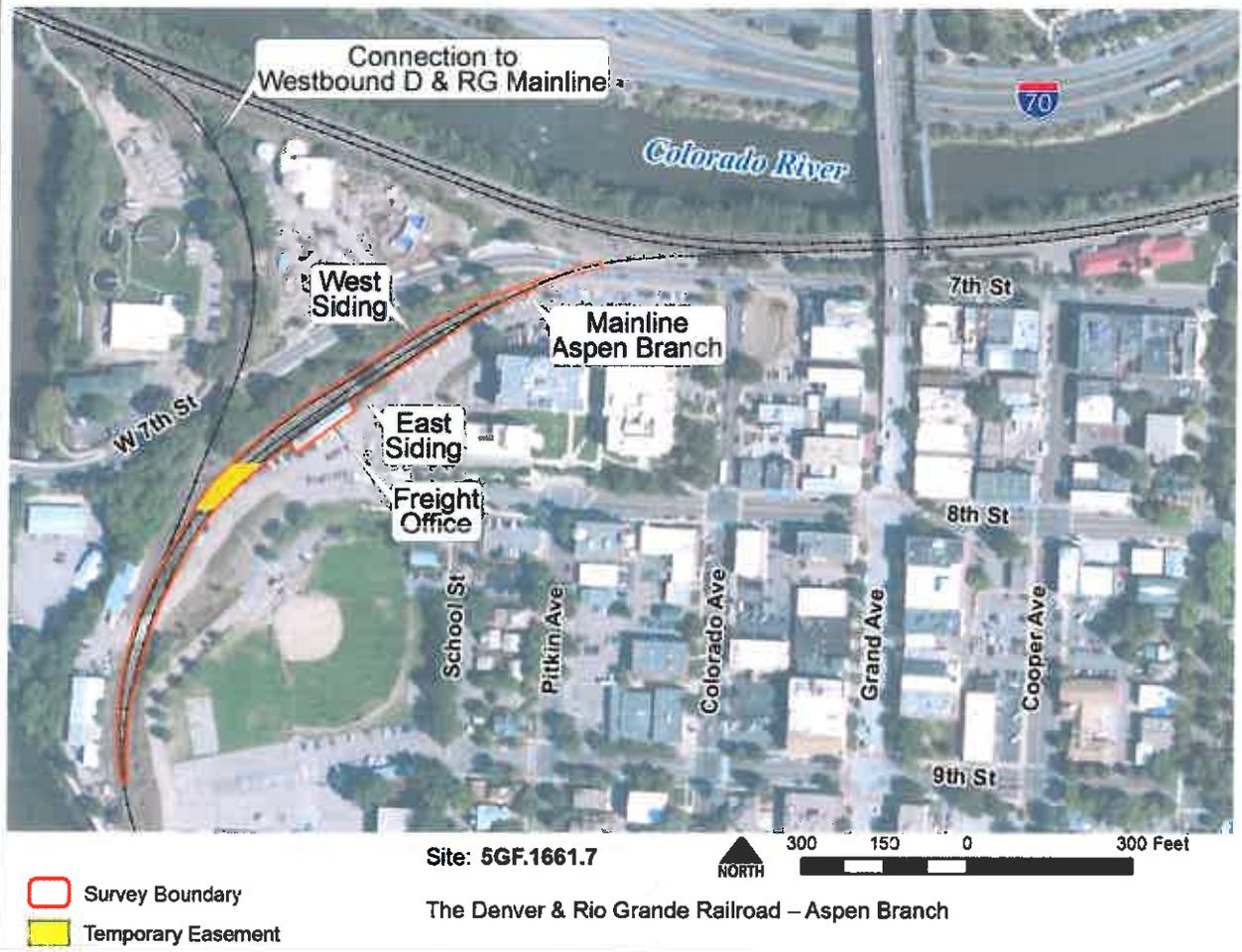
*JH*

Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Figures 1 and 2: Easement Graphics



Figure 2: Temporary Construction Easement: Denver & Rio Grande Railroad - Aspen Branch





Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

August 27, 2014

Ms. Rachel Parris  
Colorado Preservation Incorporated  
Emerson School  
1420 Ogden Street, Suite 104  
Denver, CO 80218

**SUBJECT: Additional Information, Section 106 Determinations of Eligibility and Effect CDOT Project FBR 0821-094, SH 82 / Grand Ave. Bridge Replacement Project Environmental Assessment, Glenwood Springs (CHS #60723)**

Dear Ms. Parris:

You were initially provided an opportunity to comment on eligibility and effect determinations for this project in August 2013 and were sent additional information in April 2014. Since then, the scope of the project has changed to include some expanded and additional temporary construction easements on three historic properties identified in the Area of Potential Effects (APE). The APE outlined in our April 2014 correspondence has not changed as a result of these updates to easements. Effect determinations based on these changes to easements are included below. This submittal also includes some additional information regarding the existing pedestrian bridge and effects to one of the historic railroad segments.

### **Pedestrian Bridge**

In our previous consultation efforts in August 2013 and April 2014, we indicated that the pedestrian bridge, located east of the existing highway bridge and built in 1985, will be replaced as part of this project. However, replacement of the pedestrian bridge may be considered as a design option under the Build Alternative. As such, it may or may not be replaced. This pedestrian bridge is not historic, but the design of the dual elevators associated with the new pedestrian bridge's southern access was assessed as part of the no adverse effect determination for the Denver & Rio Grande Railroad Station (5GF1050.3) as noted in our March 2014 consultation. If the pedestrian bridge is left in place, the dual elevator will not be part of the project scope and, therefore, the visual setting of the railroad station would not be affected. We will inform you once we know the final plan for the pedestrian bridge.

### **UPDATED EFFECTS DETERMINATIONS**

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August 2013 submittal. The entire railroad is eligible and the segment retains integrity. In our April 2014 submittal, we indicated that CDOT would acquire one permanent aerial easement totaling about 0.01 acre from the railroad for the widened Grand Avenue Bridge and four areas of temporary easements totaling approximately 0.19 acre. The project scope now requires an additional 3,838 square-foot temporary aerial easement within the

historic property boundary east of the pedestrian bridge to accommodate overhead construction activities associated with replacement of the pedestrian bridge. This easement is separated into two areas on the attached figure (the 2,865 square-foot area and the 973 square-foot area) to make it clear which portion of the easement falls within the historic district boundary. The separate areas of this easement are outlined in black in the black cross-hatched area east of the existing pedestrian bridge. The other easements associated with the railroad and outlined in our previous consultation are still valid.

We consulted previously about the effect of the permanent aerial easement and temporary easements in April 2014 and SHPO concurred with the finding of *no adverse effect*. The additional temporary aerial easement is a minor increase to what was initially expected and will not alter the qualities of significance of the railroad, and the finding of *no adverse effect* is still appropriate.

**Glenwood Hot Springs Historic District (5GF1050):** In our April 2014 submittal we consulted on an additional 0.14-acre temporary easement on the legal parcel of the hot springs property that was outside of the historic boundary. That easement has been expanded by approximately 2,370 square feet—this is outlined in black on the attached graphic. The area of the expanded easement is within the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) property north of I-70 but is outside of the historic district boundary. There are also two areas of enlarged easements near and in the north and south sides of the river that fall within the historic district boundary. On the north side of the river, the easement is approximately 14,795 square feet. It would serve as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70 and would include activities such as grading and operation of heavy machinery. On the south side of the river, the easement is approximately 2,625 square feet. It would serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with demolishing and building the new pedestrian bridge. This would include operation of heavy machinery. Also, an additional temporary aerial easement is required over the railroad (5GF1000.7), of which approximately 973 square feet falls within the historic district boundary. This easement would accommodate overhead construction activities associated with replacement of the pedestrian bridge. The additional temporary easements are outlined in black within the yellow-shaded area indicating the historic district on the attached graphic. Because these are temporary easements, there will be a temporary use of the land during construction but the qualities that make the district significant will not be affected by the easements and there will be *no adverse effect*.

**Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2):** There is an enlarged temporary easement north of I-70 that encroaches into the historic boundary of the Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2) but does not encroach into the overall Glenwood Hot Springs historic district (5GF1050). The easement would be approximately 2,370 square feet and is outlined in black on the attached graphic. There is a slight discrepancy between the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) and the overall historic district boundary (5GF1050). The bathhouse and natatorium (5GF1050.2) was found to be individually eligible in 1993 but a property boundary was not established at that time. In our August 2013 submittal, we identified the boundary of this property so that it encompassed the features outlined on the plan shape of the original site form. This boundary includes the natatorium, bathhouse, therapy pool, kiddie pool, cocktail spring, shed, and Yampa spring but excludes the pedestrian bridge. In our August 2013 consultation, we also revised the boundary of the overall Hot Springs historic district to include the Hotel Colorado, the Glenwood Springs Hydroelectric Plant, the Yampah

Hot Springs Vapor Caves, a segment of the Denver & Rio Grande railroad, and the Denver & Rio Grande Railroad Station but to exclude the pedestrian bridge. The boundary also includes all but a small area of the bathhouse and natatorium resource that is primarily used as a parking lot. This small area of the bathhouse and natatorium boundary that is outside the overall hot springs historic district will require a 2,730 square foot temporary easement as noted above. This is an easement that will be used temporarily during construction in an existing paved parking area that does not contain any of the major features of the bathhouse and natatorium property. This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work. The qualities that make this property significant will not be altered or diminished and there will be *no adverse effect*.

#### **Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7)**

In our April 2014 consultation letter, we outlined two options for effects to the Denver & Rio Grande Railroad Aspen Branch segment 5GF1661.7 based on the 8<sup>th</sup> street detour. Per our April 2014 consultation, Option 1 involved the removal of a section of the railroad segment for an 8<sup>th</sup> street detour and not replacing the rails in anticipation of the City of Glenwood Springs 8<sup>th</sup> Street Extension project. Option 2 involved temporarily removing the railroad segment and restoring the connection once the CDOT project was completed. FHWA and CDOT met with Amy Pallante of the SHPO office to discuss these options and the possibility of using the Section 4(f) Net Benefit for Option 1.

Since then, Option 1 is no longer under consideration and instead CDOT is moving forward with a modified version of Option 2. For your convenience we have included below, and in italicized text, the description of Option 2 excerpted from our April 2014 letter. The State Historic Preservation Office (SHPO) concurred that Option 2 would result in *no adverse effect* to the railroad.

*During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour for the project. The detour route for regional traffic begins at Exit 114 on I-70, and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary "square about" for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad - Aspen Branch that currently extends between the two sections of 8<sup>th</sup> Street.*

*Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for an estimated two to four years.*

*To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. Under both original detour options, this would require temporary removal and storage of between approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements will be*

*required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:*

- *West Wye Track - approximately 150 feet*
- *West Track of East Wye - approximately 165 feet*
- *Middle Track of East Wye - approximately 175 feet*
- *East Track of East Wye - approximately 175 feet*

*The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the Grand Avenue bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks would be returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition is anticipated to last between five and six months*

*After completion of the project, the railbed will be restored and the overall railroad would still portray the key role that the railroad played in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and would continue to represent the railroad's association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in no adverse effect to this resource under this option.*

Under the modified Option 2, the work involves a shallow cut to the railroad that will not require the level of earthwork associated with the deep cut planned for the initial Option 2. The same number of tracks would be temporarily removed, but the length of track removed would be reduced, as summarized below:

- *West Wye Track - approximately 102 feet*
- *West Track of East Wye - approximately 73 feet*
- *Middle Track of East Wye - approximately 76 feet*
- *East Track of East Wye - approximately 70 feet*

Excavation of the existing rail bed/berm to accommodate construction of the temporary two-lane paved roadway would still occur, and a temporary easement would still be required within the railroad boundary, as shown in attached Figure 2.

As we noted previously for Option 2, the rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to this resource under this option.

Ms. Parris  
August 27, 2014  
Page 5 of 7

As a consulting party for this project, we welcome your comments on these findings. Should you choose to respond, we request that you do so within 30 days of receipt of these materials. If we do not receive a response from you within that time frame, we will assume you do not plan to comment. Thank you for your attention to this matter. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at 303-512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,



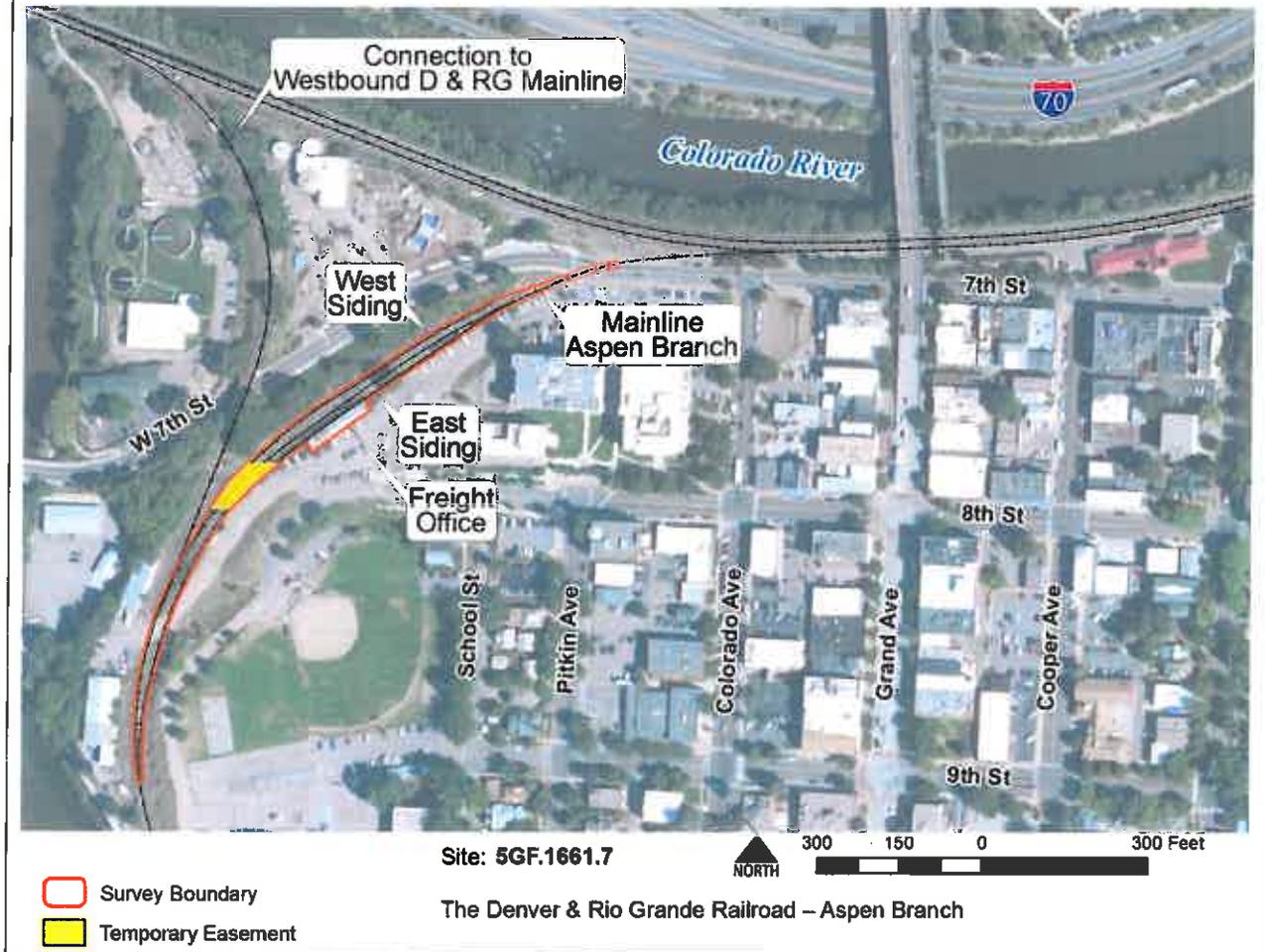
*For*

Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Figures 1 and 2: Easement Graphics



Figure 2: Temporary Construction Easement: Denver & Rio Grande Railroad - Aspen Branch





## **COLORADO**

**Department of Transportation**

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

August 27, 2014

Ms. Gretchen Ricehill  
City of Glenwood Springs  
Historic Preservation Commission  
101 8<sup>th</sup> Street  
Glenwood Springs, CO 81601

**SUBJECT: Additional Information, Section 106 Determinations of Eligibility and Effect, CDOT Project FBR 0821-094, SH 82 / Grand Ave. Bridge Replacement Project Environmental Assessment, Glenwood Springs (CHS #60723)**

Dear Ms. Ricehill:

Your organization initially reviewed eligibility and effect determinations for this project in August 2013 and reviewed additional information in April 2014. Since then, the scope of the project has changed to include some expanded and additional temporary construction easements on three historic properties identified in the Area of Potential Effects (APE). The APE outlined in our April 2014 correspondence has not changed as a result of these updates to easements. Effect determinations based on these changes to easements are included below. This submittal also includes some additional information regarding the existing pedestrian bridge and effects to one of the historic railroad segments.

### **Pedestrian Bridge**

In our previous consultation efforts in August 2013 and April 2014, we indicated that the pedestrian bridge, located east of the existing highway bridge and built in 1985, will be replaced as part of this project. However, replacement of the pedestrian bridge may be considered as a design option under the Build Alternative. As such, it may or may not be replaced. This pedestrian bridge is not historic, but the design of the dual elevators associated with the new pedestrian bridge's southern access was assessed as part of the no adverse effect determination for the Denver & Rio Grande Railroad Station (5GF1050.3) as noted in our March 2014 consultation. If the pedestrian bridge is left in place, the dual elevator will not be part of the project scope and, therefore, the visual setting of the railroad station would not be affected. We will inform you once we know the final plan for the pedestrian bridge.

### **UPDATED EFFECTS DETERMINATIONS**

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August 2013 submittal. The entire railroad is eligible and the segment retains integrity. In our April 2014 submittal, we indicated that CDOT would acquire one permanent aerial easement totaling about 0.01 acre from the railroad for the widened Grand Avenue Bridge and four areas of temporary easements totaling approximately 0.19 acre. The project scope now requires an additional 3,838 square-foot temporary aerial easement within the historic property boundary east of the pedestrian bridge to accommodate overhead

construction activities associated with replacement of the pedestrian bridge. This easement is separated into two areas on the attached figure (the 2,865 square-foot area and the 973 square-foot area) to make it clear which portion of the easement falls within the historic district boundary. The separate areas of this easement are outlined in black in the black cross-hatched area east of the existing pedestrian bridge. The other easements associated with the railroad and outlined in our previous consultation are still valid.

We consulted previously about the effect of the permanent aerial easement and temporary easements in April 2014 and SHPO concurred with the finding of *no adverse effect*. The additional temporary aerial easement is a minor increase to what was initially expected and will not alter the qualities of significance of the railroad, and the finding of *no adverse effect* is still appropriate.

**Glenwood Hot Springs Historic District (5GF1050):** In our April 2014 submittal we consulted on an additional 0.14-acre temporary easement on the legal parcel of the hot springs property that was outside of the historic boundary. That easement has been expanded by approximately 2,370 square feet—this is outlined in black on the attached graphic. The area of the expanded easement is within the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) property north of I-70 but is outside of the historic district boundary. There are also two areas of enlarged easements near and in the north and south sides of the river that fall within the historic district boundary. On the north side of the river, the easement is approximately 14,795 square feet. It would serve as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70 and would include activities such as grading and operation of heavy machinery. On the south side of the river, the easement is approximately 2,625 square feet. It would serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with demolishing and building the new pedestrian bridge. This would include operation of heavy machinery. Also, an additional temporary aerial easement is required over the railroad (5GF1000.7), of which approximately 973 square feet falls within the historic district boundary. This easement would accommodate overhead construction activities associated with replacement of the pedestrian bridge. The additional temporary easements are outlined in black within the yellow-shaded area indicating the historic district on the attached graphic. Because these are temporary easements, there will be a temporary use of the land during construction but the qualities that make the district significant will not be affected by the easements and there will be *no adverse effect*.

**Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2):** There is an enlarged temporary easement north of I-70 that encroaches into the historic boundary of the Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2) but does not encroach into the overall Glenwood Hot Springs historic district (5GF1050). The easement would be approximately 2,370 square feet and is outlined in black on the attached graphic. There is a slight discrepancy between the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) and the overall historic district boundary (5GF1050). The bathhouse and natatorium (5GF1050.2) was found to be individually eligible in 1993 but a property boundary was not established at that time. In our August 2013 submittal, we identified the boundary of this property so that it encompassed the features outlined on the plan shape of the original site form. This boundary includes the natatorium, bathhouse, therapy pool, kiddie pool, cocktail spring, shed, and Yampa spring but excludes the pedestrian bridge. In our August 2013 consultation, we also revised the boundary of the overall Hot Springs historic district to include the Hotel Colorado, the Glenwood Springs Hydroelectric Plant, the Yampah Hot Springs Vapor Caves, a segment of the Denver & Rio Grande railroad, and the Denver &

Rio Grande Railroad Station but to exclude the pedestrian bridge. The boundary also includes all but a small area of the bathhouse and natatorium resource that is primarily used as a parking lot. This small area of the bathhouse and natatorium boundary that is outside the overall hot springs historic district will require a 2,730 square foot temporary easement as noted above. This is an easement that will be used temporarily during construction in an existing paved parking area that does not contain any of the major features of the bathhouse and natatorium property. This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work. The qualities that make this property significant will not be altered or diminished and there will be *no adverse effect*.

#### **Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7)**

In our April 2014 consultation letter, we outlined two options for effects to the Denver & Rio Grande Railroad Aspen Branch segment 5GF1661.7 based on the 8<sup>th</sup> street detour. Per our April 2014 consultation, Option 1 involved the removal of a section of the railroad segment for an 8<sup>th</sup> street detour and not replacing the rails in anticipation of the City of Glenwood Springs 8<sup>th</sup> Street Extension project. Option 2 involved temporarily removing the railroad segment and restoring the connection once the CDOT project was completed. FHWA and CDOT met with Amy Pallante of the SHPO office to discuss these options and the possibility of using the Section 4(f) Net Benefit for Option 1.

Since then, Option 1 is no longer under consideration and instead CDOT is moving forward with a modified version of Option 2. For your convenience we have included below, and in italicized text, the description of Option 2 excerpted from our April 2014 letter. The State Historic Preservation Office (SHPO) concurred that Option 2 would result in *no adverse effect* to the railroad.

*During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour for the project. The detour route for regional traffic begins at Exit 114 on I-70, and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary "square about" for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad - Aspen Branch that currently extends between the two sections of 8<sup>th</sup> Street.*

*Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has tracks rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for an estimated two to four years.*

*To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. Under both original detour options, this would require temporary removal and storage of between approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements will be*

*required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:*

- *West Wye Track - approximately 150 feet*
- *West Track of East Wye - approximately 165 feet*
- *Middle Track of East Wye - approximately 175 feet*
- *East Track of East Wye - approximately 175 feet*

*The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the Grand Avenue bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks would be returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition is anticipated to last between five and six months*

*After completion of the project, the railbed will be restored and the overall railroad would still portray the key role that the railroad played in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and would continue to represent the railroad's association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in no adverse effect to this resource under this option.*

Under the modified Option 2, the work involves a shallow cut to the railroad that will not require the level of earthwork associated with the deep cut planned for the initial Option 2. The same number of tracks would be temporarily removed, but the length of track removed would be reduced, as summarized below:

- *West Wye Track - approximately 102 feet*
- *West Track of East Wye - approximately 73 feet*
- *Middle Track of East Wye - approximately 76 feet*
- *East Track of East Wye - approximately 70 feet*

Excavation of the existing rail bed/berm to accommodate construction of the temporary two-lane paved roadway would still occur, and a temporary easement would still be required within the railroad boundary, as shown in attached Figure 2.

As we noted previously for Option 2, the rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to this resource under this option.

As a consulting party for this project, we welcome your comments on these findings. Should you choose to respond, we request that you do so within 30 days of receipt of these

Ms. Ricchill  
August 27, 2014  
Page 5 of 7

materials. If we do not receive a response from you within that time frame, we will assume you do not plan to comment. Thank you for your attention to this matter. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at 303-512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,

  
FOIA

Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Figures 1 and 2: Easement Graphics

Figure 1: Additional Temporary Construction Easements



-  Temporary Easements
-  Glenwood Hot Springs Historic District (5GF.1050)
-  Glenwood Hot Springs Bathhouse and Natatorium (5GF.1050.2)
-  Denver & Rio Grande Railroad Tracks (5GF.1000.7)

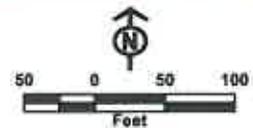
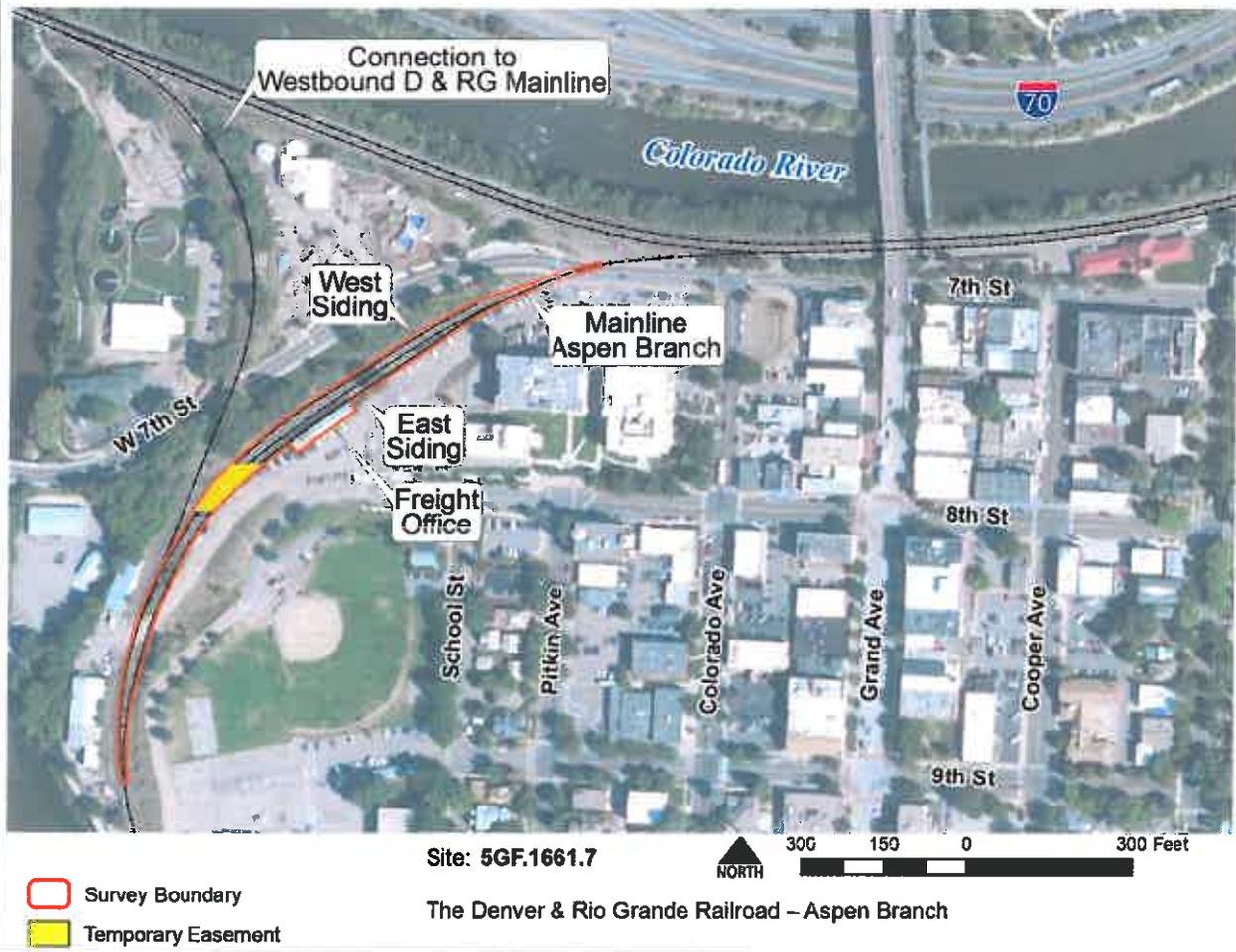


Figure 2: Temporary Construction Easement: Denver & Rio Grande Railroad - Aspen Branch





Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

August 27, 2014

Mr. John Martin  
Mr. Tom Jankovsky  
Board of County Commissioners  
108 8th Street, Suite 213  
Glenwood Springs, CO 81601

**SUBJECT: Additional Information, Section 106 Determinations of Eligibility and Effect, CDOT Project FBR 0821-094, SH 82 / Grand Ave. Bridge Replacement Project Environmental Assessment, Glenwood Springs (CHS #60723)**

Dear Mr. Martin and Mr. Jankovsky:

You were initially provided an opportunity to comment on eligibility and effect determinations for this project in August 2013 and were sent additional information in April 2014. Since then, the scope of the project has changed to include some expanded and additional temporary construction easements on three historic properties identified in the Area of Potential Effects (APE). The APE outlined in our April 2014 correspondence has not changed as a result of these updates to easements. Effect determinations based on these changes to easements are included below. This submittal also includes some additional information regarding the existing pedestrian bridge and effects to one of the historic railroad segments.

### **Pedestrian Bridge**

In our previous consultation efforts in August 2013 and April 2014, we indicated that the pedestrian bridge, located east of the existing highway bridge and built in 1985, will be replaced as part of this project. However, replacement of the pedestrian bridge may be considered as a design option under the Build Alternative. As such, it may or may not be replaced. This pedestrian bridge is not historic, but the design of the dual elevators associated with the new pedestrian bridge's southern access was assessed as part of the no adverse effect determination for the Denver & Rio Grande Railroad Station (5GF1050.3) as noted in our March 2014 consultation. If the pedestrian bridge is left in place, the dual elevator will not be part of the project scope and, therefore, the visual setting of the railroad station would not be affected. We will inform you once we know the final plan for the pedestrian bridge.

### **UPDATED EFFECTS DETERMINATIONS**

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August 2013 submittal. The entire railroad is eligible and the segment retains integrity. In our April 2014 submittal, we indicated that CDOT would acquire one permanent aerial easement totaling about 0.01 acre from the railroad for the widened Grand Avenue Bridge and four areas of temporary easements totaling approximately 0.19 acre. The project

scope now requires an additional 3,838 square-foot temporary aerial easement within the historic property boundary east of the pedestrian bridge to accommodate overhead construction activities associated with replacement of the pedestrian bridge. This easement is separated into two areas on the attached figure (the 2,865 square-foot area and the 973 square-foot area) to make it clear which portion of the easement falls within the historic district boundary. The separate areas of this easement are outlined in black in the black cross-hatched area east of the existing pedestrian bridge. The other easements associated with the railroad and outlined in our previous consultation are still valid.

We consulted previously about the effect of the permanent aerial easement and temporary easements in April 2014 and SHPO concurred with the finding of *no adverse effect*. The additional temporary aerial easement is a minor increase to what was initially expected and will not alter the qualities of significance of the railroad, and the finding of *no adverse effect* is still appropriate.

**Glenwood Hot Springs Historic District (5GF1050):** In our April 2014 submittal we consulted on an additional 0.14-acre temporary easement on the legal parcel of the hot springs property that was outside of the historic boundary. That easement has been expanded by approximately 2,370 square feet—this is outlined in black on the attached graphic. The area of the expanded easement is within the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) property north of I-70 but is outside of the historic district boundary. There are also two areas of enlarged easements near and in the north and south sides of the river that fall within the historic district boundary. On the north side of the river, the easement is approximately 14,795 square feet. It would serve as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70 and would include activities such as grading and operation of heavy machinery. On the south side of the river, the easement is approximately 2,625 square feet. It would serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with demolishing and building the new pedestrian bridge. This would include operation of heavy machinery. Also, an additional temporary aerial easement is required over the railroad (5GF1000.7), of which approximately 973 square feet falls within the historic district boundary. This easement would accommodate overhead construction activities associated with replacement of the pedestrian bridge. The additional temporary easements are outlined in black within the yellow-shaded area indicating the historic district on the attached graphic. Because these are temporary easements, there will be a temporary use of the land during construction but the qualities that make the district significant will not be affected by the easements and there will be *no adverse effect*.

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our August 2013 consultation, we also revised the boundary of the overall Hot Springs historic district to include the Hotel Colorado, the Glenwood Springs Hydroelectric Plant, the Yampah Hot Springs Vapor Caves, a segment of the Denver & Rio Grande railroad, and the Denver & Rio Grande Railroad Station but to exclude the pedestrian bridge. The boundary also includes all but a small area of the bathhouse and natatorium resource that is primarily used as a parking lot. This small area of the bathhouse and natatorium boundary that is outside the overall hot springs historic district will require a 2,730 square foot temporary easement as noted above. This is an easement that will be used temporarily during construction in an existing paved parking area that does not contain any of the major features of the bathhouse and natatorium property. This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work. The qualities that make this property significant will not be altered or diminished and there will be *no adverse effect*.

#### **Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7)**

In our April 2014 consultation letter, we outlined two options for effects to the Denver & Rio Grande Railroad Aspen Branch segment 5GF1661.7 based on the 8<sup>th</sup> street detour. Per our April 2014 consultation, Option 1 involved the removal of a section of the railroad segment for an 8<sup>th</sup> street detour and not replacing the rails in anticipation of the City of Glenwood Springs 8<sup>th</sup> Street Extension project. Option 2 involved temporarily removing the railroad segment and restoring the connection once the CDOT project was completed. FHWA and CDOT met with Amy Pallante of the SHPO office to discuss these options and the possibility of using the Section 4(f) Net Benefit for Option 1.

Since then, Option 1 is no longer under consideration and instead CDOT is moving forward with a modified version of Option 2. For your convenience we have included below, and in italicized text, the description of Option 2 excerpted from our April 2014 letter. The State Historic Preservation Office (SHPO) concurred that Option 2 would result in *no adverse effect* to the railroad.

*During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour for the project. The detour route for regional traffic begins at Exit 114 on I-70, and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary "square about" for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad - Aspen Branch that currently extends between the two sections of 8<sup>th</sup> Street.*

*Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has tracks rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for an estimated two to four years.*

*To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. Under both original detour options, this*

would require temporary removal and storage of between approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements will be required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:

- West Wye Track - approximately 150 feet
- West Track of East Wye - approximately 165 feet
- Middle Track of East Wye - approximately 175 feet
- East Track of East Wye - approximately 175 feet

The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the Grand Avenue bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks would be returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition is anticipated to last between five and six months

After completion of the project, the railbed will be restored and the overall railroad would still portray the key role that the railroad played in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and would continue to represent the railroad's association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in no adverse effect to this resource under this option.

Under the modified Option 2, the work involves a shallow cut to the railroad that will not require the level of earthwork associated with the deep cut planned for the initial Option 2. The same number of tracks would be temporarily removed, but the length of track removed would be reduced, as summarized below:

- West Wye Track - approximately 102 feet
- West Track of East Wye - approximately 73 feet
- Middle Track of East Wye - approximately 76 feet
- East Track of East Wye - approximately 70 feet

Excavation of the existing rail bed/berm to accommodate construction of the temporary two-lane paved roadway would still occur, and a temporary easement would still be required within the railroad boundary, as shown in attached Figure 2.

As we noted previously for Option 2, the rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be

Mr. Martin  
Mr. Jankovsky  
August 27, 2014  
Page 5 of 7

restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to this resource under this option.

As a consulting party for this project, we welcome your comments on these findings. Should you choose to respond, we request that you do so within 30 days of receipt of these materials. If we do not receive a response from you within that time frame, we will assume you do not plan to comment. Thank you for your attention to this matter. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at 303-512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,



For

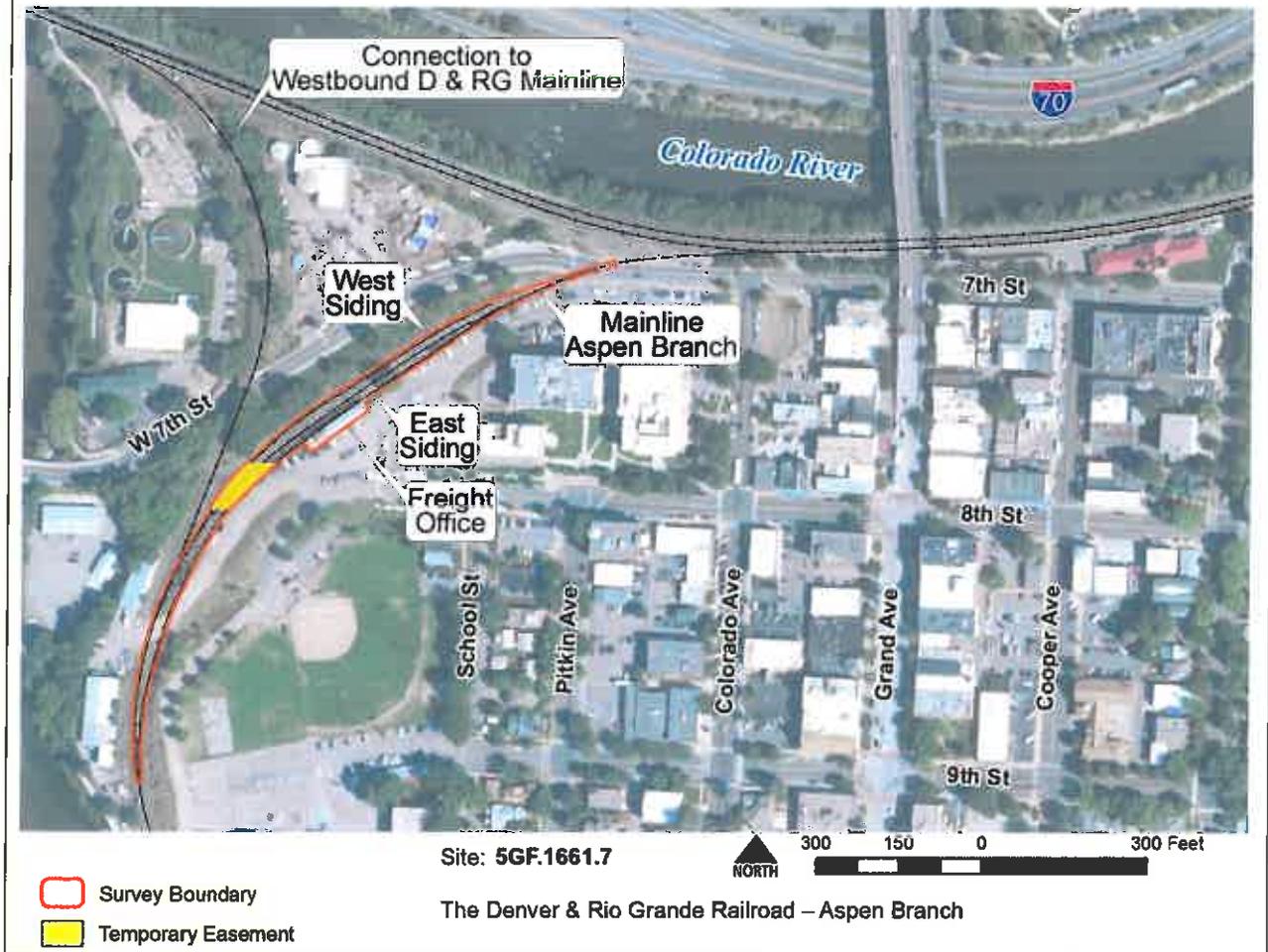
Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Figures 1 and 2: Easement Graphics

Figure 1: Additional Temporary Construction Easements



Figure 2: Temporary Construction Easement: Denver & Rio Grande Railroad - Aspen Branch





**COLORADO**

Department of Transportation

Division of Transportation Development

Environmental Programs Branch  
4201 E. Arkansas Ave., Shumate Bldg.  
Denver, CO 80222-3400  
(303) 757-9281

August 27, 2014

Ms. Cindy Hines, Director  
Frontier Historical Society  
1001 Colorado Avenue  
Glenwood Springs, CO81601

**SUBJECT:** Additional Information, Section 106 Determinations of Eligibility and Effect, CDOT Project FBR 0821-094, SH 82 / Grand Ave. Bridge Replacement Project Environmental Assessment, Glenwood Springs (CHS #60723)

Dear Ms. Hines:

Your organization initially reviewed eligibility and effect determinations for this project in August 2013 and reviewed additional information in April 2014. Since then, the scope of the project has changed to include some expanded and additional temporary construction easements on three historic properties identified in the Area of Potential Effects (APE). The APE outlined in our April 2014 correspondence has not changed as a result of these updates to easements. Effect determinations based on these changes to easements are included below. This submittal also includes some additional information regarding the existing pedestrian bridge and effects to one of the historic railroad segments.

### **Pedestrian Bridge**

In our previous consultation efforts in August 2013 and April 2014, we indicated that the pedestrian bridge, located east of the existing highway bridge and built in 1985, will be replaced as part of this project. However, replacement of the pedestrian bridge may be considered as a design option under the Build Alternative. As such, it may or may not be replaced. This pedestrian bridge is not historic, but the design of the dual elevators associated with the new pedestrian bridge's southern access was assessed as part of the no adverse effect determination for the Denver & Rio Grande Railroad Station (5GF1050.3) as noted in our March 2014 consultation. If the pedestrian bridge is left in place, the dual elevator will not be part of the project scope and, therefore, the visual setting of the railroad station would not be affected. We will inform you once we know the final plan for the pedestrian bridge.

### **UPDATED EFFECTS DETERMINATIONS**

**Denver & Rio Grande Railroad (5GF1000.7):** The railroad was documented and included in the initial August 2013 submittal. The entire railroad is eligible and the segment retains integrity. In our April 2014 submittal, we indicated that CDOT would acquire one permanent aerial easement totaling about 0.01 acre from the railroad for the widened Grand Avenue Bridge and four areas of temporary easements totaling approximately 0.19 acre. The project scope now requires an additional 3,838 square-foot temporary aerial easement within the historic property boundary east of the pedestrian bridge to accommodate overhead construction activities associated with replacement of the pedestrian bridge. This easement is

separated into two areas on the attached figure (the 2,865 square-foot area and the 973 square-foot area) to make it clear which portion of the easement falls within the historic district boundary. The separate areas of this easement are outlined in black in the black cross-hatched area east of the existing pedestrian bridge. The other easements associated with the railroad and outlined in our previous consultation are still valid.

We consulted previously about the effect of the permanent aerial easement and temporary easements in April 2014 and SHPO concurred with the finding of *no adverse effect*. The additional temporary aerial easement is a minor increase to what was initially expected and will not alter the qualities of significance of the railroad, and the finding of *no adverse effect* is still appropriate.

**Glenwood Hot Springs Historic District (5GF1050):** In our April 2014 submittal we consulted on an additional 0.14-acre temporary easement on the legal parcel of the hot springs property that was outside of the historic boundary. That easement has been expanded by approximately 2,370 square feet—this is outlined in black on the attached graphic. The area of the expanded easement is within the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) property north of I-70 but is outside of the historic district boundary. There are also two areas of enlarged easements near and in the north and south sides of the river that fall within the historic district boundary. On the north side of the river, the easement is approximately 14,795 square feet. It would serve as a staging area for construction activities associated with lengthening of the eastbound on-ramp to I-70 and would include activities such as grading and operation of heavy machinery. On the south side of the river, the easement is approximately 2,625 square feet. It would serve as an on-ground staging area (temporary construction pad built within the river) for construction activities associated with demolishing and building the new pedestrian bridge. This would include operation of heavy machinery. Also, an additional temporary aerial easement is required over the railroad (5GF1000.7), of which approximately 973 square feet falls within the historic district boundary. This easement would accommodate overhead construction activities associated with replacement of the pedestrian bridge. The additional temporary easements are outlined in black within the yellow-shaded area indicating the historic district on the attached graphic. Because these are temporary easements, there will be a temporary use of the land during construction but the qualities that make the district significant will not be affected by the easements and there will be *no adverse effect*.

**Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2):** There is an enlarged temporary easement north of I-70 that encroaches into the historic boundary of the Glenwood Hot Springs Bathhouse and Natatorium (5GF1050.2) but does not encroach into the overall Glenwood Hot Springs historic district (5GF1050). The easement would be approximately 2,370 square feet and is outlined in black on the attached graphic. There is a slight discrepancy between the historic boundary of the Glenwood Springs Bathhouse and Natatorium (5GF1050.2) and the overall historic district boundary (5GF1050). The bathhouse and natatorium (5GF1050.2) was found to be individually eligible in 1993 but a property boundary was not established at that time. In our August 2013 submittal, we identified the boundary of this property so that it encompassed the features outlined on the plan shape of the original site form. This boundary includes the natatorium, bathhouse, therapy pool, kiddie pool, cocktail spring, shed, and Yampa spring but excludes the pedestrian bridge. In our August 2013 consultation, we also revised the boundary of the overall Hot Springs historic district to include the Hotel Colorado, the Glenwood Springs Hydroelectric Plant, the Yampah Hot Springs Vapor Caves, a segment of the Denver & Rio Grande railroad, and the Denver & Rio Grande Railroad Station but to exclude the pedestrian bridge. The boundary also includes

all but a small area of the bathhouse and natatorium resource that is primarily used as a parking lot. This small area of the bathhouse and natatorium boundary that is outside the overall hot springs historic district will require a 2,730 square foot temporary easement as noted above. This is an easement that will be used temporarily during construction in an existing paved parking area that does not contain any of the major features of the bathhouse and natatorium property. This area is currently a paved parking lot and would be used for temporary storage of materials and reconstruction of the hot springs parking lot. Activities would include grading, paving, and concrete sidewalk work. The qualities that make this property significant will not be altered or diminished and there will be *no adverse effect*.

#### **Denver & Rio Grande Railroad-Aspen Branch (5GF1661.7)**

In our April 2014 consultation letter, we outlined two options for effects to the Denver & Rio Grande Railroad Aspen Branch segment 5GF1661.7 based on the 8<sup>th</sup> street detour. Per our April 2014 consultation, Option 1 involved the removal of a section of the railroad segment for an 8<sup>th</sup> street detour and not replacing the rails in anticipation of the City of Glenwood Springs 8<sup>th</sup> Street Extension project. Option 2 involved temporarily removing the railroad segment and restoring the connection once the CDOT project was completed. FHWA and CDOT met with Amy Pallante of the SHPO office to discuss these options and the possibility of using the Section 4(f) Net Benefit for Option 1.

Since then, Option 1 is no longer under consideration and instead CDOT is moving forward with a modified version of Option 2. For your convenience we have included below, and in italicized text, the description of Option 2 excerpted from our April 2014 letter. The State Historic Preservation Office (SHPO) concurred that Option 2 would result in *no adverse effect* to the railroad.

*During the approximate two-month full closure of the Grand Avenue Bridge between 8th Street south of the river and 6th Street north of the river, SH 82 traffic will be rerouted onto the designated detour for the project. The detour route for regional traffic begins at Exit 114 on I-70, and proceeds south on Midland Avenue to 8th Street across the Roaring Fork River then along a new 8th Street connection into downtown. In the downtown grid, the traffic will be routed through a temporary "square about" for continuation south on SH 82/Grand Avenue to Aspen. The detour would involve disconnecting this section of the historic Denver & Rio Grande Railroad - Aspen Branch that currently extends between the two sections of 8<sup>th</sup> Street.*

*Four railroad tracks currently traverse the area where 8<sup>th</sup> Street would be extended west to connect to the existing 8<sup>th</sup> Street bridge. The Union Pacific railroad (UPRR) uses the west leg of the wye about four times a year, which includes work train storage and operation of a special UPRR-only passenger train. This use has not extended past the wye, even though the UPRR has track rights south to 13<sup>th</sup> Street. The UPRR also has track rights on the east leg of the wye, but the switch with the UPRR main line is currently disconnected and the east leg has not been used for an estimated two to four years.*

*To accommodate the temporary extension of 8<sup>th</sup> Street, a temporary road connection would be built under the four sets of tracks in this area. Under both original detour options, this would require temporary removal and storage of between approximately 150 to 175 feet of the existing tracks and excavation of the existing rail bed/berm to accommodate construction of a temporary two-lane paved roadway. Temporary easements will be required within the railroad boundary. The approximate length of each track that would be temporarily removed is described below:*

- *West Wye Track - approximately 150 feet*
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- *Middle Track of East Wye - approximately 175 feet*
- *East Track of East Wye - approximately 175 feet*

*The detour would be in operation during the approximate 60-day total closure of the Grand Avenue (SH 82) bridge. Once the Grand Avenue bridge is reopened, the temporary construction detour would be removed, and the railroad bed and tracks would be returned to their preconstruction condition. This would involve backfilling the removed portion of the railbed and reinstalling the previously removed railroad tracks. Detour construction, operation, and returning the temporary detour area to its preconstruction condition is anticipated to last between five and six months*

*After completion of the project, the railbed will be restored and the overall railroad would still portray the key role that the railroad played in the settlement and commercial growth of Glenwood Springs and the state of Colorado, and would continue to represent the railroad's association with the settlement of the Roaring Fork Valley and the 19th century development of Aspen's mining industry. The rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in no adverse effect to this resource under this option.*

Under the modified Option 2, the work involves a shallow cut to the railroad that will not require the level of earthwork associated with the deep cut planned for the initial Option 2. The same number of tracks would be temporarily removed, but the length of track removed would be reduced, as summarized below:

- *West Wye Track - approximately 102 feet*
- *West Track of East Wye - approximately 73 feet*
- *Middle Track of East Wye - approximately 76 feet*
- *East Track of East Wye - approximately 70 feet*

Excavation of the existing rail bed/berm to accommodate construction of the temporary two-lane paved roadway would still occur, and a temporary easement would still be required within the railroad boundary, as shown in attached Figure 2.

As we noted previously for Option 2, the rail materials will be replaced, and the railroad connection, alignment, and corridor -all of which represent integrity of location and setting—will be restored. Materials and the general alignment as well as grade and width will also be restored, which are critical to retaining the feeling and association of the railroad. For these reasons, the proposed project would result in *no adverse effect* to this resource under this option.

As a consulting party for this project, we welcome your comments on these findings. Should you choose to respond, we request that you do so within 30 days of receipt of these

Ms. Hines  
August 27, 2014  
Page 5 of 7

materials. If we do not receive a response from you within that time frame, we will assume you do not plan to comment. Thank you for your attention to this matter. If you have any questions or require additional information in order to complete your review, please contact CDOT Senior Staff Historian Lisa Schoch at 303-512-4258 or [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us).

Very truly yours,



for

Jane Hann, Manager  
Environmental Programs Branch

Enclosures: Figures 1 and 2: Easement Graphics

Figure 1: Additional Temporary Construction Easements

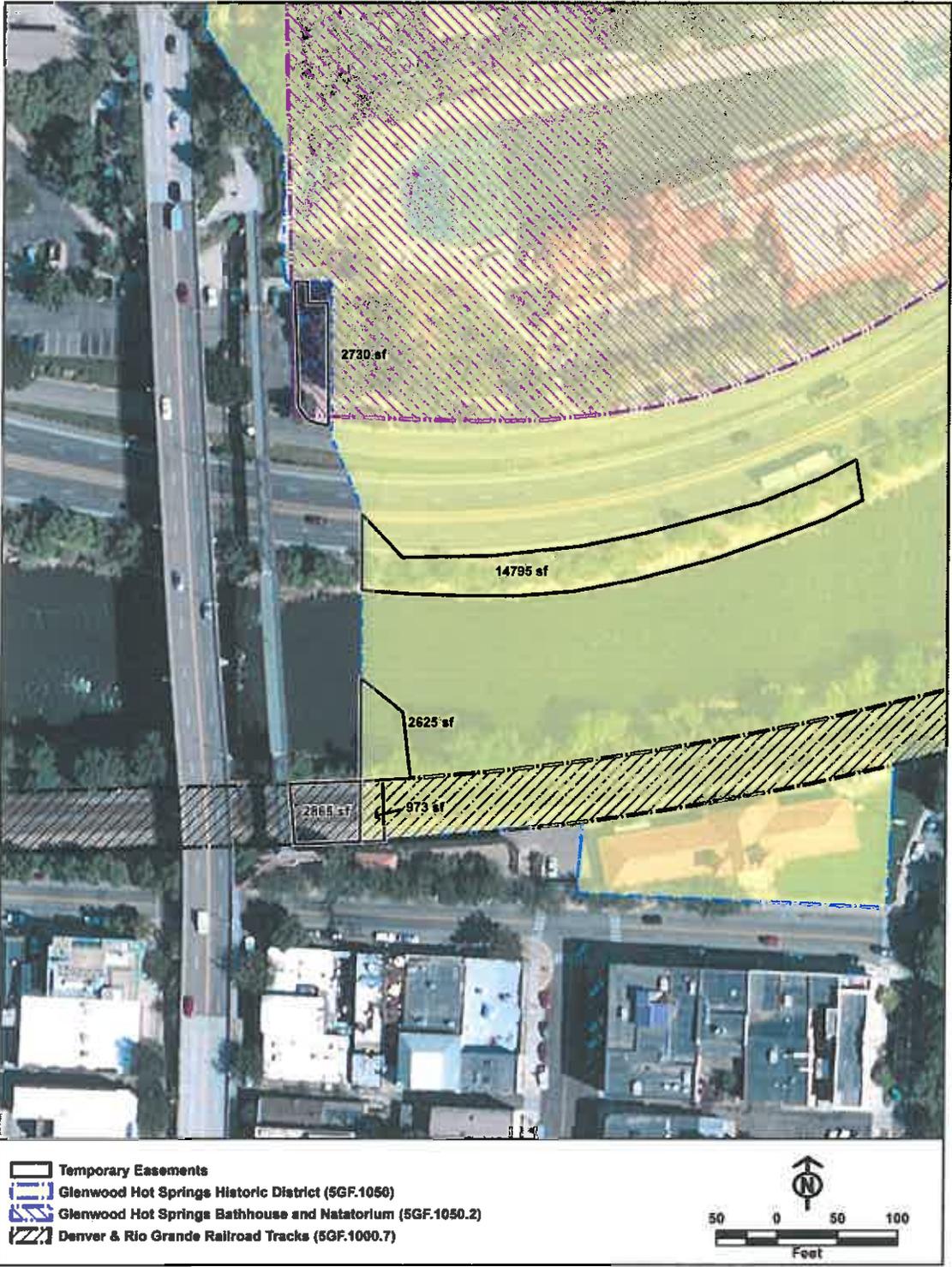
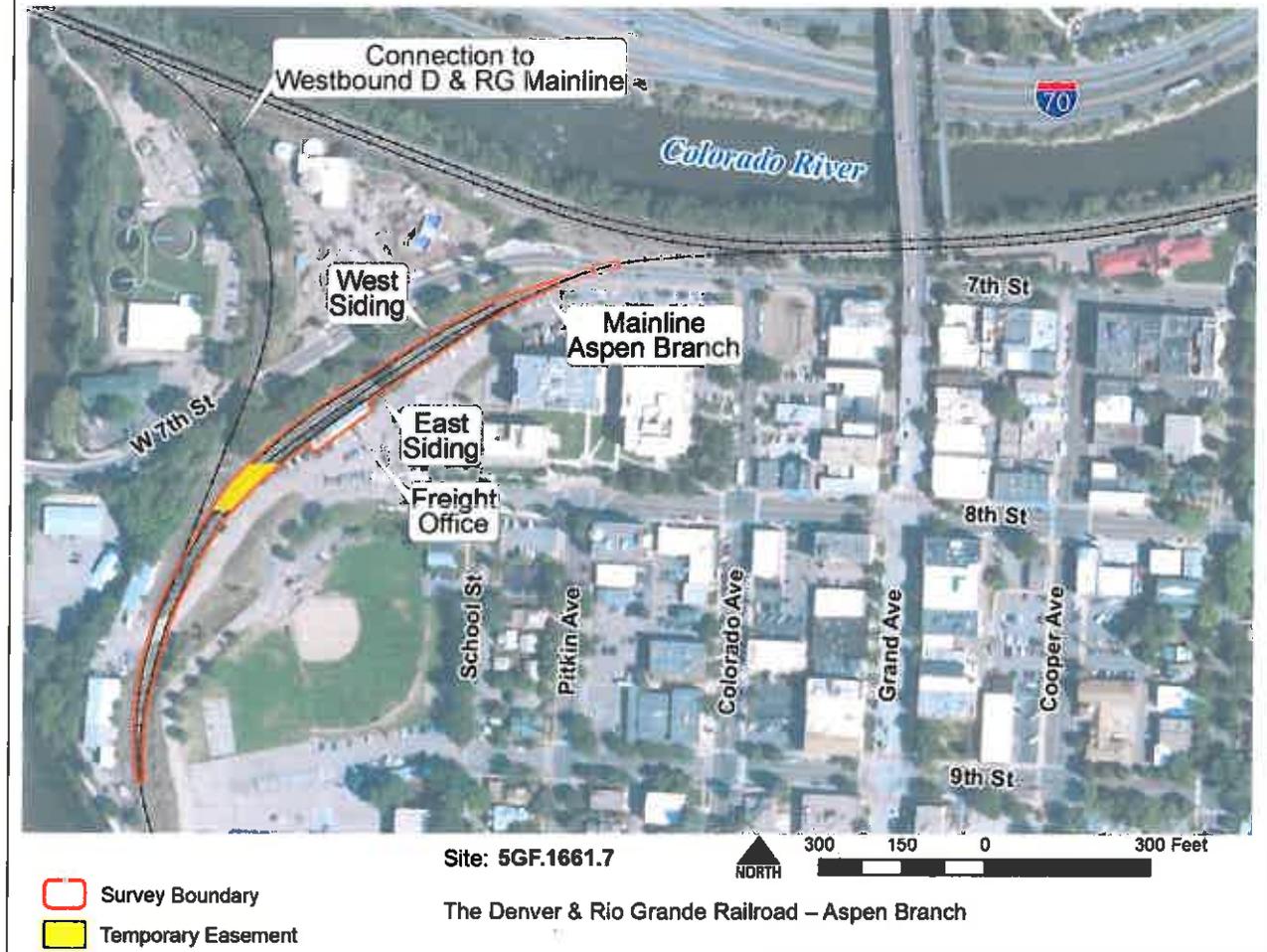


Figure 2: Temporary Construction Easement: Denver & Rio Grande Railroad - Aspen Branch





September 5, 2014

Jane Hann  
Manager, Environmental Program Branch  
Colorado Department of Transportation  
Environmental Programs Branch  
4201 East Arkansas Avenue  
Denver, CO 80222

Re: Additional Information on Determinations of Eligibility and Effect, and Application of Section 4(f) Net Benefit and Temporary Occupancy Exception, CDOT Project FBR 0821-094, SH 82/Grand Ave. Bridge Replacement Project (CHS #60723)

Dear Ms. Hann,

Thank you for your correspondence dated August 27, 2014 and received by email by our office on August 29, 2014 regarding the review of the above-mentioned project under Section 106 of the National Historic Preservation Act (Section 106). After review of the scope of work and updated assessment of adverse effect, we concur with the recommended finding of *no adverse effect* [36 CFR 800.5(b)] under Section 106 to resources:

- 5GF.1000, including segment 5GF.1000.7.
- 5GF.1050
- 5GF.1050.2
- 5GF.1661, including segment 5GF.1661.7.

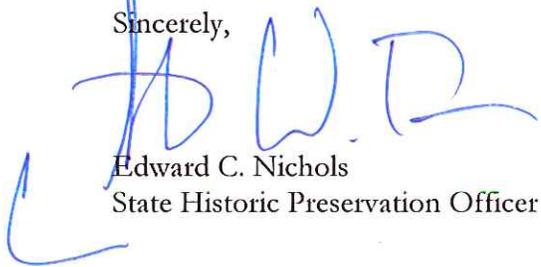
Our office also agrees that the project meets the conditions of temporary occupancy as described in your letter for resources:

- 5GF.1050
- 5GF.1050.2
- 5GF.2441
- 5GF.1000, including segment 5GF.1000.7
- 5GF.1050.3
- 5GF.1661, including segment 5GF.1661.7.
- 5GF.5021

If unidentified archaeological resources are discovered during construction, work must be interrupted until the resources have been evaluated in terms of the National Register criteria, 36 CRF 60.4, in consultation with this office. We request being involved in the consultation process with the local government, which as stipulated in 36 CFR 800.3 is required to be notified of the undertaking, and with other consulting parties. Additional information provided by the local government or consulting parties might cause our office to re-evaluate our eligibility and potential effect findings.

Please note that our compliance letter does not end the 30-day review period provided to other consulting parties. If we may be of further assistance, please contact Amy Pallante, our Section 106 Compliance Manager, at (303) 866-4678.

Sincerely,

A handwritten signature in blue ink, appearing to read "E. Nichols", with a large, sweeping flourish underneath.

Edward C. Nichols  
State Historic Preservation Officer

From: **Rachel Parris** <[rparris@coloradopreservation.org](mailto:rparris@coloradopreservation.org)>  
Date: Mon, Sep 15, 2014 at 1:29 PM  
Subject: Re: Additional Information Project FBR 0821-094  
To: [lisa.schoch@state.co.us](mailto:lisa.schoch@state.co.us)

Lisa,

Thank you for the additional information/update regarding the pedestrian bridge and effects to one of the historic railroad segments associated with the Grand Avenue Bridge Replacement Project Environmental Assessment, Glenwood Springs.

After review of the materials CPI is comfortable with the determinations outlined in the report and has no additional comment.

Best,

**Rachel Parris**

*Projects Manager, Colorado Preservation, Inc.*

1420 Ogden Street, Suite 104

Denver, CO 80218

Phone: [303-893-4260](tel:303-893-4260), Ext. 236

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SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Wildlife



## FINAL Meeting Minutes

**Project:** CDOT Region 3 – SH 82 Grand Avenue Bridge

**Purpose:** Wildlife and Aquatic Species Construction Recommendations

**Date Held:** October 25, 2012

**Location:** Teleconference

**Attendees:**

**CPW:** Dan Cacho, Kendall Bakich

**Jacobs:** Sandy Beazley, Samantha Clark

**Copies:** Attendees, File

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### SUMMARY OF DISCUSSION:

#### INTRODUCTIONS

Attendees introduced themselves.

#### PROJECT OVERVIEW

Sandy Beazley provided an overview of the project. The existing Grand Avenue bridge on State Highway 82 in Glenwood Springs across the Colorado River is in poor condition. CDOT R3 has initiated an Environmental Assessment (EA) to study improvements to the bridge.

The purpose of this teleconference was to obtain input from Colorado Parks and Wildlife (CPW) and recommendations regarding construction timing in regard to wildlife fisheries.

#### SUMMARY OF DISCUSSION

Kendall Bakich aquatic species in the segment of the Colorado River that present concerns in regards to spawning in the Colorado River include the following:

- Rainbow trout, which spawn in the spring
- Brown trout, which spawn in late fall

Fine sediment is the primary threat to spawning from construction. Fine sediment can cover redds and substrate, thereby smothering eggs and/or fry. Based on the spawning habits of both of these fish August through mid-October is the primary time that construction can occur without threatening redds and/or fry.

There is flexibility in this construction window, but modifying construction timing will require sediment BMPs to be used to minimize impacts. One advantage of a spring construction is the high flow rates which provide a naturally elevated sediment load. As a result, additional sediment produced by construction is less likely to stress fish.

The use of a clear span bridge minimizes the potential for sediment release. The demolition of the existing bridge, including removal of the pier located in the river, have the potential to introduce sediment to the river. The use of pads for crane and any other elements for construction staging are encouraged since it keeps equipment out of the riverbed and minimizes sediment/substrate disturbance.

The project will mitigate any impacts to riparian vegetation. Any increase in vegetation cover is encouraged and other creative measures, such as improving fringe wetlands or removing tamarisk and Russian olive, is favorable. In addition, if there are extra materials, specifically boulders, these could be used to enhance fish habitat in the river channel, as was done on a project at the 27<sup>th</sup> Street Bridge.

CPW is interested in additional access to the river, but acknowledge the challenges in the project area due to the constraints posed by the proximity of the railroad and I-70 to the river. We did discuss the potential for pedestrian and cyclists improvements at the intersection of US 6 and SH 82, which would improve connectivity between 6<sup>th</sup> Street and Two Rivers Park.

In addition to fish, Dan Cacho noted that river otters do occur in the Colorado River, including areas immediately north of the project. The river otter is a species of state concern. CPW has requested that while future monitoring for the river otter is planned, it would be appreciated if we complete monitoring for the river otter in the vicinity of the project.

Bighorn sheep wintering ground is located north of the project area and would not be impacted by the project.

Bear are found in town so bear-specific BMPs should be used during construction. This includes bear proof trashcans and education of contractor staff to properly dispose of food waste or any other bear attractants.

CPW has requested that pier design on the banks not effect channel mechanics, which is something the USACE will also require.

## **ACTION ITEMS**

Jacobs to request a species list from CPW by letter.

## Meeting Minutes

**Project:** CDOT Region 3—SH 82 Grand Avenue Bridge

**Purpose:** Wildlife and Aquatic Species Construction Recommendations

**Date Held:** March 6, 2013

**Location:** Colorado Parks and Wildlife Offices: West Glenwood

**Attendees:**

<b>CPW:</b>	Dan Cacho
<b>CDOT:</b>	Mike Vanderhoof
<b>Jacobs:</b>	Jim Clarke
<b>TSH:</b>	Clint Krajnik

**Copies:** Attendees, File

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### SUMMARY OF DISCUSSION:

#### INTRODUCTIONS

Attendees introduced themselves.

#### OVERVIEW

1. Mike and Jim updated Dan on the project status and reviewed highlights of the teleconference held in October 2012 with Colorado Parks and Wildlife (CPW). This meeting continued the discussions with CPW on construction activities, potential effects to wildlife and fisheries, and other related items.

#### SUMMARY OF DISCUSSION

1. Clint provided a graphic showing approximate locations of causeways that would be placed in the river to allow for bridge demolition and construction. Causeways on both sides of the river would be linked with temporary bridges. Causeways would be sized and placed to allow adequate hydraulic passage. August to September is the optimal time for in-stream construction, so Dan recommended construction and removal of the causeways during this time.
2. Dan asked about construction duration and when construction might start. A construction start of Fall 2014 is possible. Construction likely will occur over two years. During Spring high flows, Dan recommended armoring the causeways for protection and removing all equipment, but keeping the causeways in-place over the course of construction. However,

## FINAL Meeting Minutes

### SH 82 Grand Avenue Bridge EA – CPW Species Discussion

March 6, 2013

Page 2 of 4

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the causeways (or part of causeways) might need to be removed prior to the Spring high water, depending on the results of the hydraulic analysis the Study team will conduct.

3. All construction activities need to seek to minimize sediment/substrate disturbance. CDOT is in the process of hiring a contractor so more construction details will be forthcoming.
4. Jim provided construction specifications from the Dotsero Bridge project to minimize effects to fish species (see appendix). Dan is familiar with this project since it falls within his territory, and indicated the same or very similar specs would apply to the Grand Avenue Bridge project.
5. Jim mentioned that, for the I-70 Twin Tunnels project, CPW conducted pre-construction monitoring to determine if trout were spawning in the project area. Don will check with Kendall Bakick, CPW aquatic biologist, about monitoring. (Note: After the meeting, Dan emailed indicating that CPW has not done any spawning fish surveys in the Grand Ave. Bridge construction area recently but plan to survey that stretch in a few weeks. CPW will provide the results.)
6. Dan asked about rafters and kayakers during construction. The Study team would coordinate with the rafting companies about construction activities and develop a plan for individual or private boaters.
7. Mike confirmed the river will be clear spanned and the existing in-stream pier removed.
8. Since construction may require moving boulders in the river, some in-stream habitat improvements could occur as part of the project. Dan will get input from Kendall on this matter.
9. Dan recommended removing invasive species such as tamarisk during construction. This is common practice, and the Study team will coordinate with CPW on the revegetation plan.
10. Jim followed up on a previous CPW comment about possible presence of river otters, a species of state concern. CPW is unsure about otter activity. The Study team will survey for possible otter presence while conducting its field work.
11. Migratory Birds: The Study team will conduct a nesting survey and coordinate with CPW. Dan can't envision any nests that would require buffer restrictions since any nesting birds would be acclimated to the urban environment.
12. Jim mentioned Utes Ladies Tress Orchid was located near the confluence area and the Study team plans to survey for this species. He will provide more details (e.g. location, size, etc.)
13. Dan asked about opportunities to improve river access and recreation opportunities on the south side. Although this is challenging due to the railroad's location and property ownership, Dan would like to broach the subject with the UPRR. The Study team has been in contact with the railroad regarding the project and Jim will provide Dan with UPRR contact information.

## FINAL Meeting Minutes

### SH 82 Grand Avenue Bridge EA – CPW Species Discussion

March 6, 2013

Page 3 of 4

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#### ACTION ITEMS

1. Dan to get input from Kendall on potential for in-stream habitat improvements.
2. Jim to provide details on Utes Ladies Tress Orchid community located near the confluence area.
3. Jim to provide Dan with UPRR contact information.

## **Appendix A: Dotsero Bridge Spawning Season Specifications**

### **From Commencement and Completion of Work Spec:**

Temporary river access shall not be constructed or remain in the river during anticipated high water from March 1 until water elevation drops below the normal high water elevation of 6138 or as approved by the Engineer.

Construction activities or equipment shall not work in flowing water or disturb sediment during recognized spawning seasons as follows:

Rainbow Trout March 1 – May 31

Brown Trout Oct. 1 – Nov. 30

### **REVISION OF SECTION 208 - WATER CONTROL SPEC**

#### SEDIMENT CONTROL

The Contractor shall minimize sediment entrainment within the river flow and the diversion channels through use of protected control structures. Such protection shall consist of, but not necessarily be limited to, geotextiles fabrics, riprap, and conduits.

In no instance shall construction activities or equipment be allowed to work in flowing water during recognized spawning seasons or any other time not approved by the Engineer. See commencement and completion of work for additional information.

#### MEASUREMENT AND PAYMENT

Payment will be full compensation for all work necessary to complete the temporary access into the river, including all measures implemented to protect the work and minimize sediment entrainment in the river, including but not limited to temporary shoring, diversion berms (earthen, sheet pile or other), earthwork, temporary low flow crossings, geotextile fabrics, riprap, dewatering wells, pumps, associated grading, and all work associated with temporary wetland vegetation protection and cleanup.

The cost of temporary access and cofferdams in the river shall be based on an average water surface elevation of 6136 feet between the new and existing bridges. Actual water elevations in excess of 6136 feet will require additional costs to construct higher access roads and cofferdams. Scheduling around the fall 2013 spawning season for bridge demolition may require additional costs for remobilization to construct access and demolish the bridge. The following risk pool (Access Risk Pool) has been established to allow compensation for these additional costs.

Ms. Alison Dean-Michael  
 US Fish and Wildlife Service  
 PO Box 25486, DFC (MS 65412)  
 Denver, CO 80225  
 Attn: Alison

RE: **Federally Listed Species Assessment for Informal Section 7 Consultation for the Grand Avenue Bridge Replacement Project**

Dear Ms. Dean-Michael:

This letter contains the list and analysis of federally threatened, endangered, and candidate species that may occur in the Grand Avenue Bridge Replacement study area. This list, which is attached and shows the study area, was determined using the USFWS ECOS-IPaC website on April 17, 2013.

The Grand Avenue Bridge serves as a vital link of State Highway 82 (SH 82) across the Colorado River, Interstate 70 (I-70), and the Union Pacific Railroad. It connects downtown Glenwood Springs and the Roaring Fork Valley with the historic Hot Springs, iconic Hotel Colorado, and I-70. The purpose of the project is to provide a safe, secure, and effective connection, thereby maintaining this route. To achieve this, the existing vehicle and pedestrian bridges will be replaced and connectivity to these bridges will be improved (see attached map).

Per the USFWS ECOS system, ten federally listed species were identified with the potential to occur in the study area. The study team conducted surveys in May and November 2013 to evaluate the presence or absence of suitable habitat for the below-listed species within the study area. The results of this survey are shown in **Table 1**.

**Table 1. Federally Listed Species Potentially Found in the Study Area**

Species	Status	Potential to Occur in the Study Area
<b>Mammals</b>		
Canada lynx <i>Lynx canadensis</i>	FT	No—Lack of suitable habitat and prey species, as well as the proximity of residential and commercial development indicate that Canada lynx would not be present in the study area.
North American wolverine <i>Gulo gulo luscus</i>	FC	No— Lack of suitable habitat and prey species, as well as the proximity of residential and commercial development indicate that wolverine would not be present in the study area.
<b>Birds</b>		
Greater Sage-Grouse <i>Centrocercus urophasianus</i>	FC	No—Lack of suitable in habitat, as there are no sagebrush flats or rolling sagebrush hills present in the study area.
Mexican Spotted Owl <i>Strix occidentalis lucida</i>	FT	No—Lack of suitable habitat, as there are no mixed conifer forests and rocky canyons within the study area.
Western Yellow-Billed Cuckoo <i>Coccyzus americanus</i>	FC	No— The narrow, steep banks between the Colorado River and I-70 are steep with “rip-rap” for soil stabilization and river channelization. Both hydrology and vegetation are highly altered in the riparian corridor adjacent to the study area. Native vegetation is largely absent with Siberian elm, tamarisk, and ash composing the majority of the study area’s canopy. No mature cottonwood or willows are present.
<b>Fish</b>		
Bonytail chub <i>Gila elegans</i>	FE	Unlikely-Bonytail chub is historically known to the Colorado River Basin though considered “functionally extinct” by the USFWS. According to the species’ ECOS profile, bonytail chub may occur in Mesa, Moffat, and Saguache Counties, Colorado though not Garfield County where the

**Table 1. Federally Listed Species Potentially Found in the Study Area**

Species	Status	Potential to Occur in the Study Area
		study area is located.
Colorado pikeminnow <i>Ptychocheilus lucius</i>	FE	Unlikely-Colorado pikeminnow are historically known to the Colorado River in Garfield County where the study area is located. However, this fish requires muddy, slower-flowing riverine habitat. The Colorado River in the study area is a high-gradient, relatively clear stretch.
Humpback chub <i>Gila cypha</i>	FE	Unlikely-According to the species' ECOS profile, humpback chub may occur in Mesa, Moffat, and Saguache Counties, Colorado though not Garfield County where the study area is located.
Razorback sucker <i>Xyrauchen texanus</i>	FE	Unlikely-According to the species' ECOS profile, razorback sucker is historically known to the Colorado River in Garfield County, where the study area is located. However, the nearest currently known population lies roughly 90 river miles downstream of the study area in an off-channel pond near Grand Junction, Colorado.
Plants		
Ute ladies'-tresses orchid <i>Spiranthes diluvialis</i>	FT	No—The long-altered hydrology found in the study area does not provide <i>Spiranthes</i> habitat. This stretch of the Colorado River is channelized with steep, "rip-rap" covered banks. CDOT is aware of a <i>Spiranthes</i> population located west of the study area downstream from the confluence of the Colorado and Roaring Fork River.

Source: USFWS ECOS

FE = Federally Endangered, FT = Federally Threatened, FC = Federal Candidate

As shown in **Table 1**, there are no federally listed terrestrial species anticipated to occur in the study area. The combination of unsuitable habitat and historical population records indicate that the four fish species mentioned above are unlikely to occur in the Colorado River near the study area. Therefore, it is our opinion that the project will have *no effect* on any species protected by the Endangered Species Act. As such, we hereby request USFWS concurrence of this determination, and the need for no additional consultation.

If USFWS requires further information or has questions pertaining to our determinations, please do not hesitate to contact me at (303) 512-4959 or [jeff.peterson@state.co.us](mailto:jeff.peterson@state.co.us).

Sincerely,

Jeff Peterson  
CDOT Wildlife Program Manager

Cc. Mike Vanderhoof, CDOT  
Jim Clarke, Jacobs

Attachment

May 9, 2013

Dan Soucy  
Biologist/Environmental Scientist  
JACOBS  
707 17th St.  
Denver, CO 80202

Dear Dan:

The Colorado Natural Heritage Program (CNHP) is in receipt of your request for information regarding the Grand Avenue Bridge located in Glenwood Springs, Colorado. In response, I have searched our Biodiversity Tracking and Conservation System (BIOTICS) for natural heritage elements (occurrences of significant natural communities and rare, threatened or endangered plants and animals) documented from the vicinity of the area specified in your request, specifically within a two-mile radius of the bridge footprint.

The enclosed report describes natural heritage resources known from this area and gives location (by Township, Range, and Section), precision information, and the date of last observation of the element at that location. This report includes elements known to occur within the specified project site, as well as elements known from similar landscapes near the site. Please note that “precision” reflects the resolution of original data. For example, an herbarium record from “4 miles east of Colorado Springs” provides much less spatial information than a topographic map showing the exact location of the occurrence. “Precision” codes of Seconds, Minutes, and General are defined in the footer of the enclosed report.

The report also outlines the status of known elements. We have included status according to Natural Heritage Program methodology and legal status under state and federal statutes. Natural Heritage ranks are standardized across the Heritage Program network, and are assigned for global and state levels of rarity. They range from “1” for critically imperiled or extremely rare elements, to “5” for those that are demonstrably secure.

You may notice that some occurrences do not have sections listed. Those species have been designated as “sensitive” due to their rarity and threats by human activity. Peregrine falcons, for example, are susceptible to human breeders removing falcon eggs from their nests. For these species, CNHP does not normally provide location information beyond township and range. Please contact us should you require more detailed information for sensitive occurrences.

There is one CNHP designated Potential Conservation Area (PCA) and no Network of Conservation Areas (NCA) that overlap with the search area (see enclosed PDF site report and ArcGIS shapefile). In order to successfully protect populations or occurrences, it is necessary to delineate conservation areas. These conservation areas focus on capturing the ecological processes that are necessary to support the continued existence of a particular element of natural heritage significance. Conservation areas may include a single occurrence of a rare element or a suite of rare elements or significant features.



The goal of the process is to identify a land area that can provide the habitat and ecological processes upon which a particular element or suite of elements depends for their continued existence. The best available knowledge of each species' life history is used in conjunction with information about topographic, geomorphic, and hydrologic features, vegetative cover, as well as current and potential land uses. The proposed boundary does not automatically exclude all activity. It is hypothesized that some activities will cause degradation to the element or the process on which they depend, while others will not. Consideration of specific activities or land use changes proposed within or adjacent to the preliminary conservation planning boundary should be carefully considered and evaluated for their consequences to the element on which the conservation unit is based.

The Colorado Division of Wildlife has legal authority over wildlife in the state. CDOW would therefore be responsible for the evaluation of and final decisions regarding any potential effects a proposed project may have on wildlife. If you would like more specific information regarding these or other vertebrate species in the vicinity of the area of interest, please contact the Colorado Division of Wildlife.

The information contained herein represents the results of a search of Colorado Natural Heritage Program's (CNHP) Biodiversity Tracking and Conservation System (BIOTICS), and can be used as notice to anticipate possible impacts or identify areas of interest. Care should be taken in interpreting these data. Sensitive elements are currently known from within the vicinity of the project area, and as always, other undocumented elements may also exist (see enclosed species PDF report). We also searched our watch-listed species observation database and found additional records for the Colorado Springs site (see enclosed PDF observations report). Please note that the absence of data for a particular area, species, or habitat does not necessarily mean that these natural heritage resources do not occur on or adjacent to the project site, rather that our files do not currently contain information to document their presence. CNHP information should not replace field studies necessary for more localized planning efforts, especially if impacts to wildlife habitat are possible.

Although every attempt is made to provide the most current and precise information possible, please be aware that some of our sources provide a higher level of accuracy than others, and some interpretation may be required. CNHP's data system is constantly updated and revised. Please contact CNHP for an update or assistance with interpretation of this natural heritage information.

The data contained in the report is the product and property of the Colorado Natural Heritage Program (CNHP), a sponsored program at Colorado State University (CSU). The data contained herein are provided on an as is, as available basis without warranties of any kind, expressed or implied, including (but not limited to) warranties of merchantability, fitness for a particular purpose, and non-infringement. CNHP, CSU and the state of Colorado further expressly disclaim any warranty that the data are error free or current as of the date supplied.

Sincerely,



Michael Menefee  
Environmental Review Coordinator

Enc.



# CNHP Observation Report

**Source Feature ID:** 2,733      **Shape ID:** 31,145  
**Elcode:** AFCJC02110      **Element Global ID:** 6,250      **Element Subnational ID:** 23,002  
**Global Scientific Name:** *Catostomus latipinnis*  
**Global Imperilment Rank:** G3G4  
**Subnational Scientific Name:** *Catostomus latipinnis*  
**Subnational Imperilment Rank:** S3  
**State Common Name:** Flannelmouth Sucker

## Element Protection Status

**Fed ESA Status:** -      **State Prot Status:** -      **CNHP Sensitive Element:** No

### Sensitive Status on:

USFS managed lands

BLM managed lands

## EO Information Source Feature Identifiers

**Source Feature Descriptor:** -

**Source Feature Locator:** -

**Conceptual Source Feature Type:** Point

**Locational Uncertainty Type:** Negligible

**Locational Uncertainty Distance Class:** -

**Locational Uncertainty Distance:** -

## Mapping

**Digitizing Base:** 1:24,000 Quad - Digital

**Digitizing Comments:** -

**Mapping Comments:** -

## Observations

### Observer

Green, Larry

### Obs Date

1983-04-13

### Observation Data

70% of suckers sited were flannelmouths.

# Level 2 Potential Conservation Area (PCA) Report

Name No Name Creek

Site Code S.USCOHP\*21717

## IDENTIFIERS

Site ID 121 Site Class PCA  
 Site Alias None

## Network of Conservation Areas (NCA)

<u>NCA Site ID</u>	<u>NCA Site Code</u>	<u>NCA Site Name</u>
-		No Data

Site Relations No Data

## LOCATORS

Nation United States Latitude 393721N  
 State Colorado Longitude 1071742W

<u>Quad Code</u>	<u>Quad Name</u>
39107-E3	Glenwood Springs
39107-F3	Carbonate

County  
 Garfield (CO)

<u>Watershed Code</u>	<u>Watershed Name</u>
14010001	Colorado headwaters
14010005	Colorado headwaters-Plateau

## SITE DESCRIPTION

<b>Minimum Elevation</b>	6,000.00 Feet	1,829.00 Meters
<b>Maximum Elevation</b>	10,700.00 Feet	3,261.00 Meters

### Site Description

This is a very large site that encompasses the entire watershed for No Name Creek, which is used by the City of Glenwood Springs for their city water supply. The lower reach of the creek is dominated by a dense and diverse overstory of trees and shrubs including narrowleaf cottonwood (*Populus angustifolia*), Douglas-fir (*Pseudotsuga menziesii*), river birch (*Betula occidentalis*), red-osier dogwood (*Cornus sericea*), black twinberry (*Lonicera involucrata*), Rocky Mountain maple (*Acer glabrum*), and thimbleberry (*Rubus parviflorum*). Upland slopes along the lower reach are very steep and mainly covered with scattered Douglas-fir. A globally imperiled community consisting of aspen (*Populus tremuloides*) and sticky-laurel (*Ceanothus velutinus*) is found near the top of the ridge on the east-facing slopes. Non-native species such as orchard grass (*Dactylis glomerata*) and Kentucky bluegrass (*Poa pratensis*) are fairly abundant in this area. Upstream, above the narrow limestone canyon (approx. 6 miles), the vegetation changes to a riparian system more typical of subalpine environments. These subalpine areas have gentler upland slopes dominated by Engelmann spruce (*Picea engelmannii*) and aspen (*Populus tremuloides*). The riparian areas are dominated by Drummond's willow (*Salix drummondiana*), planeleaf willow (*S. planifolia*), and a variety of herbaceous species. About 3 miles upstream from the mouth of No Name Creek, an aqueduct dumps water from nearby Grizzly Creek to the east, into No Name Creek to supplement Glenwood Springs' city water supply. Soils are not mapped by the county soil survey. The U.S. Forest Service in Glenwood Springs may have soil maps for this area. This site contains a long and extensive riparian area with a high cover of woody vegetation plus numerous subalpine meadows and ponds, thus the capacity of this wetland to perform flood attenuation and bank stabilization may be good. The diversity of habitats, including scrub-shrub, forested, and emergent wetlands, provide excellent habitat for avian species and large and small mammals. Excellent vegetation structure along the creek provides shade and woody debris and thus excellent fish habitat.

### Key Environmental Factors

No Data

### Climate Description

No Data

### Land Use History

No Data

### Cultural Features

No Data

# Level 2 Potential Conservation Area (PCA) Report

Name No Name Creek

Site Code S.USCOHP\*21717

## SITE DESIGN

Site Map Y - Yes Mapped Date 01/15/2001  
 Designer Rocchio, F.J.

### Boundary Justification

The boundary encompasses the entire No Name Creek watershed and thus, ensures continued hydrological flow and allows natural fluvial processes to dynamically maintain the riparian plant communities.

Primary Area 7,880.15 Acres 3,189.00 Hectares

## SITE SIGNIFICANCE

Biodiversity Significance Rank B2: Very High Biodiversity Significance

### Biodiversity Significance Comments

This site supports one good (B-ranked) occurrence of the globally imperiled (G2/S2) aspen / sticky-laurel (*Populus tremuloides* / *Ceanothus velutinus*) forest, one good (B-ranked) occurrence of the state rare (G4/S3) narrowleaf cottonwood / red-osier dogwood (*Populus angustifolia* / *Cornus sericea*) riparian forest and two good (B-ranked) occurrences of the common (G4/S4) Drummond's willow / mesic forb (*Salix drummondiana* / mesic forb) deciduous alluvial shrubland. There is also a poor (D-ranked) occurrence of the globally vulnerable (G3T3/S3) hanging garden sullivania (*Sullivantia hapemanii* var. *purpusii*).

Other Values Rank No Data

### Other Values Comments

No Data

## LAND MANAGEMENT ISSUES

### Land Use Comments

No Data

### Natural Hazard Comments

No Data

### Exotics Comments

Non-native species such as orchard grass (*Dactylis glomerata*) and Kentucky bluegrass (*Poa pratensis*) are fairly abundant.

### Offsite

No Data

### Information Needs

No Data

## ASSOCIATED ELEMENTS OF BIODIVERSITY

<u>Element State ID</u>	<u>State Scientific Name</u>	<u>State Common Name</u>	<u>Global Rank</u>	<u>State Rank</u>	<u>Driving Site Rank</u>
24961	<i>Salix drummondiana</i> / Mesic Forbs Shrubland	Drummonds Willow/Mesic Forb	G4	S4	No
22214	<i>Sullivantia hapemanii</i> var. <i>purpusii</i>	Hanging Garden sullivania	G3T3	S3	No
24961	<i>Salix drummondiana</i> / Mesic Forbs Shrubland	Drummonds Willow/Mesic Forb	G4	S4	No
24860	<i>Populus angustifolia</i> / <i>Cornus sericea</i> Woodland	Cottonwood Riparian Forest	G4	S3	No
24822	<i>Populus tremuloides</i> / <i>Ceanothus velutinus</i> Forest	Aspen Forests	G2	S2	Yes

## REFERENCES

<u>Reference ID</u>	<u>Full Citation</u>
160919	Lyon, P. 2000. Colorado Natural Heritage Program Biological Assessment of Garfield County.
160810	Rocchio, J. 2000. Colorado Natural Heritage Program Wetland Inventory/Assessment of Garfield County.

## ADDITIONAL TOPICS

### Additional Topics

No Data

## VERSION

Version Date 01/15/2001  
 Version Author Rocchio, F.J.

# Level 2 Potential Conservation Area (PCA) Report

Name No Name Creek

Site Code S.USCOHP\*21717



Locations and Status of Rare and/or Imperiled Species and Natural Communities known from or likely to occur within a two-mile radius of the Grand Avenue Bridge in Glenwood Springs, CO

Report generated: 8 May 2013

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<i>EO_ID</i>	<i>major group</i>	<i>scientific name</i>	<i>common name</i>	<i>Prec</i>	<i>last obs</i>	<i>Town/ Range</i>	<i>Sec</i>	<i>TRS Note</i>	<i>grank</i>	<i>srank</i>	<i>eo- rank</i>	<i>ESA</i>	<i>fed stat</i>	<i>st stat</i>
12,642	Birds	<i>Melanerpes lewis</i>	Lewis's Woodpecker	M	2002-05-20	005S089W 006S089W 006S089W	34 06 09		G4	S4	C	-	USFS	
4,396	Fish	<i>Gila robusta</i>	Roundtail Chub	M	1983-99-99	006S089W	09		G3	S2	H	-	BLM USFS	SC
13,253	Mammals	<i>Corynorhinus townsendii pallescens</i>	Townsend's Big-eared Bat Subsp	S	2001-09-25	006S089W			G3G4T 3T4	S2	E	-	BLM USFS	SC





# United States Department of the Interior



FISH AND WILDLIFE SERVICE  
COLORADO FIELD OFFICE/LAKEWOOD  
P.O. BOX 25486, DENVER FEDERAL CENTER  
DENVER, COLORADO 80225-0486

IN REPLY REFER TO:  
ES/CO: CDOT  
TAILS: 06E24000-2014-I-0126

December 9, 2013

Jeff Peterson  
Colorado Department of Transportation  
4202 East Arkansas Avenue, Shumate Building  
Denver, Colorado 80222

Dear Mr. Peterson:

Based on the authority conferred to the U.S. Fish and Wildlife Service (Service) by the Endangered Species Act of 1973 (ESA), as amended (16 U.S.C. 1531 *et seq.*), the Service reviewed your November 27, 2013, report regarding the replacement of the Grand Avenue bridge over I-70, the Union Pacific Railroad, and the Colorado River in Glenwood Springs, Garfield County, Colorado, and its potential impact on federally listed species.

Given your project location, the Service agrees that the project is not likely to impact any federally listed species.

Please note that should project plans change or if additional information regarding listed or proposed species becomes available, this determination may be reconsidered under the ESA. If the proposed project has not commenced within one year, please contact the Colorado Field Office to request an extension.

We appreciate your submitting this report to our office for review and comment. If the Service can be of further assistance, please contact Alison Deans Michael of my staff at (303) 236-4758.

Sincerely,

Susan C. Linner  
Colorado Field Supervisor

ec: Michael

Ref: Alison\H:\My Documents\CDOT 2007\Region 3\Grand\_Avenue\_bridge\_replacement\_agree.docx





SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Hazardous Materials



## Robyn Kullas

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**From:** Robyn Kullas <Kullas@pinyon-env.com>  
**Sent:** Wednesday, April 17, 2013 11:37 AM  
**To:** Crouse, Anne (Anne.Crouse@dphe.state.co.us)  
**Subject:** Request for Files

Hi Anne,

I'm performing an environmental site assessment in Glenwood Springs. Do you have any files for the following addresses/facilities?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
3. 810 Grand Avenue, Glenwood Springs, CO (Corky's gas)
4. 115 6<sup>th</sup> Street, Glenwood Springs, CO (Sunlight Motors)
5. 210 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
6. 105 6<sup>th</sup> Street, Glenwood Springs, CO (Kum + Go/Gilcomart)
7. 216 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas  
Environmental Scientist  
Cell: 303.601.6131  
Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



## Robyn Kullas

---

**From:** Crouse - CDPHE, Anne <anne.crouse@state.co.us>  
**Sent:** Wednesday, April 17, 2013 1:51 PM  
**To:** Robyn Kullas  
**Subject:** Re: Request for Files  
**Attachments:** 216 W 6th.pdf; 105 6th Street.pdf; 045-0104-1 045-0104-001 APEN 2008-11-25.pdf

I have attached the only information I was able to find for your request.  
106 6th Street is closed and there are no outstanding enforcement actions.

Please let me know if I can be of any further assistance. Anne

On Wed, Apr 17, 2013 at 11:36 AM, Robyn Kullas <[Kullas@pinyon-env.com](mailto:Kullas@pinyon-env.com)> wrote:

Hi Anne,

I'm performing an environmental site assessment in Glenwood Springs. Do you have any files for the following addresses/facilities?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
3. 810 Grand Avenue, Glenwood Springs, CO (Corky's gas)
4. 115 6<sup>th</sup> Street, Glenwood Springs, CO (Sunlight Motors)
5. 210 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
6. 105 6<sup>th</sup> Street, Glenwood Springs, CO (Kum + Go/Gilcomart)
7. 216 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas

Environmental Scientist

Cell: 303.601.6131

Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)

--

***Anne Crouse***

**Records Coordinator**

Department of Public Health and Environment

4300 Cherry Creek Drive So

office: 303.692-3129

emai: [anne.crouse@state.co.us](mailto:anne.crouse@state.co.us)

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## Co-Located Point Summary Report

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**Criteria:** Internal Plant ID = "9787"

**Co-Located Point Summary Report**

AIRSID	Bogus	Plant Name/Description/Plant Owner	Address1/Address2/Contact				
045-0143	N	WESTERN PETROLEUM -RED MOUNTAIN TEXACO RETAIL GASOLINE STATION  8405979320 WESTERN PETROLEUM	216 6TH ST GLENWOOD SPRINGS, CO, 81601-3034 STEVEN ROBINSON (970)945-6214				
<b>Customer#: 8405979320</b>		<b>Point Owner: WESTERN PETROLEUM</b>					
Point#	Point Type	Active	CP Permit	GP Package	GP Permit	O.P.#	Inv Yr
001	SER SERVICE STATION GASOLINE	Y	03GA0042S				2007
ONE 10,000 GAL UST: GASOLINE ONE 6,000 GAL UST: GASOLINE ONE 6,000 GAL UST: DIESEL							

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## Co-Located Point Summary Report

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**Criteria:** Internal Plant ID = "4013"

### Co-Located Point Summary Report

AIRSID	Bogus	Plant Name/Description/Plant Owner	Address1/Address2/Contact
<b>045-0104</b>	N	GILCO INC. GAS STATION 9999991263 GILCO INC.	105 6TH ST GLENWOOD SPRINGS, CO, 81601-2937 JENNY CARPENTER (515)261-7750

**Customer#: 9999991263      Point Owner: GILCO INC.**

Point#	Point Type	Active	CP Permit	GP Package	GP Permit	O.P.#	Inv Yr
<b>001</b>	SER SERVICE STATION GASOLINE	Y	92GA1540S.XP				2008
ONE 10,000 GAL UST: GASOLINE ONE 8000 GAL UST: GASOLINE ONE 6000 GAL UST: DIESEL							

**Air Pollutant Emission Notice (APEN) & Application for Construction Permit  
- Underground Storage Tank (UST) Fuel Dispensing Stations -**

PERMIT NUMBER: 92GA1540S.XP AIRS ID: 045 / 0104 / 001  
(Leave the above blank if this is a newly reported source. Complete this information if source previously reported.)

**Section 01 – Administrative Information**

Company Name: Gilco Inc. 211282  
 Plant Name: Kum & Go  
 Plant Location: 105 6th Street County: Garfield  
Glenwood Springs, CO 81601  
 Mailing Address: P.O. Box 1061 ZIP Code: 81650  
Rifle, CO 81650  
 Person to Contact: Evelyn Gilstrap Phone Number: (970) 625-0797  
 E-mail Address: gilstrap1112@yahoo.com Fax Number: (970) 625-1686

**Section 02 – Requested Action (Check Applicable Request Box)**

- Request for New Permit or Newly Reported Emission Source
- Request Modification to Existing Permit (Check each box below that describes the modification)
  - Equipment Change
  - Company Name Change
  - Permit Limit Change
  - Transfer of Ownership (Attach Copy of Sale Contract)
- Request APEN Update Only (Check the box below that describes APEN update)
  - Revision to Actual Calendar Year Emissions for Emission Inventory and Annual Fee Purposes.
  - Update 5-Year APEN Term Without Change to Permit Limits or Previously Reported Emissions

Reason for APEN Submittal: \_\_\_\_\_

All sections of this APEN and application must be completed for both new and existing facilities. An application with missing information may be determined incomplete and returned to the applicant. This may result in longer engineer processing times.

**Section 03 – General Information**

For Existing Sources: Operation of this source began on: 11 / 03 / 1994  
 For New or Modified Sources: The projected startup date is:     /    /      
 Normal Hours of Source Operation: 24 hours/day 7 days/week 52 weeks/year  
 General Description of Facility Function: Convenience Store/Gas Station

**Section 04 – Storage Tank & Emission Control Information (use additional sheets as necessary)**

Number of Tanks	Underground Storage Tank Capacity (gallons)	Name of Product Stored	Stage I Vapor Recovery? (Yes or No)	Stage II Vapor Recovery? (Yes or No)	Submerged Fill Pipe? (Yes or No)
1	10,000	Unleaded	Yes	No	Yes
1	8,000	Premium	Yes	No	Yes
1	6,000	Diesel	Yes	No	Yes

**Section 05 – Fuel Throughput Information**

Name of Product Stored	Actual Calendar Year Total Combined Facility Throughput (gallons/year)	Requested Permit Limit Total Combined Facility Throughput (gallons/year)
Unleaded	906,968	1,133,710
Premium	183,590	229,487
Diesel	97,049	121,311



045-0104-001-APEN-2008-11-25-01

**Air Pollutant Emission Notice (APEN) & Application for Construction Permit  
- Underground Storage Tank (UST) Fuel Dispensing Stations -**

**Section 06 - Emissions Inventory Information (Optional)**

Please complete this section if you have calculated emissions from your operation. If you provide emissions estimates, please attach a copy of the corresponding emission calculations with emission factor documentation to this APEN form. If this section is left blank, the Air Pollution Control Division will calculate emissions based on the information contained in sections 04 and 05 above.

Criteria Pollutant Emissions Inventory				
Pollutant	Actual Calendar Year Emissions		Permit Limit Requested Emissions	
	Uncontrolled Emissions (Tons/Year)	Controlled Emissions (Tons/Year)	Uncontrolled Emissions (Tons/Year)	Controlled Emissions (Tons/Year)
VOC				
The Calendar Year for which Actual Throughput and Emission Data Applies is:				

Non-Criteria Reportable Pollutant Emissions Inventory				
Chemical Name	Chemical Abstract Service (CAS) Number	Reporting BIN	Actual Calendar Year Emissions	
			Uncontrolled Emissions (Pounds/year)	Controlled Emissions (Pounds/year)

**Section 07 - Applicant Certification of Statement**

I hereby certify that all information contained herein and information submitted with this application is true and correct.

*Evelyn Gilstrap* 11/19/08  
 [Signature of Responsible Official (Not a vendor or consultant) \_\_\_\_\_ Date \_\_\_\_\_]  
 Evelyn Gilstrap President  
 [Name of Responsible Official (Please print) \_\_\_\_\_ Title \_\_\_\_\_]

Check the following boxes to request:

- A draft version of the permit for review prior to issuance.
- A copy of the preliminary analysis conducted by the Air Pollution Control Division (APCD).

This notice is valid for five (5) years unless a significant change is made, such as an increased production, new equipment, change in fuel type, etc. An APEN update shall be filed no less than 30 days prior to the expiration date of this APEN form.

Mail this form along with a check for \$152.90 to: **Colorado Department of Public Health & Environment  
APCD-SS-B1  
4300 Cherry Creek Drive South  
Denver, CO 80246-1530**

For guidance on how to complete this APEN form, please contact the Air Pollution Control Division or SBAP at the phone numbers listed below.

**Air Pollution Control Division:** (303) 692-3150  
**Small Business Assistance Program (SBAP):** (303) 692-3148 or (303) 692-3175

In addition, the following information may be obtained by accessing the Internet:

Copies of this APEN form at: <http://www.cdphe.state.co.us/ap/downloadforms.html>  
 Status of your permit application review at: <http://www.cdphe.state.co.us/ap/ss/sspcpt.html>

Form Revision Date: April 26, 2006



## Robyn Kullas

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**From:** Robyn Kullas <Kullas@pinyon-env.com>  
**Sent:** Wednesday, April 17, 2013 11:30 AM  
**To:** Huber, Diana (diana.huber@state.co.us)  
**Subject:** Request for Files

Hi Diana,

How are things? I have a site in Glenwood Springs I'm performing an enviro assessment on. Do you have any files for the following addresses/facilities?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
3. 810 Grand Avenue, Glenwood Springs, CO (Corky's gas)
4. 115 6<sup>th</sup> Street, Glenwood Springs, CO (Sunlight Motors)
5. 210 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
6. 105 6<sup>th</sup> Street, Glenwood Springs, CO (Kum + Go/Gilcomart)
7. 216 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas  
Environmental Scientist  
Cell: 303.601.6131  
Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



## Robyn Kullas

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**From:** Huber - CDPHE, Diana <diana.huber@state.co.us>  
**Sent:** Wednesday, April 17, 2013 3:05 PM  
**To:** Robyn Kullas  
**Subject:** Re: Request for Files

Robyn,

Hope you are staying dry. The only address that has information is the first one @ 106 6th Street, Glenwood S[prings. There was a inspection at Amoco June 17, 2003. Amoco was found to be in compliance. The latest info for that address shows it as a waste tire hauler. The registered name is Classical Gas, Inc.

On Wed, Apr 17, 2013 at 11:30 AM, Robyn Kullas <[Kullas@pinyon-env.com](mailto:Kullas@pinyon-env.com)> wrote:

Hi Diana,

How are things? I have a site in Glenwood Springs I'm performing an enviro assessment on. Do you have any files for the following addresses/facilities?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
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7. 216 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas

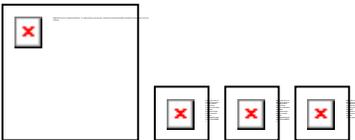
Environmental Scientist

Cell: 303.601.6131

Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



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Diana Huber  
Public Records Search Specialist  
Administration Program Unit  
Colorado Department of Public Health and Environment  
4300 Cherry Creek Drive South  
Denver, CO 80246-1530  
Phone: 303-692-3331 | Fax: 303-759-5355  
[diana.huber@state.co.us](mailto:diana.huber@state.co.us)



## Robyn Kullas

---

**From:** Robyn Kullas <Kullas@pinyon-env.com>  
**Sent:** Wednesday, April 17, 2013 11:34 AM  
**To:** CDPHE - Water Quality Control (cdphe.wqrecordscenter@state.co.us)  
**Subject:** Request for Files

Hello,

I'm performing an environmental assessment for a site in Glenwood Springs. Do you have any files for the following addresses/facilities?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
3. 810 Grand Avenue, Glenwood Springs, CO (Corky's gas)
4. 115 6<sup>th</sup> Street, Glenwood Springs, CO (Sunlight Motors)
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6. 105 6<sup>th</sup> Street, Glenwood Springs, CO (Kum + Go/Gilcomart)
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8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas  
Environmental Scientist  
Cell: 303.601.6131  
Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



## Robyn Kullas

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**From:** WQRecordsCenter, cdphe <cdphe.wqrecordscenter@state.co.us>  
**Sent:** Thursday, April 18, 2013 2:25 PM  
**To:** Robyn Kullas  
**Subject:** Re: Request for Files

Hello Robyn,

I have not located any responsive documents, to the addresses provided, within the Water Quality Control Division databases.

Thanks,

Frank

Frank Dale  
Records Manager  
Water Quality Control Division  
Colorado Department of Public Health and Environment  
4300 Cherry Creek Drive South  
Denver, CO 80246-1530  
303-692-3565 | [CDPHE.WQRecordsCenter@state.co.us](mailto:CDPHE.WQRecordsCenter@state.co.us)

On Wed, Apr 17, 2013 at 11:34 AM, Robyn Kullas <[Kullas@pinyon-env.com](mailto:Kullas@pinyon-env.com)> wrote:

Hello,

I'm performing an environmental assessment for a site in Glenwood Springs. Do you have any files for the following addresses/facilities?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
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8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)

9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas

Environmental Scientist

Cell: 303.601.6131

Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



## Robyn Kullas

---

**From:** Robyn Kullas <Kullas@pinyon-env.com>  
**Sent:** Monday, April 22, 2013 4:03 PM  
**To:** 'barbara.morris@cogs.us'  
**Subject:** Records Request

Hi Barbara,

I am performing an environmental assessment for a few properties in Glenwood Springs. Do you have any records regarding spills, releases, or underground storage tanks at the following locations?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
3. 810 Grand Avenue, Glenwood Springs, CO (Corky's gas)
4. 115 6<sup>th</sup> Street, Glenwood Springs, CO (Sunlight Motors)
5. 210 6<sup>th</sup> Street, Glenwood Springs, CO (Red Mountain Texaco)
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8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas  
Environmental Scientist  
Cell: 303.601.6131  
Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



## Robyn Kullas

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**From:** Barbara Morris <barbara.morris@cogs.us>  
**Sent:** Wednesday, April 24, 2013 3:29 PM  
**To:** Robyn Kullas  
**Subject:** RE: Records Request

Hi Robyn,

I just completed a search on these addresses.

None of the addresses have had any kind of incident related to hazardous materials or any sort of environmental impact from 1982 to today that I am able to verify.

Barbara Morris

Administrative Assistant - Glenwood Springs Fire Department  
101 W. 8th Street, Glenwood Springs, CO 81601  
970-384-6436 Phone - 970-945-8506 Fax



---

**From:** Robyn Kullas [mailto:Kullas@pinyon-env.com]  
**Sent:** Monday, April 22, 2013 4:03 PM  
**To:** Barbara Morris  
**Subject:** Records Request

Hi Barbara,

I am performing an environmental assessment for a few properties in Glenwood Springs. Do you have any records regarding spills, releases, or underground storage tanks at the following locations?

1. 106 6<sup>th</sup> Street, Glenwood Springs, CO (AMOCO/Shell)
2. 116 6<sup>th</sup> Street, Glenwood Springs, CO (Shell)
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8. 331 5<sup>th</sup> Street, Glenwood Springs, CO (cleaners)
9. 101 W. 6<sup>th</sup> Street, Glenwood Springs, CO (Swallow Oil)

Thanks!

Robyn

Robyn Kullas  
Environmental Scientist  
Cell: 303.601.6131  
Email: [kullas@pinyon-env.com](mailto:kullas@pinyon-env.com)



Corporate Headquarters  
9100 West Jewell Avenue, Suite 200  
Lakewood, CO 80232  
TEL 303 980 5200 FAX 303 980 0089  
[www.pinyon-env.com](http://www.pinyon-env.com)

DEPARTMENT OF LABOR AND EMPLOYMENT  
 Division of Oil and Public Safety  
 633 17<sup>th</sup> Street, Suite 500  
 Denver CO 80202-3610  
 (303) 318-8525; Fax (303) 318-8529  
[ops.filereview@state.co.us](mailto:ops.filereview@state.co.us)

## Public Records Center File Review Request Form

Please note the day, date, and time when you wish to view the selected files. Send this completed form via email, fax, or postal mail to the address above. We will call to confirm your visit.

<b>NAME:</b>		Robyn Kullas						
<b>COMPANY:</b>		Pinyon Environmental						
<b>ADDRESS:</b>		9100 W. Jewell Avenue						
<b>CITY:</b>		Lakewood		<b>STATE:</b>		CO	<b>ZIP:</b>	80232
<b>PHONE:</b>				<b>E-MAIL</b>				
Monday	Tuesday	Wednesday	Thursday	Friday		<b>Date:</b>	<b>Time:</b>	
X						04-22-13	1:30pm	

### SITE INFORMATION

SITE NAME: Glenwood Shell		FAC # 10444
ADDRESS: 106 6 <sup>th</sup> Street		EVENT #
CITY: Glenwood Springs		
SITE INFORMATION? Multiple Event Id's		
SITE NAME: Sunlight Motors		FAC # 11467
ADDRESS: 115 6 <sup>th</sup> Street		EVENT #
CITY: Glenwood Springs		
SITE INFORMATION?		
SITE NAME: Kum & Go		FAC #8906
ADDRESS: 105 6 <sup>th</sup> Street		EVENT #
CITY: Glenwood Springs		
SITE INFORMATION? Multiple Event Id's		
SITE NAME: Red Mountain Texaco		FAC # 1652
ADDRESS: W 6 <sup>th</sup> Street		EVENT #
CITY: Glenwood Springs		
SITE INFORMATION? Multiple Event Id's		
SITE NAME: Swallow Oil		FAC # 3879
ADDRESS: 101 W 6 <sup>th</sup> Street		EVENT #
CITY: Glenwood Springs		
SITE INFORMATION:		
SITE NAME: Way Station		FAC # 11576

ADDRESS: 6 <sup>th</sup> and Pine	EVENT #
CITY: Glenwood Springs	
SITE INFORMATION:	

\*\* If known, please enter FAC (Facility) # , Event # and/or SARDONYX Code.

SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Water Resources



**TELEPHONE LOG**

Date: April 18, 2013

Time: 12:50 p.m.

Person Making Call: Amanda Cushing      Representing: Pinyon

Person Being Called: Eddie Rubin  
Representing: Division of Natural Resources

Subject: Water resources within the study area

Discussion:
Mr. Rubin stated that no public water supply sources and no water supply protection areas are present within the study area. Mr. Rubin suggested that I consult the water well maps maintained by the Colorado Division of Water Resources for water wells within the study area. In addition, Mr. Rubin stated that the drinking water within the Glenwood Springs area is collected from Grizzly Creek and No Name Creek.

Signature: 

Date: April 18, 2013



SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Relocation/Right-of-Way Impacts



# STATE OF COLORADO

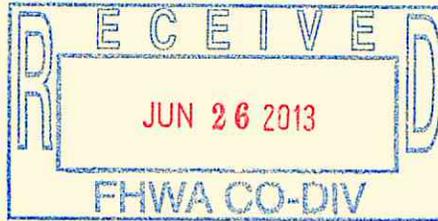
## DEPARTMENT OF TRANSPORTATION

Region 3  
222 South Sixth Street, Room 317  
Grand Junction, CO 81501-2769



June 19, 2013

Mr. John M. Cater  
Attn: Eva LaDow, P.E.  
Federal Highway Administration  
12300 West Dakota Ave, Suite 180  
Lakewood, Colorado 80228



**Re: FHWA Concurrence regarding permissible design activities during the NEPA Process  
SH 82 Grand Avenue Bridge Project (FBR 0821-094, 18158)**

Dear Mr. Cater:

In accordance with Federal Highway Administration's (FHWA) Order 6640.1A and the Colorado Department of Transportation's (CDOT) Design Bulletin 2011 Number 1, CDOT hereby requests FHWA's concurrence to allow CDOT to go beyond the normal Scope of Preliminary Design for the State Highway (SH) 82 Grand Avenue Bridge Project. These design efforts would focus on the following subject area(s):

- SH 82 Grand Avenue Bridge replacement
- Replacement of the existing pedestrian bridge
- 6th and Laurel Avenue intersection reconfiguration
- Pedestrian/bike connections
- Construction detour
- I-70 on and off ramps at Exits 114 and 116
- Construction site accesses

We believe this is permissible for the above project activities for the following reasons:

1. The activities are necessary to identify impacts and mitigation in the NEPA process. Resource categories that may benefit from this additional design include Section 4(f), visual, economic, and water resources. The Grand Avenue Bridge, the pedestrian bridge, and several of the pedestrian/bike connections all affect Section 4(f) resources. All design elements affect visual resources and economic resources, and the bridge improvements affect the Colorado River within the ordinary high water mark.
2. The activities are beneficial to enhance the project schedule. The SH 82 Grand Avenue Bridge project will use the Construction Manager/General Contractor (CM/GC) project delivery method. This approach allows for increased collaboration between CDOT, the design consultant, and the construction contractor, who is currently under contract. This innovative delivery method has

been selected due to the complicated set of conditions in this project area. Their involvement is intended to reduce risk of final design changes and ensure the constructability of the design. It is also hoped that it will compress the schedule for the overall project. Their involvement at this stage will be to provide consulting advice that may at times include activities that exceed the traditional definition of preliminary engineering. Allowing the advancement of these activities will help minimize impacts to SH 82 Grand Avenue traffic from bridge closure during construction, which is a major economic and traffic congestion concern of project stakeholders and will increase the likelihood of a successful CM/GC delivery process.

In addition, CDOT is pursuing the early acquisition of property in order to advance the start of construction activities. Coordinating right-of-way activities more successfully with other key project development actions can substantially shorten project delivery. CDOT will be at risk for this acquisition, in accordance with 23 CFR 710.501. Permitting the advancement of the design will help CDOT minimize risk to the schedule and identify impacts associated with this parcel. It will also allow CDOT to determine the precise amount of property needed for acquisition.

The activities do not affect the NEPA decision. The study team has evaluated numerous build alternatives and after considering many factors has identified a Preferred Alignment that will be evaluated in detail in the Environmental Assessment. Advancing these design activities would assist the team in defining important impact related elements of the Preferred Alternative and to identify mitigation. This will not prevent FHWA and CDOT from making an impartial decision on the appropriate course of action. The SH 82 bridge is the only main line alternative and the pedestrian bridge options are sufficiently similar that this additional design will actually improve our ability to compare alternatives.

The activities to be pursued do not materially affect the objective consideration in the NEPA review process and are not of such a significant nature as to commit to any particular course of action.

Sincerely,



Joseph Elsen, P.E.  
CDOT Region 3 Program Engineer



Michael Vanderhoof  
CDOT Region 3 Planning and Environmental Manager

I concur: Eva Ladow  
for John M. Cater  
FHWA Division Administrator

Date: 07-01-2013

Copy: Roland Wagner, CDOT Project Manager  
Vanessa Henderson, Environmental Programs Branch NEPA Manager

SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Other Resources — Archaeological





U.S. Department  
of Transportation  
**Federal Highway  
Administration**

**Colorado Division**

January 16, 2014

12300 W. Dakota Ave., Ste. 180  
Lakewood, Colorado 80228  
720-963-3000

Mr. Jimmy R. Newton, Jr., Chairman  
Southern Ute Indian Tribe  
P.O. Box 737  
Ignacio, CO 81137

**Subject: Request for Section 106 Consultation, State Highway 82 Grand Avenue Bridge  
Environmental Assessment, Garfield County, Colorado**

Dear Chairman Newton:

The Federal Highway Administration (FHWA) and Colorado Department of Transportation (CDOT), in cooperation with the City of Glenwood Springs, Colorado, are preparing an Environmental Assessment (EA) that will address the effects of the proposed replacement of the State Highway 82/Grand Avenue Bridge and adjacent pedestrian bridge, which span Interstate 70 and the Colorado River. Replacing these structures will address functional and structural deficiencies of the vehicle bridge and provide a safe, secure and effective multimodal connection from downtown Glenwood Springs across the river and interstate to the historic hot springs area. Pursuant to the National Environmental Policy Act (NEPA) of 1969 and the Council on Environmental Quality (CEQ) implementing regulations (40 CFR 1500-1508), FHWA and CDOT are documenting the potential social, economic and environmental consequences of this action.

FHWA will serve as the lead agency for this undertaking and CDOT staff will facilitate the tribal consultation process. As a consulting party under the Section 106 regulations, you are offered the opportunity to identify concerns about cultural resources and comment on how the project might affect them. Further, if it is found that the project will impact cultural resources that are eligible for inclusion on the National Register of Historic Places and are of religious or cultural significance to your tribe, your role in the consultation process would include participation in resolving how best to avoid, minimize, or mitigate those impacts. It is our hope that by describing the proposed undertaking we can be more effective in protecting areas important to American Indian people.

The project area is located entirely within the City of Glenwood Springs and is therefore completely disturbed by commercial and residential development (see attached map). **No sites exhibiting direct evidence of Native American occupation are present within or near the project, although the natural hot springs are known to have been used by native peoples for many years.** Any information you may have regarding places or sites important to your tribe that are located in proximity to the project area would assist us in our efforts to comprehensively identify and evaluate historic properties.

We are committed to ensuring that tribal governments are informed of and involved in decisions that may impact places with cultural significance. If you have specific interest in the Grand Avenue Bridge project, please complete and return the enclosed Consultation Interest Response Form to CDOT Native American consultation liaison Dan Jepson **within 30 days** via US Mail, fax or email, as listed at the bottom of that sheet. The 30-day period has been established to encourage your participation at this early stage in project development. Failure to respond within this time frame will not prevent your tribe from entering consultation at a later date. However, studies and decision making will proceed and it may be difficult to reconsider previous determinations or findings, unless significant new information is introduced.

If you have questions or concerns about the project or the role of your tribe in the consultation process, please contact Dan Jepson at 303-757-9631 or [daniel.jepson@state.co.us](mailto:daniel.jepson@state.co.us), or FHWA Colorado Division Environmental Program Manager Stephanie Gibson at 720-963-3013 or [stephanie.gibson@dot.gov](mailto:stephanie.gibson@dot.gov).

Thank you for considering this request for consultation.

Sincerely Yours,

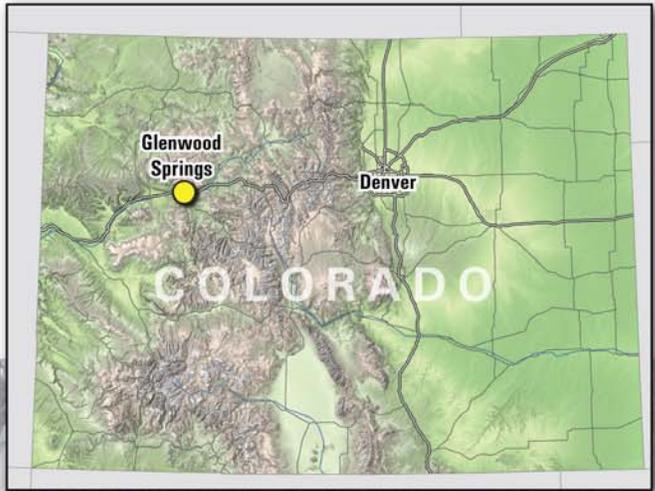


John M. Cater, P.E.  
Division Administrator

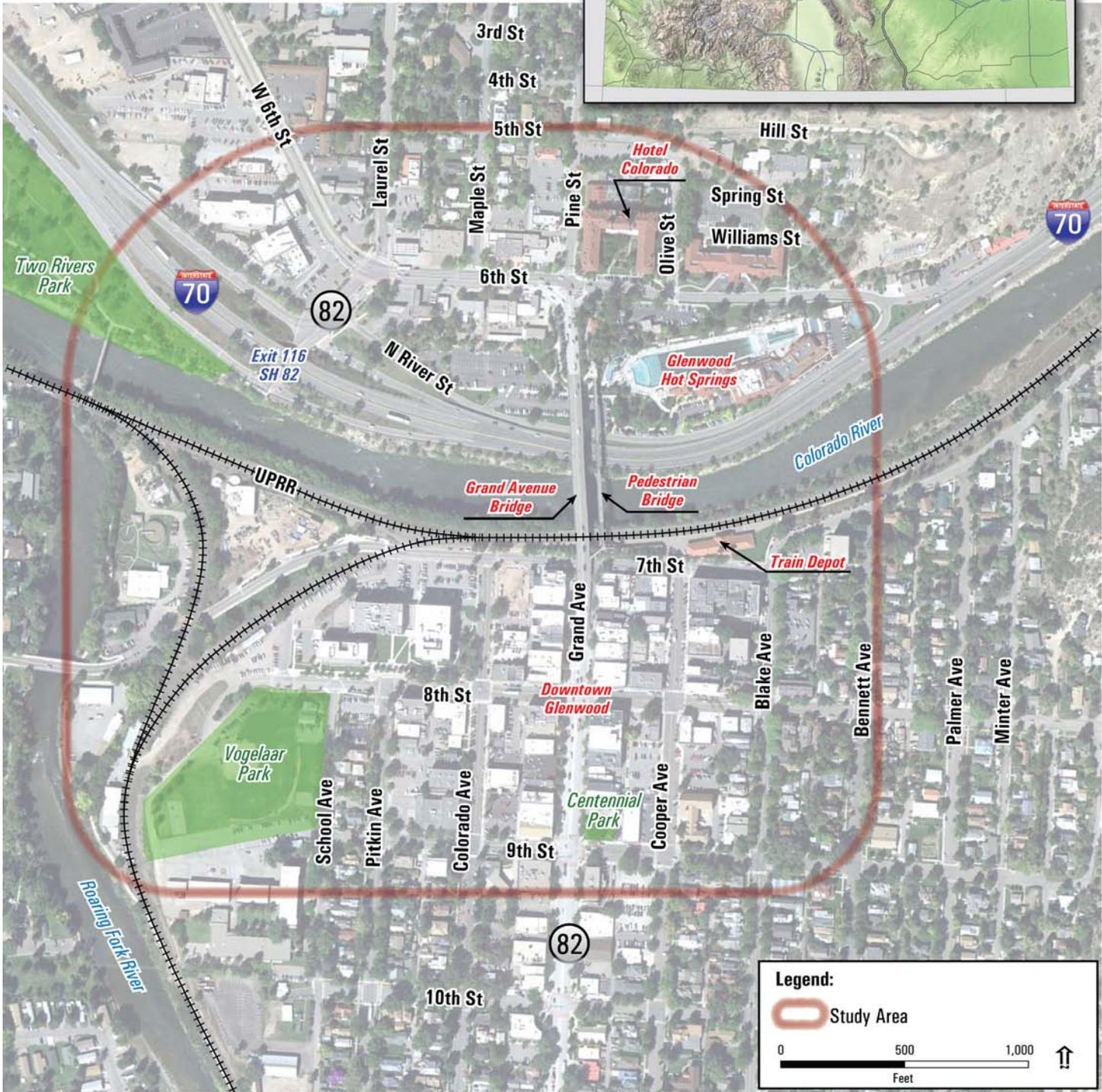
By: Stephanie Gibson  
Environmental Program Manager

Enclosures: Maps Showing Project Area  
Consultation Interest Response Form

Cc: S. Gibson & E. LaDow, FHWA  
M. Vanderhoof, CDOT Region 3  
J. Clark, Jacobs  
A. Naranjo, Southern Ute Indian Tribe



**Study Area**



**TRIBAL MAILING LIST**  
**SH 82 Grand Ave. Bridge EA**

<b>Tribal Chair (Primary Contact):</b>	<b>Send Copy of Letter and Attachments to:</b>
Mr. Jimmy R. Newton, Jr., Chairman Southern Ute Indian Tribe P.O. Box 737 Ignacio, CO 81137	Mr. Alden B. Naranjo, NAGPRA Coordinator Southern Ute Indian Tribe P.O. Box 737 Ignacio, CO 81137
Mr. Gordon Howell, Chairman Uintah & Ouray Tribal Business Committee P.O. Box 190 Ft. Duchesne, UT 84026	Ms. Betsy Chapoose, NAGPRA Representative Ute Indian Tribe (Uintah & Ouray Reservation) P.O. Box 190 Ft. Duchesne, UT 84026
Mr. Manuel Heart, Chairman Ute Mountain Ute Tribe P.O. Box 248 Towaoc, CO 81334	Mr. Terry Knight, Sr., THPO Ute Mountain Ute Tribe P.O. Box 468 Towaoc, CO 81334
<b>CDOT Region &amp; Consultant (Send copies of one letter and the mailing list to):</b>	
Mike Vanderhoof, CDOT Region 3 RPEM	
Mr. Jim Clark, Project Manager Jacobs 707 17 <sup>th</sup> St., Ste. 2300 Denver CO 80202-5131	

SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Wetlands and Waters of the United States



## Meeting Minutes

**Project:** CDOT Region 3 – SH 82 Grand Avenue Bridge

**Purpose:** Waters of the United States Impacts/Section 404 Issues

**Date Held:** February 21, 2014

**Location:** Conference Call

**Attendees:** **USACE:** Nathan Greene, Susan Nall  
**CDOT:** Mike Vanderhoof, Paula Durkin  
**TSH:** Clint Krajnik  
**Jacobs:** Jim Clarke, Ben Eddy

**Copies:** Attendees, File.

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### SUMMARY OF DISCUSSION:

1. Re-opened project-related communication with USACE Grand Junction Regulatory Office staff.
2. CDOT/Jacobs provided USACE with project background including Purpose and Need, scoping, and stakeholder coordination. Project team underwent a detailed alternatives process that resulted in the Preferred Alternative. Described elements of this alternative.
3. Project would provide much stormwater management and treatment, so should improve water quality in river. Currently, stormwater generally drains from bridge deck to river.
4. Summarized SHPO and Section 7 consultations and agency-coordination history. Have received a “Not Likely to Adversely Affect” from USFWS.
5. CDOT has met several times with CO Parks and Wildlife regarding impacts, including trout spawning and related seasonal restrictions. Dan Cacho is primary contact.
6. Discussed latest design plans and waters of the US (WUS) impacts associated with proposed project.
7. CDOT/Jacobs provided USACE with scale of permanent impacts associated with bridge piers and stormwater outfalls; provided scale of temporary impacts associated with cofferdams.

8. CDOT/Jacobs provided USACE with environmental setting information including the vegetative and hydrologically disturbed nature of Colorado River adjacent to proposed project area. Explained absence of wetlands adjacent to proposed project area.
9. CDOT/Jacobs described riparian impacts and future need for a restoration plan in compliance with SB requirements. Riparian corridor mostly comprised of invasives, so project provides an opportunity for improving corridor.
10. Sue and Nathan discussed potential permit options: Even though impacts are below thresholds for Nationwide permit(s), Corps can opt to elevate to an Individual Permit. If Nationwide Permit issued, potentially NWP 14, 3 combined with 33, or some combination of the three types.

**ACTION ITEMS:**

<b>Task</b>	<b>Responsible</b>
Assess project and provide information <i>re</i> potential permit type(s).	USACE
Consult with hydraulic engineers; try to minimize cofferdam impacts.	Jacobs
Determine exact square footage for fill associated with outfalls, cofferdams, and bridge piers within WUS.	Jacobs

J:\\_Transportation\WVXX1306\_GrandAve\meetings\CDOT\Grand Ave Bridge\_USACE\_2.21.14\_ DRAFT Mtg Min.doc

SH 82

GRAND AVENUE BRIDGE

Environmental Assessment

## Section 6(f)



From: **Halouska - CDOT, Troy** <[troy.halouska@state.co.us](mailto:troy.halouska@state.co.us)>  
Date: Wed, Sep 17, 2014 at 9:43 AM  
Subject: Section 6(f) Question and Boundary Concurrence  
To: Thomas Morrissey - DNR <[Thomas.Morrissey@state.co.us](mailto:Thomas.Morrissey@state.co.us)>, "Gose, Melanie" <[Melanie.Gose@state.co.us](mailto:Melanie.Gose@state.co.us)>

Hello Tom and Melanie,

I have a project in Glenwood Springs that is near a 6(f) park. We were going to meet about it months ago, but the meeting got canceled because the project team was still deciding impacts. Anyway, we now know that the project will not impact the park directly and therefore there will not be a conversion. However, one of the access roads to the park will be temporarily regraded. This impact is temporary and there are other access points to the park. I do not believe we need to do anything for 6(f), but wanted to just run this by you to make sure.

Also, the project team created a graphic showing their interpretation of the project boundary. I want to verify with you that their boundary interpretation is accurate.

Attached are the two boundary maps that Melanie sent me, and a pdf of the appropriate pages from the NEPA document that discuss Section 6(f). On the second page of the pdf you will see Figure 3-27 that shows the area of the roadway and access point that will be regraded, and the 6(f) boundary. The highlighted area on the third page discusses the impacts.

Please let me know if you have any comments or concerns, and that you agree with the boundary shown in Figure 3-27.

Feel free to give me a call if you would like to discuss further.

Thanks much,

Troy Halouska  
Planning and Environmental Linkages Program Manager, 4(f)/6(f) Specialist



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From: **Morrissey - DNR, Thomas** <[thomas.morrissey@state.co.us](mailto:thomas.morrissey@state.co.us)>  
Date: Wed, Oct 8, 2014 at 11:16 AM  
Subject: Fwd: Section 6(f) Question and Boundary Concurrence  
To: Troy Halouska <[troy.halouska@state.co.us](mailto:troy.halouska@state.co.us)>  
Cc: Melanie Gose <[Melanie.Gose@state.co.us](mailto:Melanie.Gose@state.co.us)>, Eric Drummond <[eric.drummond@state.co.us](mailto:eric.drummond@state.co.us)>

Troy,

CPW has reviewed CODOT's boundary delineation for the Glenwood Springs Park with CPW's project files and we are in complete agreement on the project boundaries.

Given that our assessments of the project boundaries are in agreement and the only direct impact the bridge replacement project on the area are CODOT's plans to regrade one of a number of access roads into the park, the bridge replacement project as proposed by CODOT will not have a direct impact on the park nor will the project involve the Section 6(f) protections afforded to that park by the prior LWCF investment.

Please keep CPW informed as this project proceeds and please be aware that should the project be modified in a manner that will have a direct impact on the subject park this determination will be null and void.

Thank you for your attention to this matter.

Thomas Morrissey, PE  
Colorado's LWCF Program State Liaison Officer