

US 24 “I-25 to Ridge Road” Aesthetic Guidelines

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Colorado Department of Transportation
Colorado Springs, Colorado

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SECTION ONE: Introduction/Project Map

US 24 has a long history as a transportation corridor that goes back to the Native Americans and prospectors who once used this route looking for food and wealth. Ute Pass, as it was once known, has gone from a dirt path to a four-lane limited access highway that provides local trips for daily travelers, an entryway for recreational mountain activities and a transportation route for regional commuters and freight haulers. Over the years, the populations of the Colorado Springs and Manitou Springs areas have grown and so has its appeal as a tourist destination, increasing the highway's use. Since 1964, only minimal highway improvements have been done in response to that increase to address safety and maintenance issues.

Together, the Colorado Department of Transportation (CDOT) and the Federal Highway Administration (FHWA) have taken steps to redesign sections of the highway. They have studied the current and future

traffic demands, along with highway alignment, to develop a plan that will correct roadway deficiencies and improve safety, mobility and access.

Approximately 4.5 miles of US 24 is expected to undergo infrastructure improvements. The I-25/Cimarron Interchange is the eastern end and just beyond Ridge Road is the western end of the corridor. Two new interchanges will be installed at 8th and 21st Streets. Intersection improvements will be made at 26th and 31st Streets. An overpass at Ridge Road will be constructed and the potential for a privately funded 15th Street overpass are all a part of the plan. In addition, wider shoulders, an additional travel lane for both the east and west bound traffic, new turn lanes, signage, retaining walls and pedestrian bridges are expected. As these improvements are implemented, US 24 will provide a safer and more accessible route for commuters, tourists and residents of the Colorado Springs area.

In conjunction with the redesign and development effort for US 24, CDOT, the City, users and adjacent landowners have joined forces to develop restoration strategies for Fountain Creek. Fountain Creek is such

an integral part of the highway corridor, paralleling US 24 from the I-25/Cimarron Interchange to Manitou Springs. These partners are taking the opportunity to improve water quality, flood risk management, fish habitat, access and recreation.

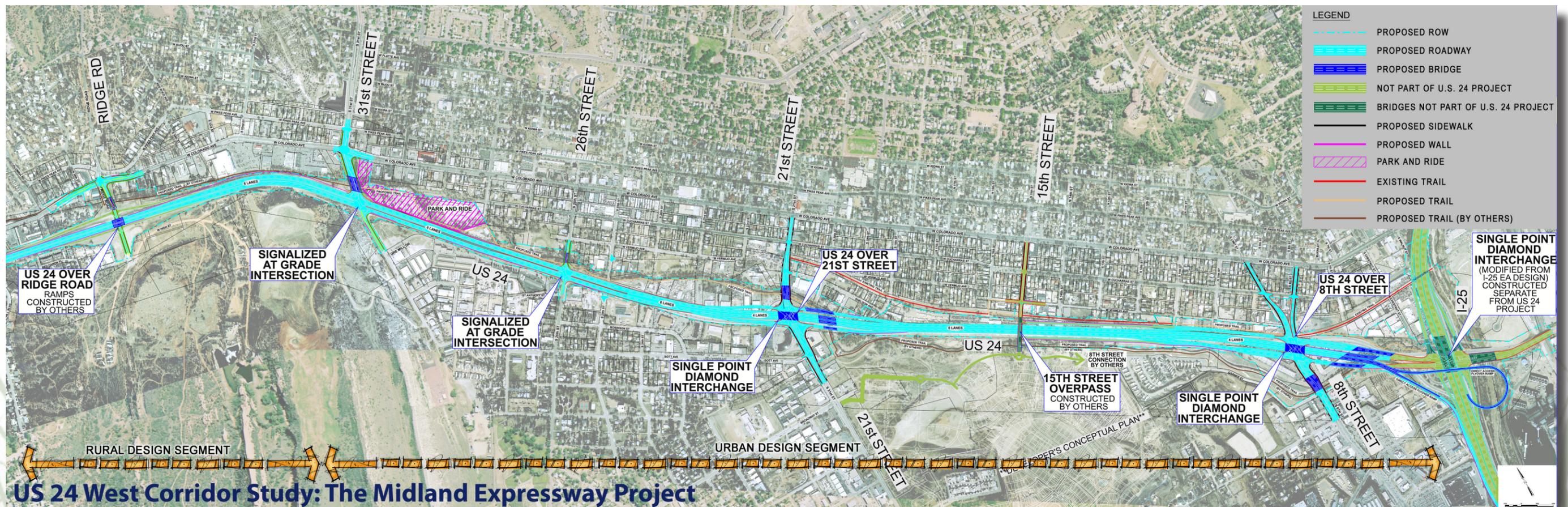
Recreational improvements are envisioned for the existing Midland Regional Trail. This includes the use of excess CDOT right-of-way for parks and creek restoration. This restoration will include landscaping, water features, public gathering spaces and additional bicycle/pedestrian paths. These improvements will involve approximately 2.5 miles of the creek. Please see the Appendix for a more detailed description of the Midland Greenway.

As part of the redesign effort for US 24 and the Fountain Creek corridor, these aesthetic guidelines have been prepared to address the aesthetic treatment of the proposed transportation and creek enhancements. Specifically, these guidelines provide direction on the aesthetic treatment of gateway features, vehicular and pedestrian bridges, noise and retaining walls, sidewalks, trails, landscaping and creek improve-

ments. These guidelines are developed in conjunction with the Environmental Assessment (EA) for the US 24 West Corridor.

The US 24 West Corridor is seen as having two distinct design segments: the Urban Design Segment and the Rural Design Segment. Each segment captures the character and intrinsic qualities of the highway and adjacent Fountain Creek corridor. These qualities were derived from community input, land use, landmarks and development patterns.

The Urban Design Segment begins at the I-25/Cimarron Interchange on the east and continues west to just past 31st Avenue. This segment includes: Old Colorado City, Gold Hill Mesa and the historic railroad roundhouse. The Rural Design Segment includes the area just west of 31st Avenue to just west of Ridge Road. This segment includes Ridge Road and Red Rock Canyon Open Space.



SECTION TWO: Design Intent

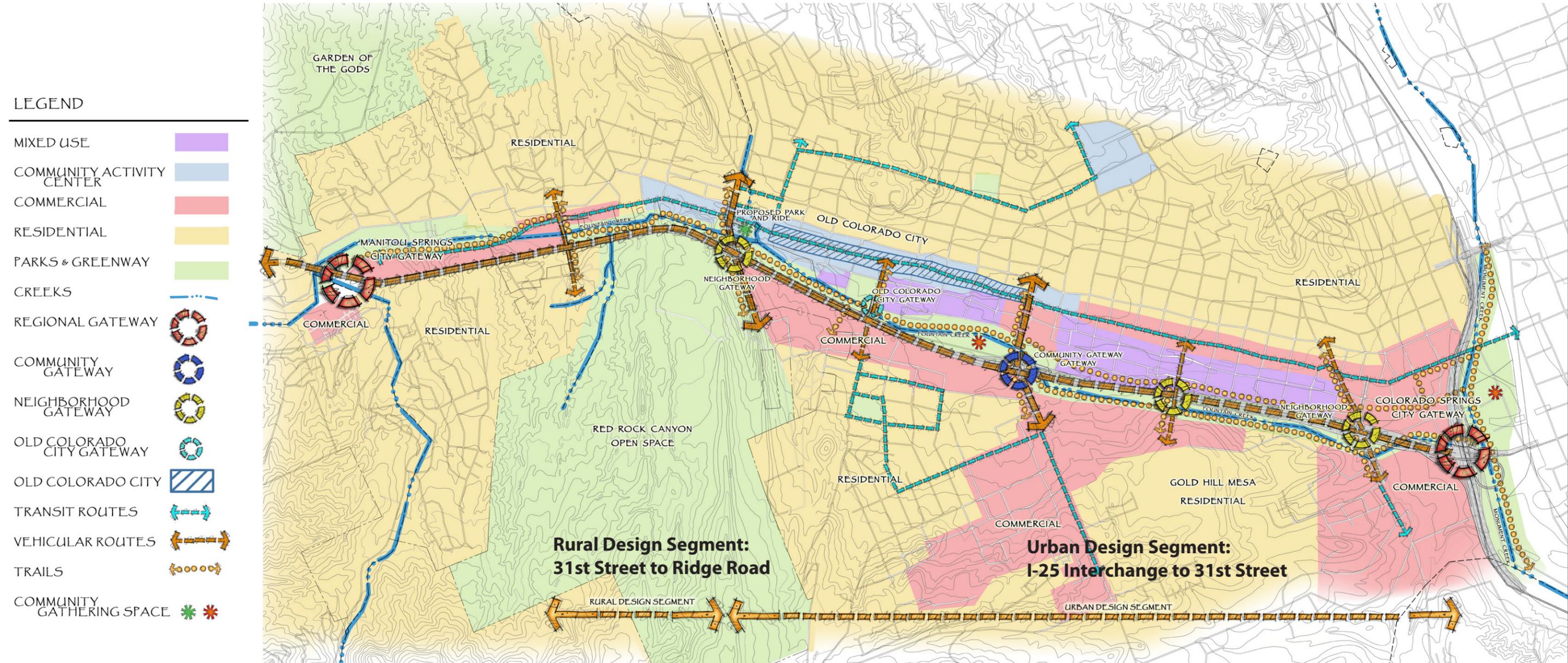
US 24 West Corridor Goals

As neighborhoods grew and traffic volumes increased along US 24, some land uses adjacent to the corridor evolved, and some did not change. Development occurred through the years resulting in many architectural styles and dissimilar land uses adjacent to one another. As a result, today this corridor is very eclectic with no defined character.

Currently, highway infrastructure is a large part of a driver's experience. Bridges and bridge abutments, guardrails, signage and Jersey barriers are features associated with highway development and are typically utilitarian. They lack any defining character, decoration or aesthetic quality.

With future highway improvements, CDOT has an opportunity to implement aesthetic features and elements that will contribute to the overall character of the corridor.

The US 24 West Corridor will have two distinct design segments: the Urban Design Segment and the Rural Design Segment. Each of these will have its own character and identity. Various form, line, color and texture will be used to create identifiable elements that are unique to each design segment. These elements will unify and support the character of each segment.



US 24 West Corridor Functional Diagram

A. Urban Design Segment Goals

The Urban Design Segment has a city feel with an abundance of retail developments and numerous intersections with city cross streets. This section of US 24 is planned to have raised medians defined by curb and gutter. Curb and gutter will also define the edges of the roadway. Forms in this area shall be linear with strong edges.

In conjunction with linear forms, lines will be clearly defined with crisp continuous edges to reinforce the linear elements that exist in the Urban Design Segment.

Colors and textures in the Urban Design Segment shall incorporate those found on the Westside (i.e., downtown Victorian architecture, commercial/retail developments and the surrounding land forms). Generally, these shall be earth tones with a concentration of tans and buffs that are found in the surrounding land forms and mountains. Textures shall reflect the block construction utilized in the area.

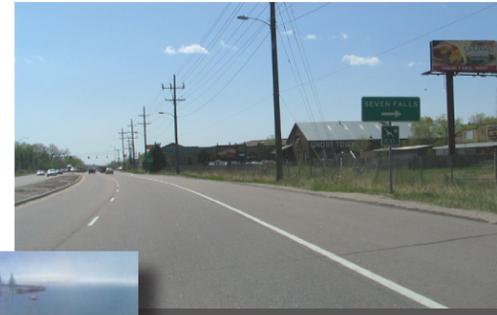
Objectives

1. Focus views on the Fountain Creek corridor and gateways.
2. Frame long range views.
3. Incorporate history, specifically: Victorian, railroad, iron work and mining heritage of the Colorado Springs area.
4. Strengthen the natural visual buffer created by the Fountain Creek riparian vegetation.
5. Emphasize gateways with the juxtaposition of formal environmental landscape against the natural landscape backdrop of Fountain Creek.
6. Create a hierarchy of gateways by the level of emphasis placed on the landscape and aesthetic elements at each gateway. The range of gateways include regional, community and neighborhood.
7. Use materials from the built environment including concrete, block, brick, metal and natural stone as an accent. The natural stone will be a common element in both the Urban and Rural Design Segments, along with the Midland Greenway.
8. Roadway structural elements including bridges, retaining walls, sound walls, etc. will be designed to be seen and make a statement.

These elements will become identifying elements for the Westside.

9. Provide visual clues to the existence of Old Colorado City business district.
10. Fountain Creek bridges on 8th, 21st, 26th and 31st Streets will relate to the creek and reflect the same character as the bridges on Ridge Road in the Rural Design Segment.

Line



Railroad Roundhouse



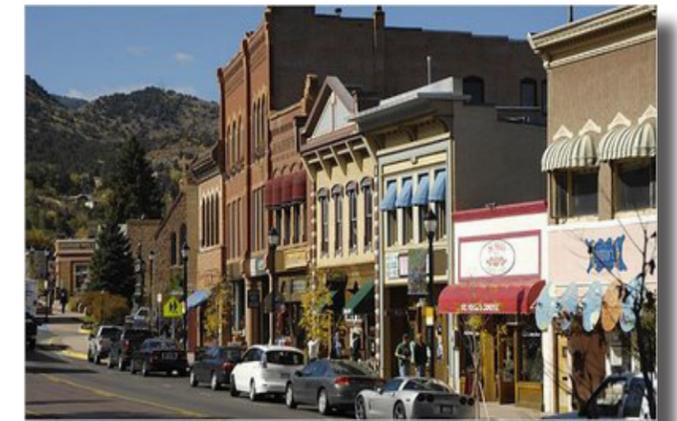
Form



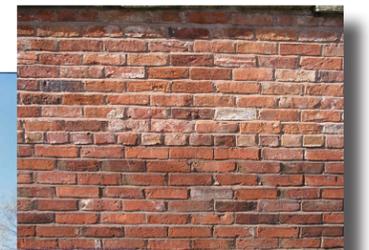
Architecture



Color



Texture



B. Rural Design Segment Goals

The Rural Design Segment of the US 24 Corridor is a stark contrast to the Urban Design Segment to the east.

Due to the expansive long-range views of the mountains, the vast amounts of open space and the lack of development directly abutting the roadway, the Rural Design Segment will have more of an undeveloped or natural feel. West of 31st Street, the roadway improvements will wind and curve around the existing land-forms. The road will contain fewer straight stretches and will respond to the natural undulating topography. The road will have no vertical curbs or hardscape medians. Instead, there will be painted medians and shoulders at grade. These all help to convey a more rural, natural appearance for this segment.

Forms and lines will be more sinuous and curved to reinforce this rural natural concept. Fountain Creek also plays an important role within this design segment.

The predominant color of the rock outcroppings and surrounding views are red oxides. Shades of red will be incorporated with these aesthetic improvements.

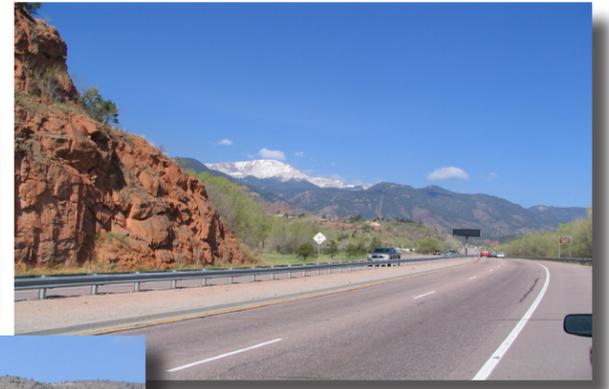
Finally, textures that replicate the rock outcroppings are envisioned here. This is not to say that everything will match the look of the rocks, but rather to imply a more course or rough texture to surfaces.

Objectives

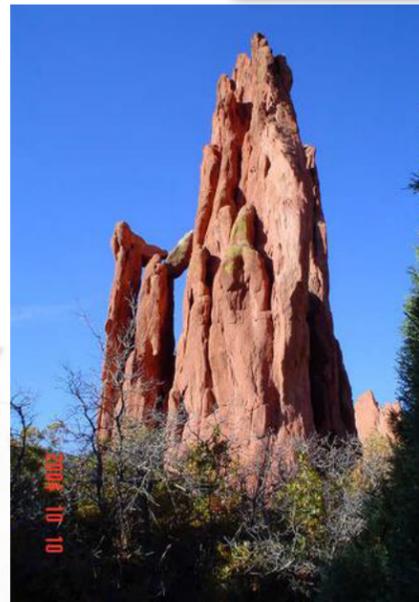
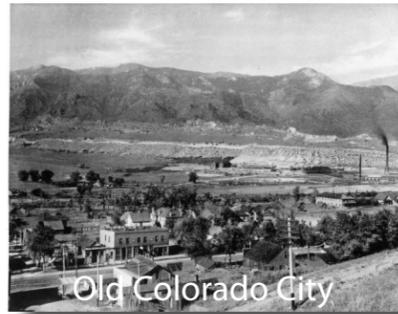
1. Emphasize long-range views to the mountains and mid-range views to Red Rock Canyon Open Space and Garden of the Gods.
2. Blend improvements into the natural setting.
3. Use natural materials and colors.
4. Incorporate history, specifically the stone bridge treatment found on the Westside and Manitou Springs. These treatments will be used on the Ridge Road bridges and all the Fountain Creek bridges including 8th, 21st, 26th and 31st Streets.
5. Emphasize the Ridge Road gateway to Red Rock Canyon Open Space.
6. Incorporate new landscape treatments as naturalized plantings.



Line



Color



Form



Texture

SECTION THREE: Design Guidelines for Aesthetic Treatments

Urban Design Segment

This section of the design guidelines is intended to convey the variety and types of aesthetic treatments that can be used to help connect the highway improvements to the community and neighborhoods along the US 24 West Corridor. Aesthetic treatments within the urban segment will vary from those proposed within the rural segment. This is due, in part, to the fact that proposed highway improvements are not uniform throughout the corridor. The character of the rural segment is less structured than that of the urban segment. The segments will relate to one another so there is a cohesiveness of aesthetic amenities along the entire stretch of US 24. This will be accomplished through the consistent use of a unifying natural accent rock.

A. Gateways

Gateways may be associated with highway bridges, large landscape features, monuments, open water and park areas. They have identifiable features that relate and tie the highway to their surroundings and are usually associated with high visibility areas that signify a main entrance or place.

Different types of gateways are envisioned within this approximate 2.5 mile stretch of corridor. They are identified as regional, community and neighborhood gateways. These will have a hierarchy of design associated with them that is related to the type of gateway and its proposed location.

1. Regional Gateways

Regional gateways are the most significant gateways and therefore, are primary in the hierarchy. Both ends of the US 24 West Corridor have been identified as regional gateways. West bound, the I-25/ Cimarron Street Interchange initiates an arrival sequence to the Rocky Mountains and points west. East bound, the US 24/ Manitou Avenue Interchange initiates the arrival sequence to Colorado Springs. As designers develop the

aesthetic elements for the US 24 West Corridor, these elements must be designed in harmony to strengthen both the east and west bound arrival experiences. Aesthetic improvements will be designed to meet multiple objectives such as regional, community and neighborhood gateway statements.

2. Community Gateway (21st Street)

In the hierarchy of gateways, the second level of gateway is the community gateway. There is one community gateway at 21st Street and it is a major cross road in the US 24 West Corridor. It provides direct regional access to southwest neighborhoods, future retail destinations at Gold Hill Mesa and Old Colorado City. 21st Street presents opportunities where excess highway right-of-way exists to implement significant creek improvements, provides a community gathering space and develops a significant community gateway feature. These concepts were explained as a part of the Midland Greenway Master Plan effort, facilitated CDOT. See the Appendix for a brief discussion of the Midland Greenway Master Plan. The aerial perspective of the 21st Street community gateway concept graphically portrays the vision.

This rendering also demonstrates the use of highway formal plantings juxtaposed against the naturalized plantings of Fountain Creek drainage to emphasize the interchange as a community gateway. Due to the widening of the highway here and the opportunity to create a more substantial entry feature, the existing Miners Statue will be relocated to 26th Street, where it will continue to be a focal point for Old Colorado City.



**Miners statue in its existing
21st Street location**



**Aerial Perspective of 21st Street Community Gateway
Concept**

US 24 is a grade separated interchange over 21st Street with full turn movements occurring below the highway. Fountain Creek crosses beneath US 24 just to the east of 21st Street creating a unique riparian feel to this interchange.

Roadway architectural improvements at this location will be most visible to all vehicular traffic. US 24 highway bridge structures, retaining walls and noise walls will be in keeping with the Victorian architectural heritage of the Westside. See Bridge Treatment Concept Alternatives 'A' and 'B' for the Urban Design Segment on page 12.

of this highway interchange. To blend with the Fountain Creek drainage, berming with ornamental trees and evergreen ground cover, as well as low stone walls, are envisioned immediately adjacent to the on and off ramps at this location. The character of these walls should be in keeping with the stone bridges. All formal landscaping adjacent to US 24 should be selected from the Highway Right-of-way. Xeric and Gateway Planting schedules; Fountain Creek drainage plantings should be selected from the Naturalized and Riparian Planting schedules found in the Appendix.

Urban Segment. As part of the Urban Design Segment bridge, large column elements are proposed. See page 12 for Bridge Treatment Concept Alternatives 'A' and 'B'. These columns can be scaled down to a pedestrian scale and used as free-standing elements. This will unify the visual character of the Urban Design Segment while helping to identify transition points from US 24 into the local neighborhoods.

8th Street

The 8th Street interchange is a part of the larger Cimarron/I-25/US 24 interchange complex. At this location, US 24 goes over 8th Street. This serves as a gateway into local neighborhood areas and retail shopping areas south of US 24.

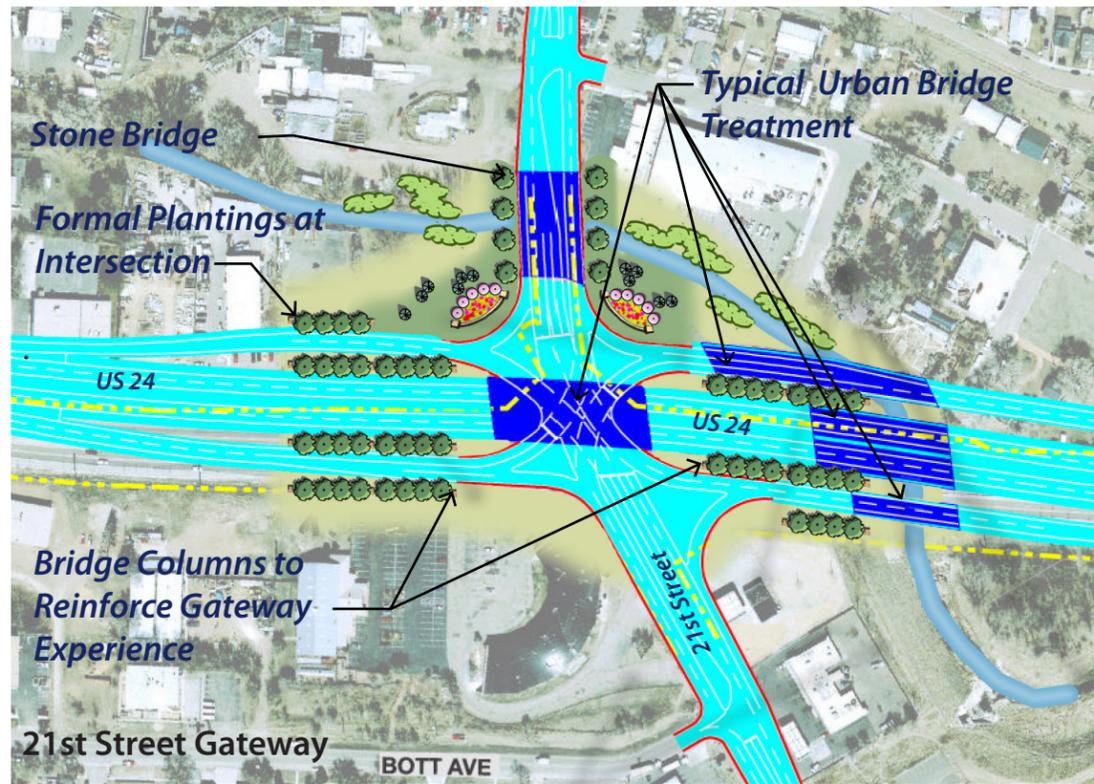
The bridge structures shall have a Victorian theme. See page 12 for Bridge Treatment Concept Alternatives 'A' and 'B'.

Raised bridge columns should be incorporated into the design of the bridge structure so that they extend above the U.S. 24 road section and become a visual architectural element seen by highway travelers.

Smaller versions of the bridge columns can be used along the ramps at 8th Street to signify entrances into the local neighborhoods. Since 8th Street south is a retail shopping area, more attention and excitement should be created in the landscape through the use of color and texture. Colorful banners and landscaping along with sculptures and artwork could be used to announce this shopping area.

Landscaping proposed at 8th Street should be more formal, which is in keeping with the character of the Urban Design Segment. Plant material installed at this location should be selected from the Gateway Plantings Schedule in the Appendix.

Raised medians are planned on 8th Street and should be planted with trees, shrubs and ornamental grasses. This formal planting concept should be carried down along the sides of the 8th Street roadway and along the US 24 right-of-way.



A stone bridge is envisioned to be constructed over Fountain Creek. This bridge will incorporate a natural stone veneer and will arch over the creek. It will use a unifying natural accent rock. The vernacular of this bridge is in keeping with the historic stone bridges on the west side of Fountain Creek. See the Fountain Creek/Ridge Road Stone Bridge perspective in the Rural Design Segment page 19.

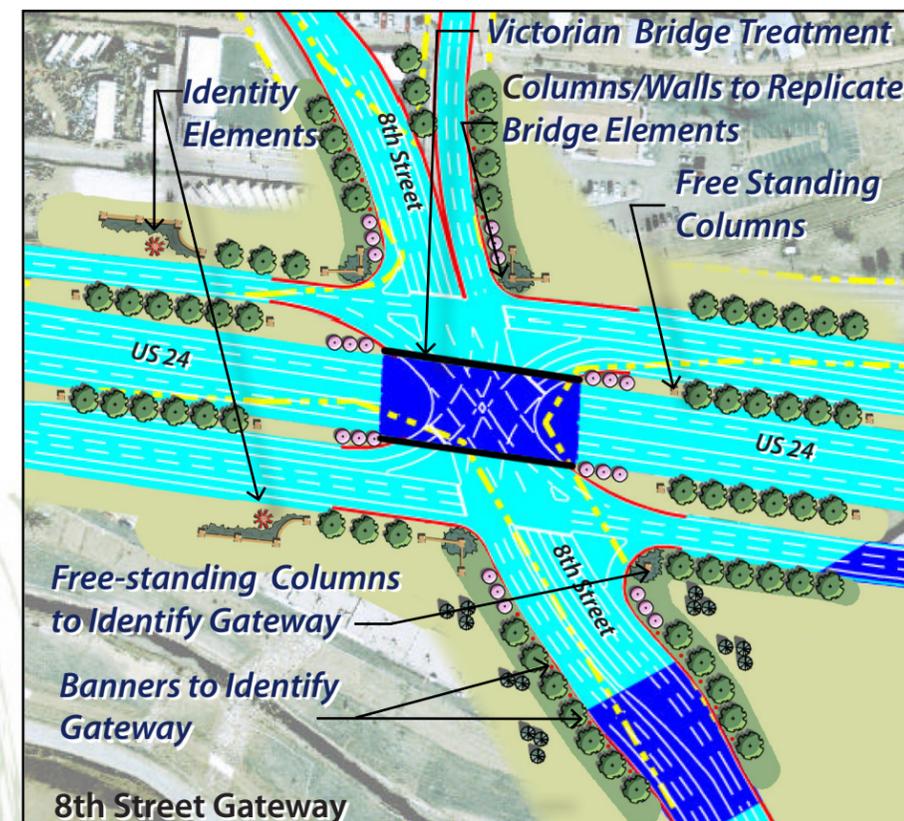
At this gateway, a mixture of formal and riparian plantings should be incorporated into the landscape. Less formalized plantings should occur on the northern side

3. Neighborhood Gateways (8th, 15th and 31st Streets)

Neighborhood gateways are located at 8th, 15th, 26th and 31st Streets. These gateways will serve as markers for local destinations along the US 24 corridor. Being less visible than regional and community gateways, neighborhood gateways will play off the design features used at the community level, but at a smaller scale. Neighborhood gateways also include landscaping and irrigation. They should incorporate the Victorian architectural elements proposed for the



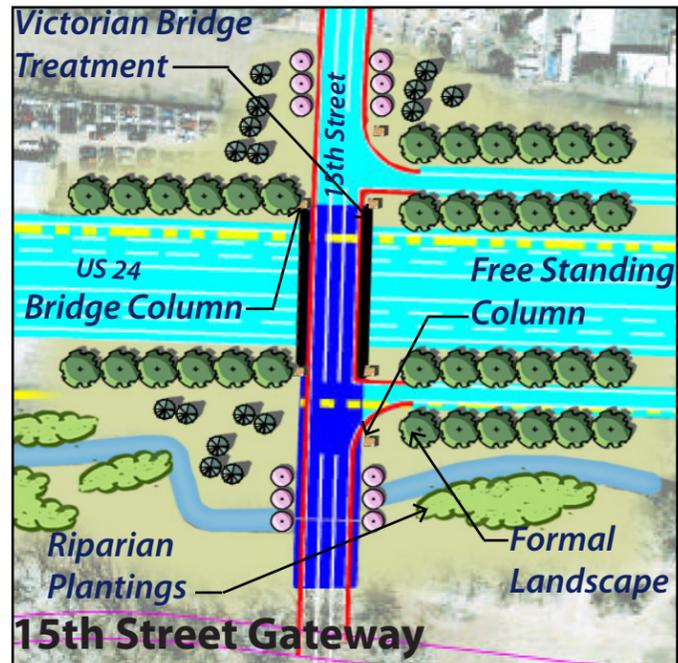
Column elements on bridges can be reduced to a pedestrian scale to identify local neighborhoods.



15th Street

15th Street is the only location along the US 24 West Corridor where a city cross street is on a bridge structure over U.S. 24. It will be constructed by a private developer and will serve to connect the developing area of Gold Hill Mesa with Colorado Avenue.

Due to its location and orientation, this bridge structure provides the best opportunity to showcase the proposed urban design segment Victorian theme. The developer is being encouraged to apply the same design vernacular applied to the 8th Street bridge. Refer to the Urban Design Segment Bridge Treatment Concept alternative 'A' and 'B' on page 12. Due to its high visibility, additional design elements could be incorporated. Elements, such as additional design reveals, more unique column treatments, etc. could all add richness to this bridge structure and gateway.



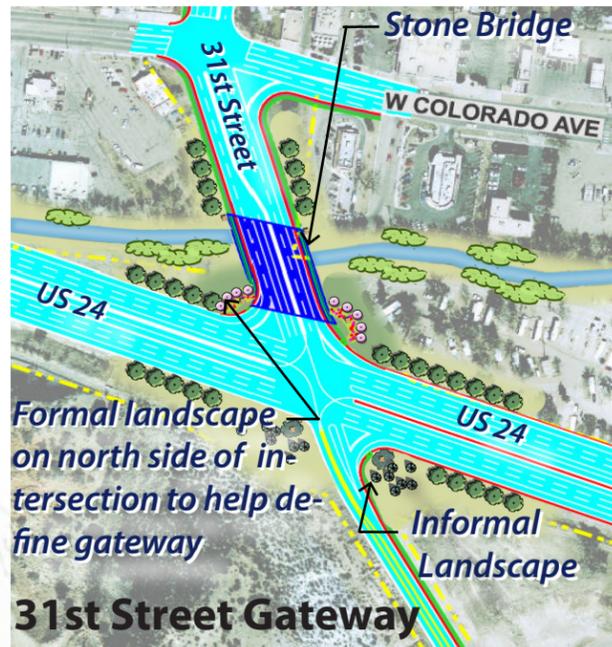
As with all interchanges within the Urban Design Segment, more formal landscape treatments will help to reinforce the gateway concept. Deciduous trees should line US 24 and ornamental trees are envisioned for 15th Street. Due to the proximity of Fountain Creek to this gateway, riparian-type plantings will also be incorporated into the landscape treatment on the southern side of the highway. All proposed landscape material should be selected from the Gateway, Naturalized and Riparian Planting Schedules in the Appendix.

31st Street

This is an at-grade signalized intersection that will serve as a gateway to Garden of the Gods, employment centers and residential neighborhoods north of US 24. This is a major north-south travel corridor connecting US 24 with northern points. For this reason, the gateway statement will be developed on the north side of US 24 using predominantly formal landscaping. Naturalized and riparian vegetation along Fountain Creek will be the backdrop to the formal landscaping. On the south side of US 24, informal xeric landscaping should be used. Evergreen trees and native shrubs should be the key elements of this landscape. This landscape is intended to blend with the native landscape of Red Rock Canyon Open Space.

Another gateway is the proposed bridge over Fountain Creek, just north of the intersection with US 24. As with all bridges crossing Fountain Creek, the actual bridge aesthetic material will be stone veneer. See the Fountain Creek/Ridge Road Stone Bridge on page 19.

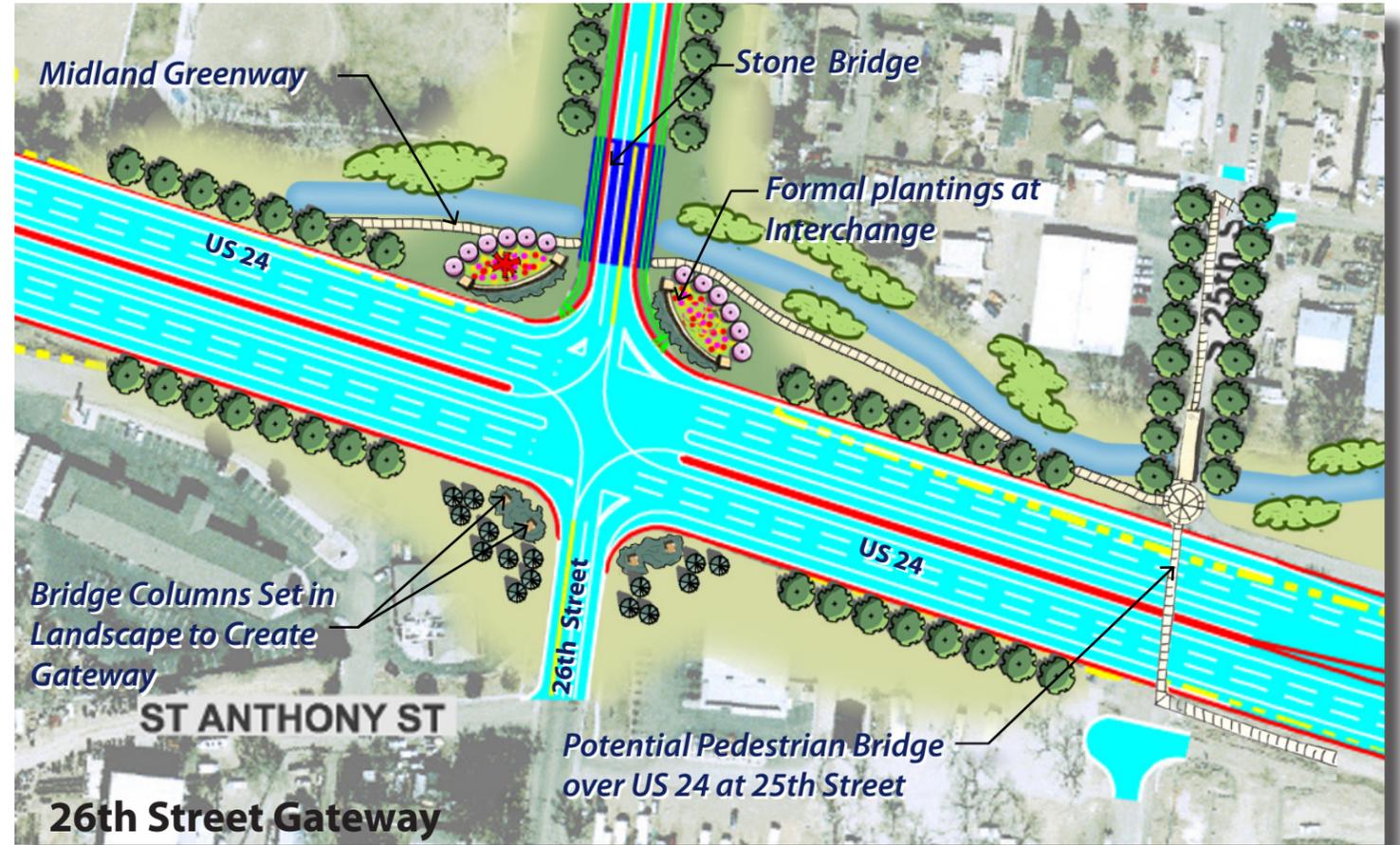
This gateway has the most diverse planting zones of all the gateways, requiring thoughtful selection from all schedules in the Appendix.



Old Colorado City Gateway (26th Street)

This is a unique at-grade signalized intersection that will serve as a neighborhood and main gateway into Old Colorado City. The Miners Statue previously located on the northwest corner of 21st Street will be relocated to this intersection. The scale and placement of this monument at the new location will need to be carefully considered when designing this gateway. Site walls and grading maybe required to give this statue the presentation it deserves.

Formal plantings of trees and shrub masses along US 24 will further enhance this gateway concept. Naturalized plantings occurring along Fountain Creek and the proposed Midland Greenway Trail will serve as a backdrop to the Gateway. Planting around the relocated Miners Statue should provide a good backdrop for the sculpture. Ornamental trees and grasses will assist with the presentation. All landscape plant materials for use at this gateway should be selected from the Gateway, Naturalized and Riparian Plantings Schedules located in the Appendix.



Other gateway features for this intersection will include vertical architectural columns on all four corners. The style of these should be in keeping with the style of the bridges being constructed along US 24 within the Urban Design Segment.

Another gateway element is the proposed bridge over Fountain Creek just north of the intersection with US 24. As with all bridges crossing Fountain Creek, the actual bridge aesthetic materials will be stone veneer. See the Fountain Creek/Ridge Road Stone Bridge on page 19.



Relocated Miners Statue at 26th Street

B. Bridges

Several vehicular bridges are proposed for construction within this design segment. They are not only viewed as a means of vehicular conveyance, but as potential gateways into the surrounding communities adjacent to US 24. They should reflect the historical and Victorian nature of the surrounding area while making a significant statement about the importance of Fountain Creek. As a family of structures, they should all appear to be related, but have individual features that distinguish one bridge from another. Each bridge will have its own features that reflect the US 24 corridor and the heritage of the area.

The idea of having a “family” of structures shall be incorporated into all vehicular and pedestrian bridges within the Urban Design Segment.

Vehicular Bridges

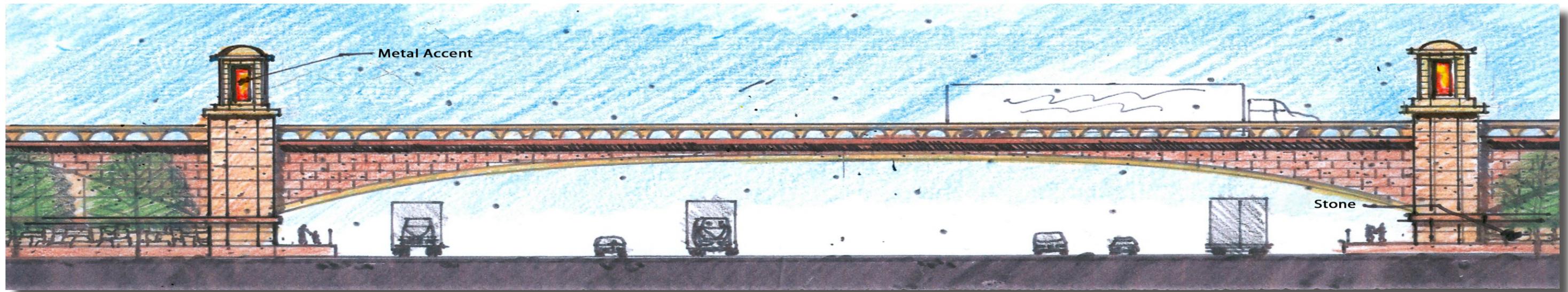
US 24 highway bridge structures will be in keeping with the Victorian architectural heritage of the Westside. See Bridge Treatment Concept Alternatives ‘A’ and ‘B’ for the Urban Design Segment on page 12. Architectural columns should extend above the US 24 roadway and be visible from US 24. These columns provide the opportunity for metal and color accents, along with the potential to display a community logo or other identity elements. Attention should also be paid to bridge railings so they match the Victorian theme. This may result in using CDOT-required safety rail on the roadway side of the bridge and an architectural screening element on the outside face of the structure so that the safety rail is not visible. The use of arched forms is very important, so curtain walls on the bridge faces and arched forms on the face of the retaining walls are desired. See Vehicular Bridge / Retaining Wall Treatment Elevation on page 14. In addition to the unifying material, accent rock, modu-

lar block, brick and concrete can be used. These materials and colors were chosen because they reflect the building material found within the older structures of Old Colorado City and Colorado Springs.

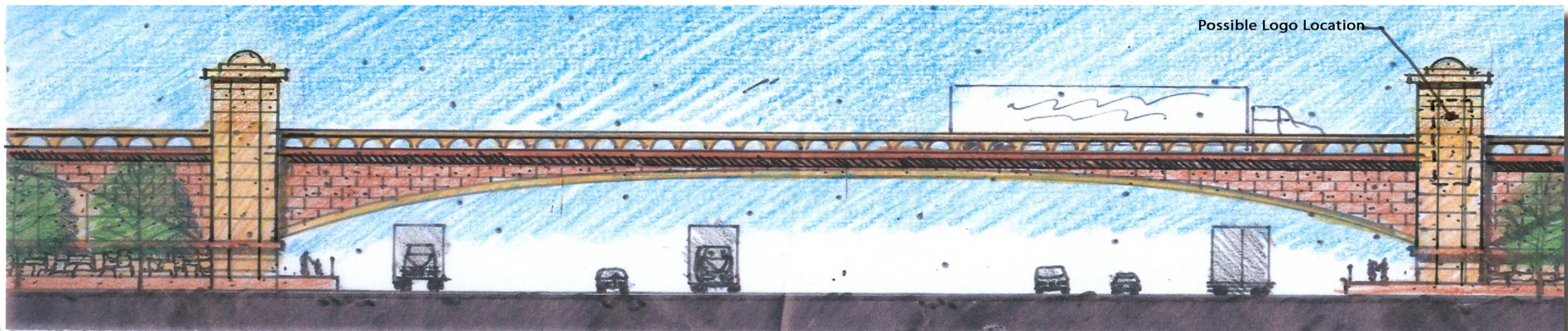
Pedestrian Bridge

The potential pedestrian bridge at 25th Street connects neighborhoods on the south of US 24 with Old Colorado City. It will require a partnership to fund its construction. This pedestrian bridge will span both the US 24 roadway and Fountain Creek. This bridge is actually two bridges. See bridge sketch page 13. A pedestrian ramp will elevate users up to the bridge structure on the south side of US 24. A trestle-style pedestrian bridge will span the highway and connect, at grade, to the elevated trail on the north side of US 24. The second trestle-style bridge will span Fountain Creek and

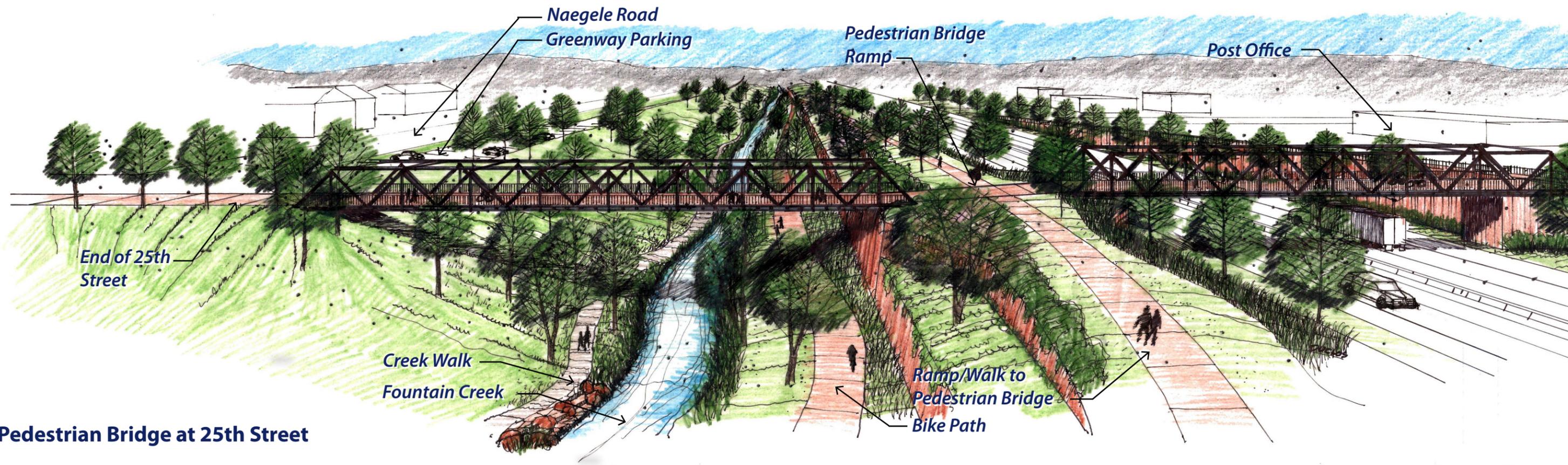
carry users to 25th Street with another ramp for pedestrians to bring them down to existing grade. The trestle design has been selected as a means of paying homage to the once-thriving railroad industry of the area. This also allows the use of a pre-engineered steel structure as long as they are truss structures.



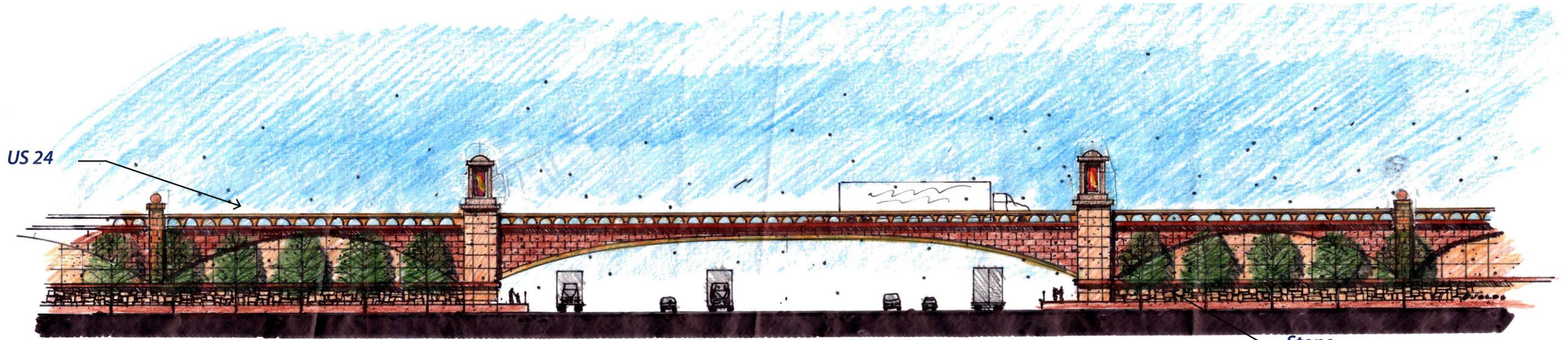
Bridge Treatment Concept Alternative A - Urban Design Segment (8th Street, 15th Street, and 21st Street)



Bridge Treatment Concept Alternative B - Urban Design Segment (8th Street, 15th Street, and 21st Street)



Pedestrian Bridge at 25th Street



Typical Vehicular Bridge/Retaining Wall Treatment

City Streets

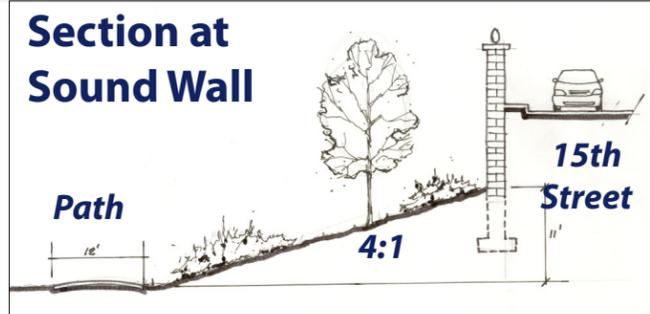
Bridges over Fountain Creek - Typical Bridge Treatment (8th, 21st, 26th and 31st Streets and Ridge Road)

Four vehicular bridges are proposed for construction over Fountain Creek within the Urban Design segment. Because of their association with the creek, much consideration was given on how to marry the bridge architecture with the natural surroundings. An arched concept was developed that pays tribute to the flowing lines and curvilinear nature of the creek. In addition, a natural stone veneer will be used to face the exterior of the bridge. This stone is intended to be the unifying natural accent rock. The overall look of these bridges is reminiscent of the historic stone bridges in Manitou Springs and the Westside of Colorado Springs.



Ashlar Stone Pattern along Westside

walls contain the 100-year flood. See page 17 for the artist concept of the Fountain Creek retaining wall. This wall would be either a CMU or poured-in-place concrete wall with a tooled or stamped grid pattern. On this wall, metal or ceramic artwork can be applied. The community preferred that the artwork reflect an environmental theme. See the Example of Applied Artwork below.



15th Street

A proposed bridge structure is planned at 15th Street that will span US 24 and Fountain Creek. The purpose of this bridge is to connect Gold Hill Mesa with Colorado Avenue. A sound wall is needed along the west bound exit ramp to 15th Street. This wall is intended to prevent vehicular noise impacts to existing residential neighborhoods north of US 24. The sound wall that will parallel this ramp will be almost 1800 feet in length and will continue along the entire ramp.

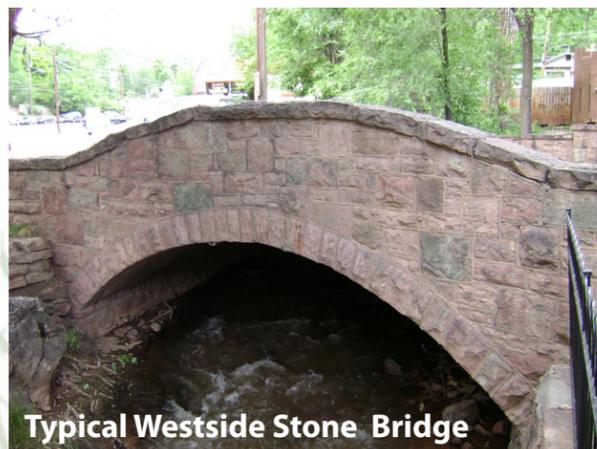
An additional sound wall is required just west of 15th Street. Two alternatives were developed for these walls. Alternative #1 maintains the design vernacular that is similar to the ramp sound wall. Alternative #2 helps to reduce the overall wall size and utilizes a simpler design vernacular.

C. Retaining and Sound Walls

Sound and retaining walls are planned in the Urban Design Segment. These walls will provide both privacy and noise abatement for the surrounding communities, as well as allow for highway trail and creek improvements. They will play an important part in the containment of the 100-year flood plain. Because these walls will be highly visible to both the vehicular traffic and the surrounding community, they will need to be designed to support the character of the Urban Design Segment.

As shown on page 15, sound walls and the retaining walls between the ramps and the US 24 mainline will reflect the Victorian theme. Retaining walls along Fountain Creek shall reflect an environmental theme. These

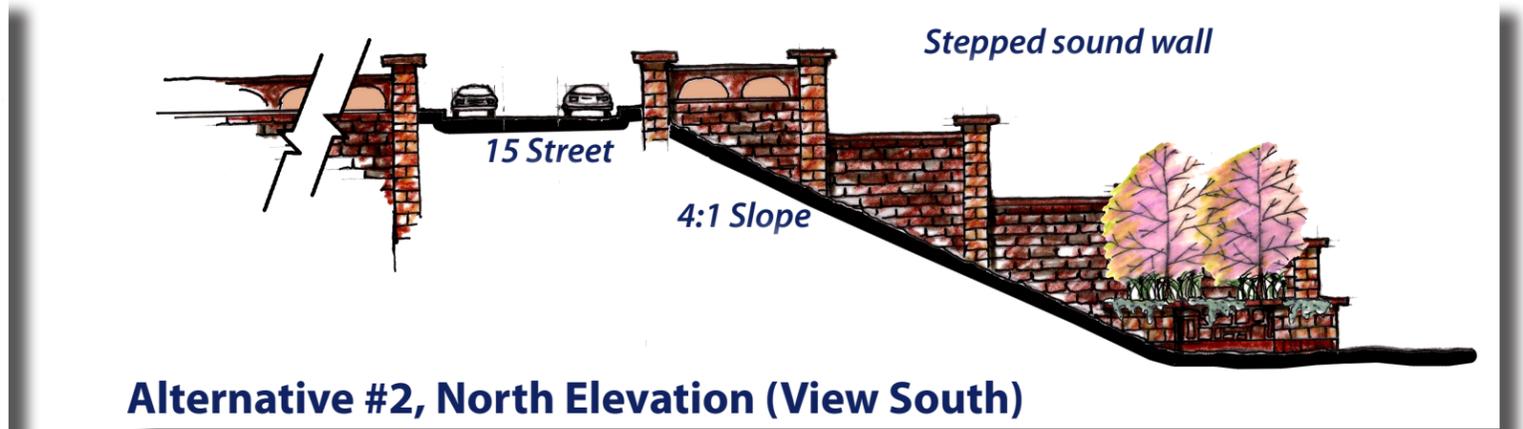
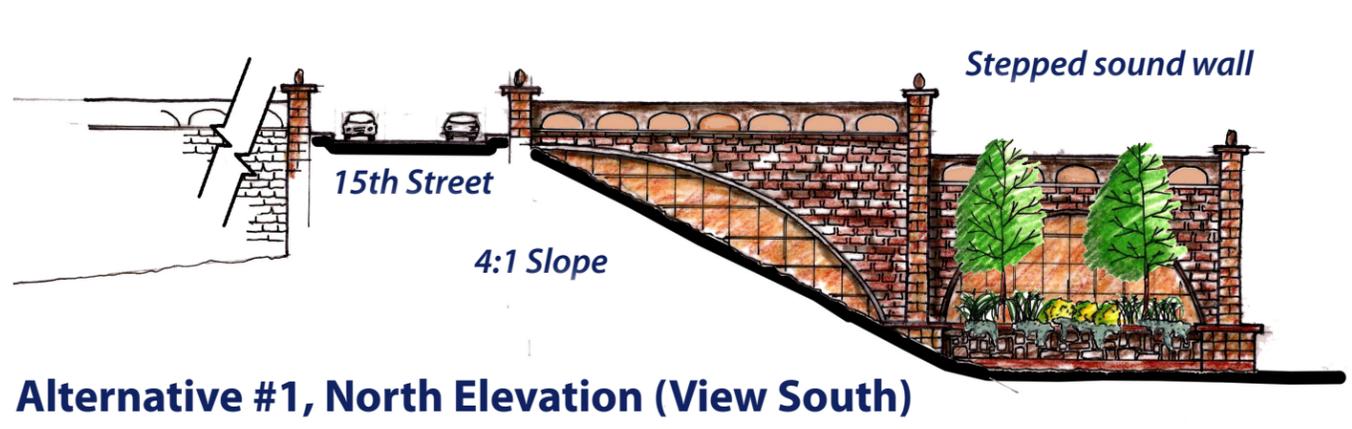
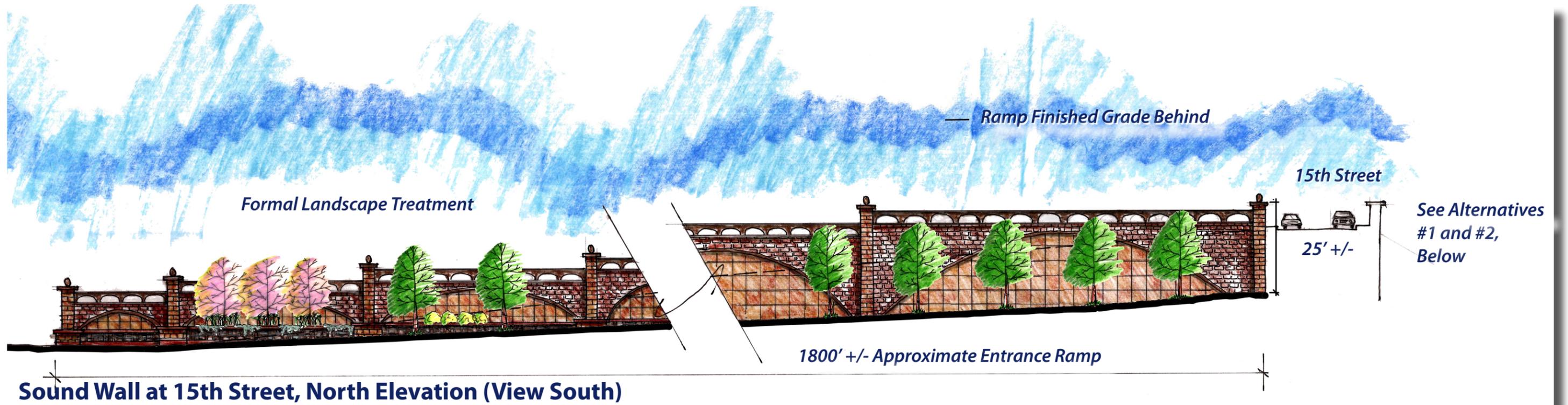
As part of the Midland Greenway (Please see Midland Greenway Master Plan description in the Appendix), a 12' pedestrian trail will be located in the north right-of-way of US 24. The overall height of the sound wall can be reduced by sloping from the path up to the wall. See Section at Sound Wall, this page. The average height of the sound wall is approximately 25' at 15th Street and steps down along the ramp entrance to approximately 12'-15' in height.



Typical Westside Stone Bridge



Applied Artwork - Courtesy of Design Workshop



D. Landscape Elements

Presently, landscaping within this design segment is limited to low growth grasses within the rights-of-way, existing riparian vegetation along Fountain Creek and the formal landscaping associated with existing Miners Statue at 21st Street.

Due to the proposed highway improvements within this design segment (i.e., intersections, trails and retaining/noise walls) new areas will be created for landscaping. Proposed plantings will focus on gateways and the drainage corridors. The implementation of any new landscaping should aid in blending the highway with the adjoining land uses.

Highway Right-of-way

Dryland grasses will be used along all roadway shoulders where stabilization is required and in all areas where other aesthetic treatments have not been identified. Xeric plantings from the Highway R.O.W. Xeric Planting schedule, located in the Appendix, should be installed to blend with the existing non-irrigated landscape.

Riparian Drainage

Landscape materials installed along US 24, adjacent to Fountain Creek, should consist of native grasses that can take advantage of local runoff to allow native vegetation, including trees and shrubs.

As a best management practice, and in order to capture runoff before it enters Fountain Creek, swales will be constructed adjacent to the highway. These swales should be vegetated with grasses and herbaceous plants selected from the Riparian Plantings schedule in the Appendix.

E. Irrigation

Irrigation has only been identified for installation at the proposed gateways. All other landscaped areas will rely on natural rainfall and natural drainage patterns. Plants and vegetation, as identified on the plant schedules in the Appendix, have been carefully selected to withstand periods of drought and to rely solely upon natural rainfall for their source of water.

F. Color and Materials

Colors and materials utilized for the Urban Design Segment of US 24 will be based on the materials and colors of the area. Materials will be based in the built environment and include colored concrete, modular block, brick, tile and stone. The stone utilized in this section will be the same stone as is used for the Rural Design Segment, the unified natural accent rock. This rock shall match the existing stone of the Garden of the Gods and Red Rock Canyon Open Space. The rock shall be red in color with some variations going toward the orange. Pink and salmon colored rock should be avoided. This unifying natural accent rock shall be Navajo Ruby metamorphic quartzite red rock cut and arranged in a random ashlar pattern. See Quarried Stone and Cultured stone images, this page. Cultured stone shall be grouted with black/dark grey grout. Joints shall be raked to minimize grout appearance.



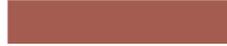
Quarried Stone

As an alternative, a cultured stone may be used. The cultured stone shall be a rough cut ashlar rock pattern with a deep red color similar to the natural rock mentioned above. Deep raked, grouted joints are desired.



Cultured Stone - Random Ashlar Pattern

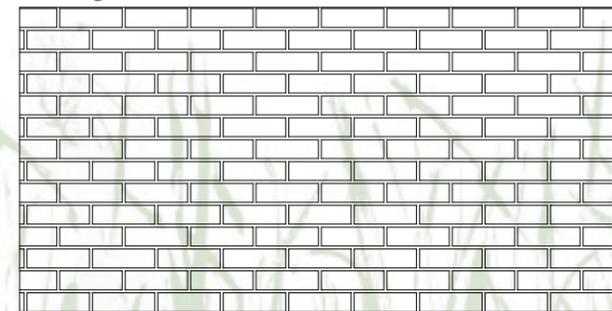
1. Color Palette

	Reds/Browns
	Federal Color-30070
	Federal Color-30075
	Federal Color-30049
	Bufs
	Federal Color-30017
	Federal Color-30107
	Federal Color-37769
	Accents
	Federal Color-32356
	Federal Color-31158
	Federal Color-34120

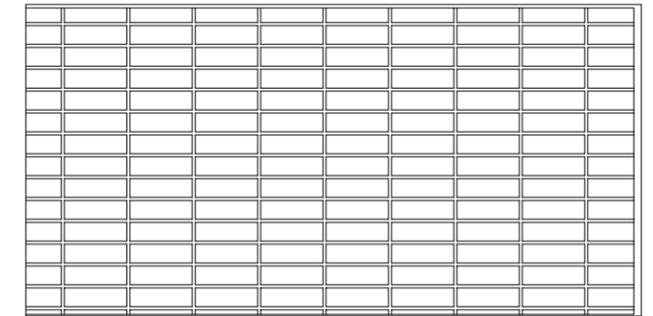
2. Man-made Pattern Palette

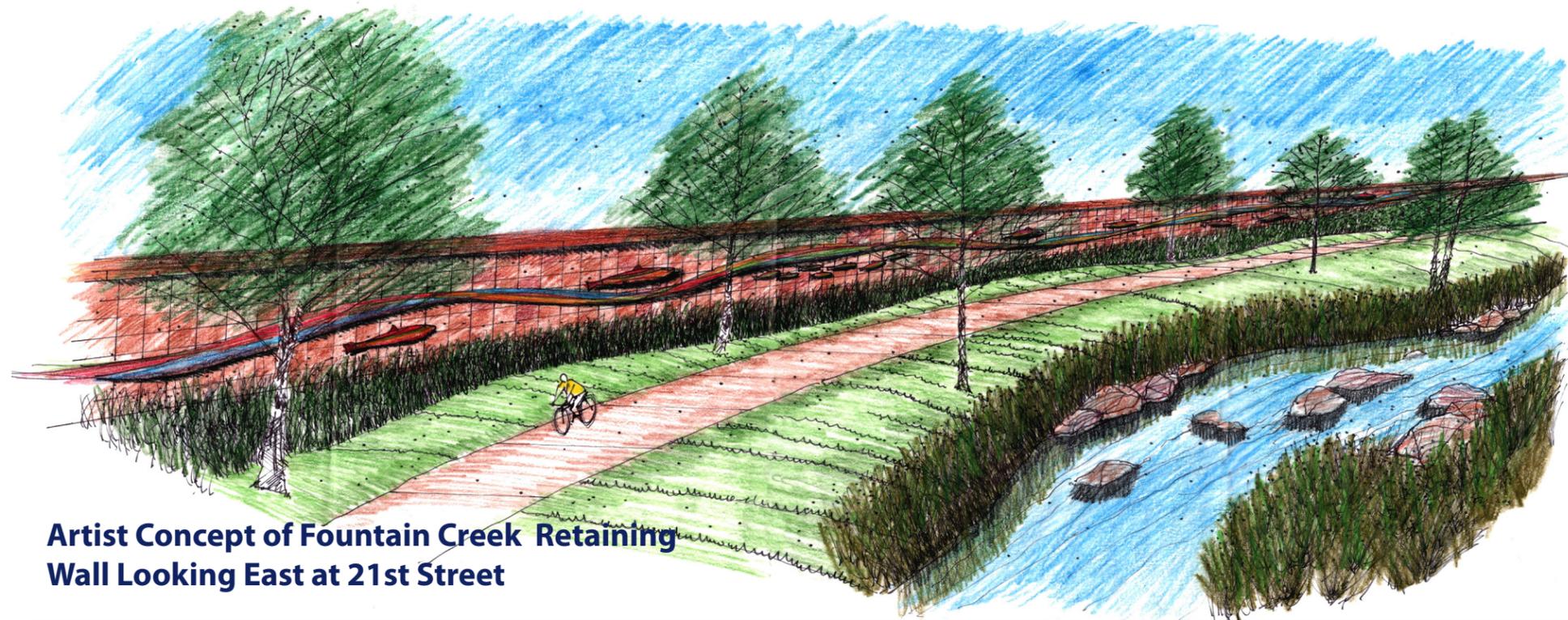
As an alternative to rock, man made modular block, tile or other modular block systems can be used and shall be arranged in either a running or stack bond pattern. Colors will be based upon the existing colors of the natural rock found within this design segment. Deep red with shades going toward orange are acceptable. No salmon or pink colored man-made modular block shall be used.

Running Bond Pattern



Stack Bond Pattern





Artist Concept of Fountain Creek Retaining Wall Looking East at 21st Street



Artist Concept of Restored Fountain Creek Looking West at Gold Hill Mesa

Rural Design Segment

The Rural Design Segment begins just west of the US 24/31st Avenue intersection. Proposed highway improvements will improve vehicular visibility and congestion along the corridor. Aesthetic treatments are envisioned to be implemented as part of the highway improvements that will help to tie the new infrastructure and roadway alignment into the surrounding and adjoining landscape.

A. Gateways

As previously described within the Urban Design Segment, gateways have identifiable features that will relate and tie the proposed highway improvements to the surrounding area. Unlike the Urban Design Segment, the Rural Design Segment does not have the number of improved intersections or proposed interchanges as the Urban Design Segment. This stretch of highway proposes only one bridge structure over Ridge Road. Future ramps, which would be built by others, have not been included.

Ridge Road

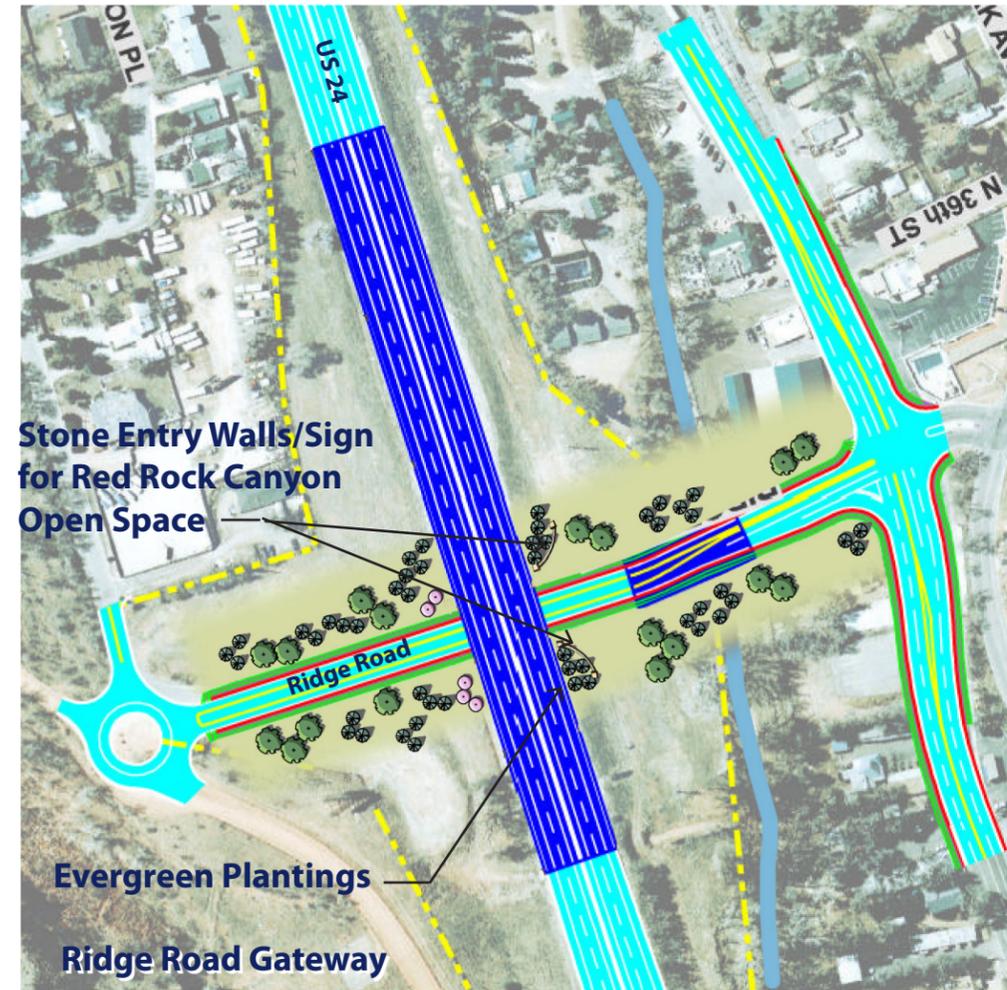
Stone entry walls into Red Rock Canyon Open Space are proposed for Ridge Road. The materials should match the stone and rock found within the open space itself. The use of stone entry walls will incorporate some of the design elements of the proposed bridges. This rock should appear related to the unifying natural accent rock.

The entry walls should be faced with stone that is an ashlar pattern. This pattern is random with differing sizes of stone randomly placed.



Random Ashlar Pattern

The joints between the stones should be deeply raked. The mortar should be an earth tone color to complement the color of the actual stone. See page 21 for a more detailed discussion of material and color. Evergreen trees with native shrubs and grasses are the landscaping anticipated for the gateway. Plants will be selected from the Highway Xeric R.O.W Xeric Plantings schedule in the Appendix. Along Fountain Creek, plants should be selected from the Naturalized Planting and Riparian Planting schedule in the Appendix.



Landscape Gateway Concept