

Welcome to the US 85

Planning and Environmental Linkages Study OPEN HOUSE

Thank you for attending! This is an Open House format, there will be no formal presentation. Please visit the project information boards around the room.

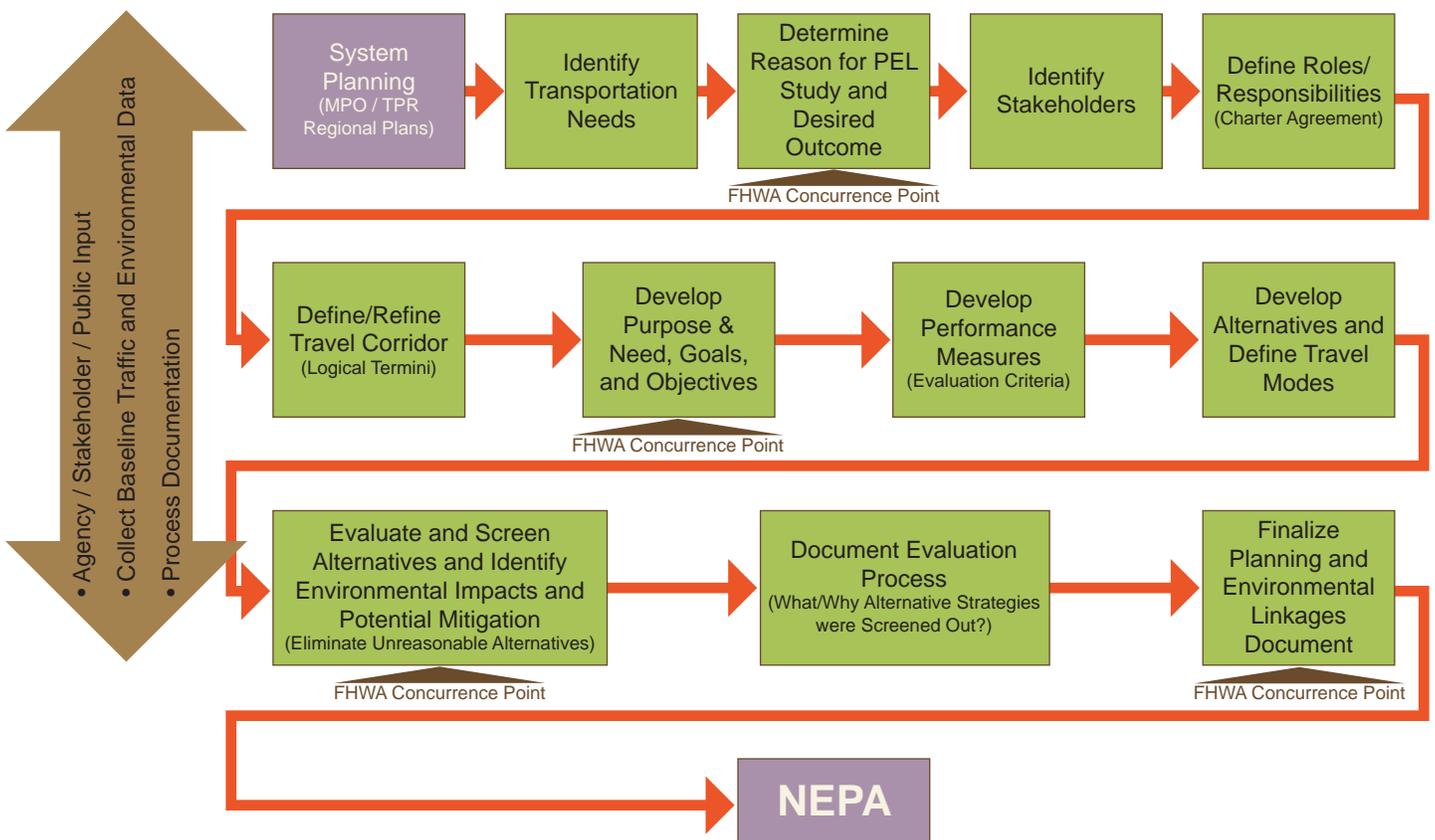
Project team members are available to discuss your questions and comments.



COLORADO
Department of
Transportation

What is a Planning and Environmental Linkages (PEL) Study?

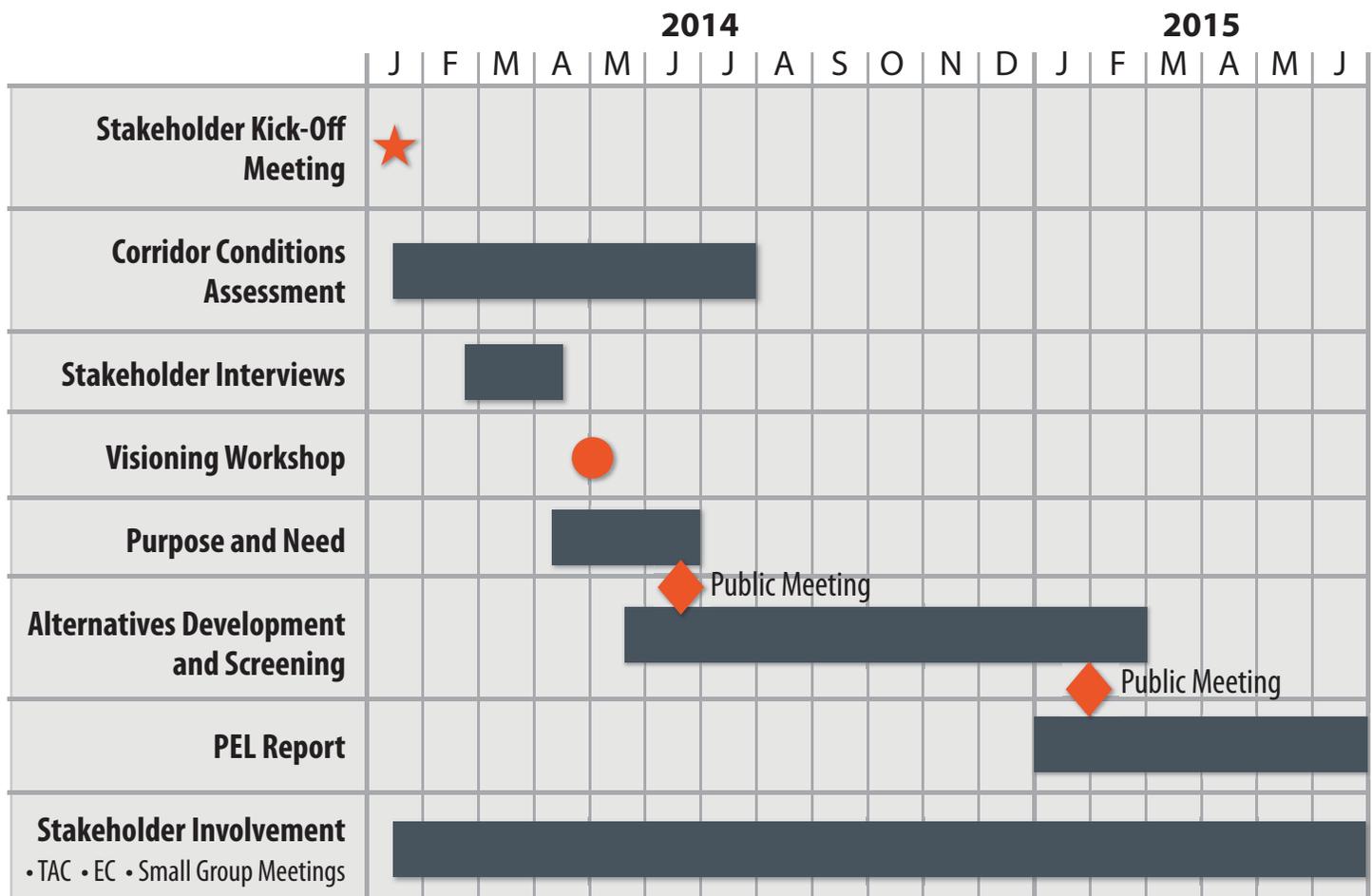
PEL is a study process that is typically used to identify transportation issues and environmental concerns. It can be applied to make planning decisions and for planning analysis. These decisions and analyses, for example, can be used to identify and prioritize future projects, develop the purpose and need for a project, determine project size or length, and/or develop and refine a range of alternatives. PEL studies should be able to link planning to environmental issues and result in useful information that can be carried forward into the National Environmental Policy Act (NEPA) process. The adoption and use of a PEL study in the NEPA process is subject to a determination by the Federal Highway Administration (FHWA).



Study Process and Schedule

The objective of the US 85 PEL Study is to develop a strategic vision for US 85 between I-76 and the Town of Nunn. The goal of the study is to identify safety, operational and other transportation needs along US 85 and determine the short-term and long-term transportation priorities. The study will consider the US 85 Access Control Plan and determine if aspects of that plan need to be refreshed. Short-term and long-term improvements will be prioritized through a collaborative process with stakeholders and the public along the corridor.

The final product will include a series of projects with an implementation plan that will help CDOT and local communities to position the corridor for funding.



Purpose and Need Summary

Purpose of the Proposed Action

The purpose of transportation improvements along the US 85 corridor is to improve safety, reduce existing and future traffic congestion, provide efficient access for existing and future development, and improve multimodal mobility and connectivity.

Need for Proposed Action

These transportation improvements are needed to address:

- ▶ **Safety Problem:** There are a higher than expected number of crashes at several intersections and along the US 85 corridor.
- ▶ **Mobility Problem:** The ability for people to move across and along the corridor is substantially impacted by traffic congestion, inadequate intersections that fail to accommodate users' needs, and unreliable travel times. These conditions are expected to worsen in the future as the region grows.
- ▶ **Access Problem:** The current number, locations, and design of public roadway accesses has contributed to traffic operational and safety deficiencies along the corridor. This is exacerbated by the proximity of the highway and railroad tracks in several locations, which further contributes to operational and safety deficiencies, especially for large commercial vehicles.
- ▶ **Alternative Travel Modes Problem:** Infrastructure for alternative travel modes (transit, pedestrian, and bicycle) along and across US 85 do not sufficiently serve the existing or future needs of populations and travel patterns along the corridor. Transit dependent residents along the corridor have limited or no access to public transportation for essential services, such as medical appointments.



2,600(14%) NB
2,800(12%) SB

6,600(15%) NB
6,700(13%) SB

5,500(18%) NB
5,300(30%) SB

12,500(13%) NB
12,200(13%) SB

11,200(20%) NB
10,200(12%) SB

10,500(20%) NB
11,000(12%) SB

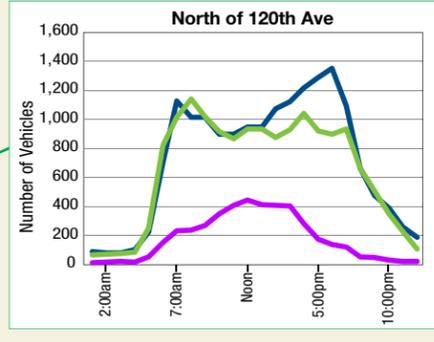
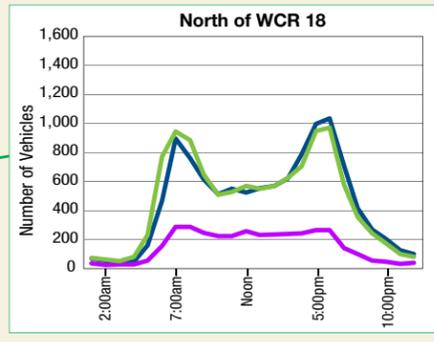
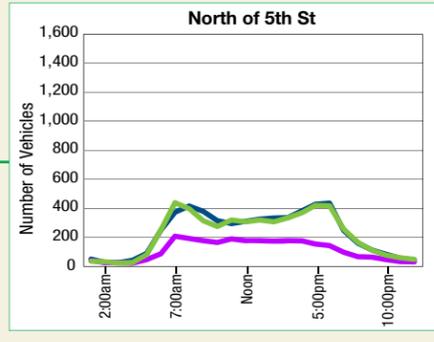
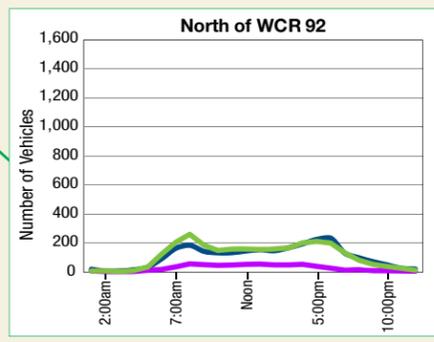
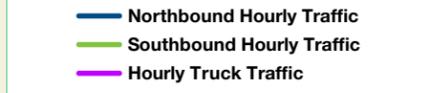
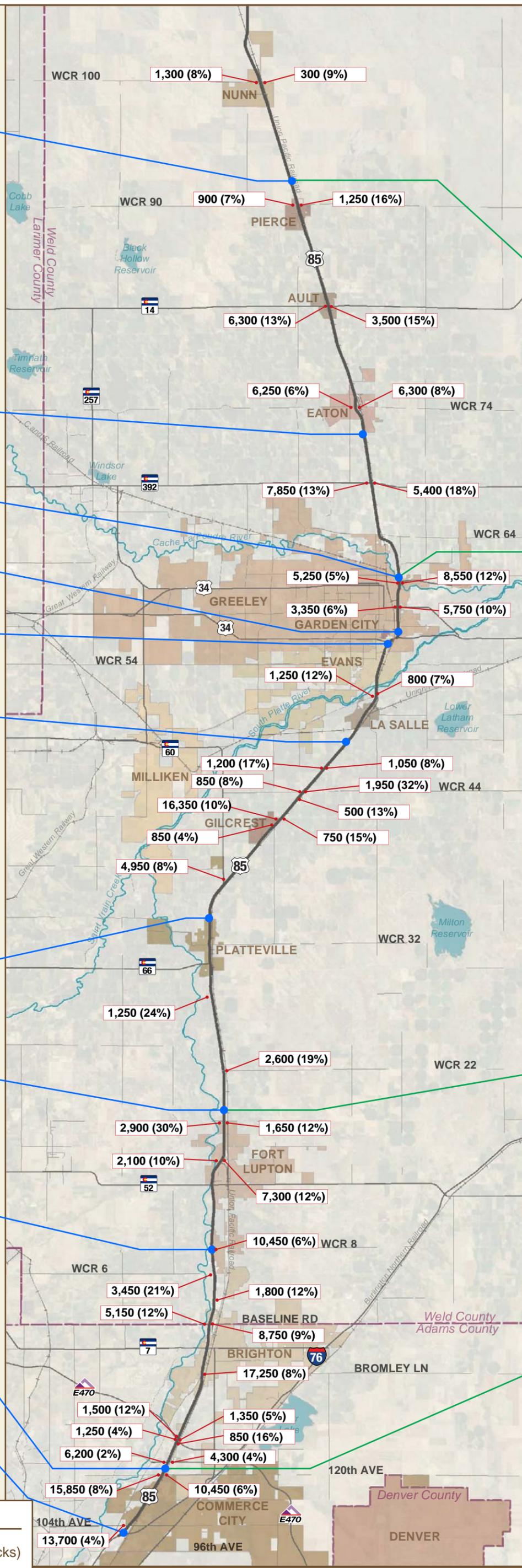
12,300(17%) NB
12,900(13%) SB

11,100(23%) NB
11,200(11%) SB

14,400(18%) NB
14,800(19%) SB

17,300(15%) NB
15,700(11%) SB

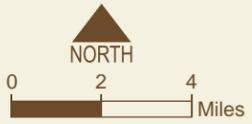
18,600(14%) NB
17,200(14%) SB



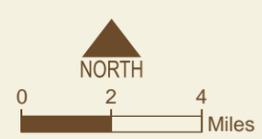
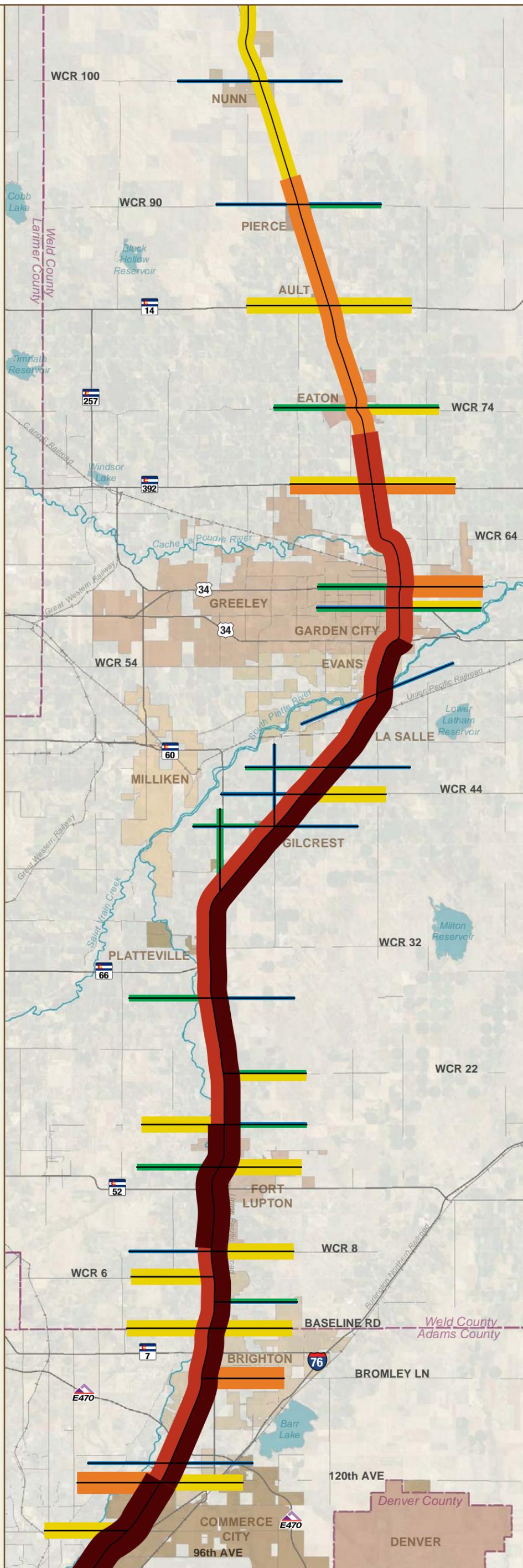
Legend

XX,XXX (XX%)NB
XX,XXX (XX%)SB US 85 ADT (% Trucks)

X,XXX (XX%) Cross Street
Bidirectional ADT
(% Trucks)



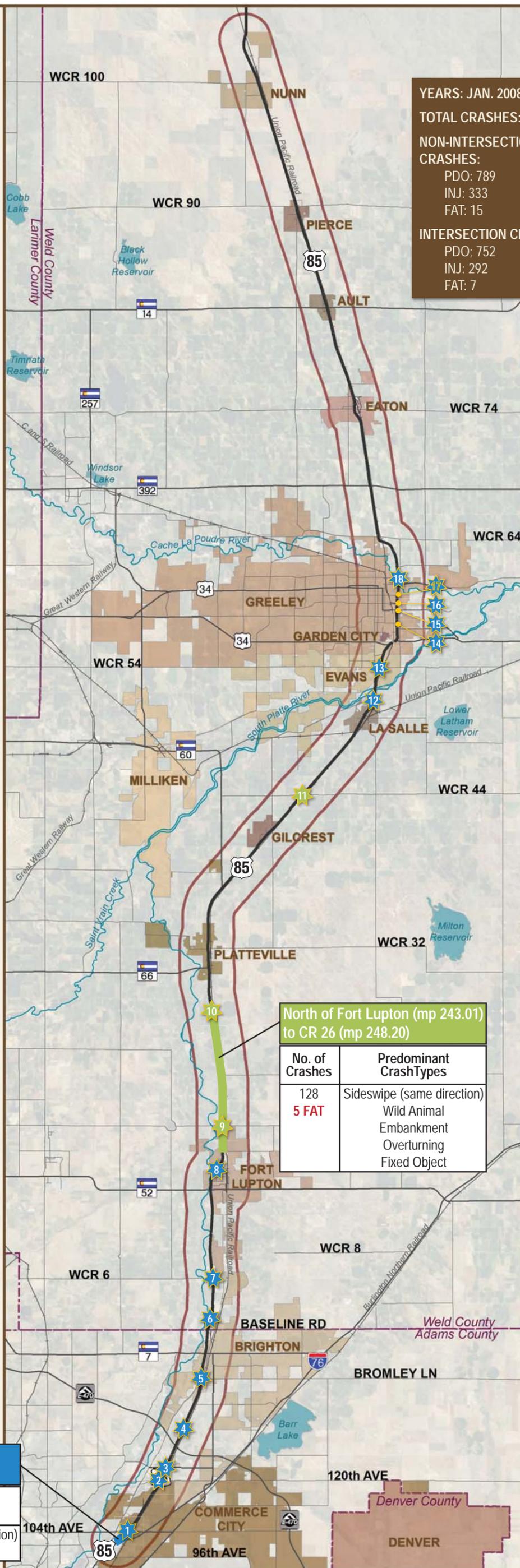
Average Daily Traffic



Legend

- Intersections Identified with Safety Concerns
 - Intersections Adjacent to Railroad
 - Truck Related Safety Concerns
 - Signalized Intersection
 - Side-Street Stop Controlled
 - Urban Segment
 - Rural Segment
- PDO = Personal Damage Only
 INJ = Injury
 FAT = Fatality

YEARS: JAN. 2008 - DEC. 2012	• 63.11 Miles
TOTAL CRASHES: 2,186	• 112 Intersections
NON-INTERSECTION CRASHES: 1,137	• 24 Signalized
PDO: 789	• 57 Adjacent to Railroad Crossing
INJ: 333	• Urban Intersections
FAT: 15	20 Signalized
INTERSECTION CRASHES: 1,049	16 Unsignalized
PDO: 752	• Rural Intersections
INJ: 292	4 Signalized
FAT: 7	72 Unsignalized



8 WCR 14.5/14th St.

No. of Crashes	Predominant CrashTypes
26	Approach Turn
1 FAT	

7 WCR 6

No. of Crashes	Predominant CrashTypes
6	Rear End

6 Denver St.

No. of Crashes	Predominant CrashTypes
12	Fixed Object

5 Bromley Lane

No. of Crashes	Predominant CrashTypes
91	Rear End
1 FAT	

4 136th Ave.

No. of Crashes	Predominant CrashTypes
35	Rear End

3 SH 22/120th Ave.

No. of Crashes	Predominant CrashTypes
40	Rear End

2 120th Ave.

No. of Crashes	Predominant CrashTypes
67	Rear End

1 SH 44/104th Ave.

No. of Crashes	Predominant CrashTypes
115	Rear End
1 FAT	Dark-Lighted

I-76 (mp 226.8) to North of SH 44 (mp 227.47)

No. of Crashes	Predominant CrashTypes
53	Sideswipe (same direction)
	Rear End

North of Fort Lupton (mp 243.01) to CR 26 (mp 248.20)

No. of Crashes	Predominant CrashTypes
128	Sideswipe (same direction)
5 FAT	Wild Animal
	Embankment
	Overtaking
	Fixed Object

18 5th St.

No. of Crashes	Predominant CrashTypes
15	Rear End

17 13th St.

No. of Crashes	Predominant CrashTypes
12	Broadside

16 16th St.

No. of Crashes	Predominant CrashTypes
41	Approach Turn

15 18th St./US 34

No. of Crashes	Predominant CrashTypes
36	Fixed Object

14 22nd St.

No. of Crashes	Predominant CrashTypes
49	Rear End

13 42nd St.

No. of Crashes	Predominant CrashTypes
32	Rear End

12 WCR 52

No. of Crashes	Predominant CrashTypes
8	Broadside

11 WCR 44

No. of Crashes	Predominant CrashTypes
28	Broadside

10 WCR 28

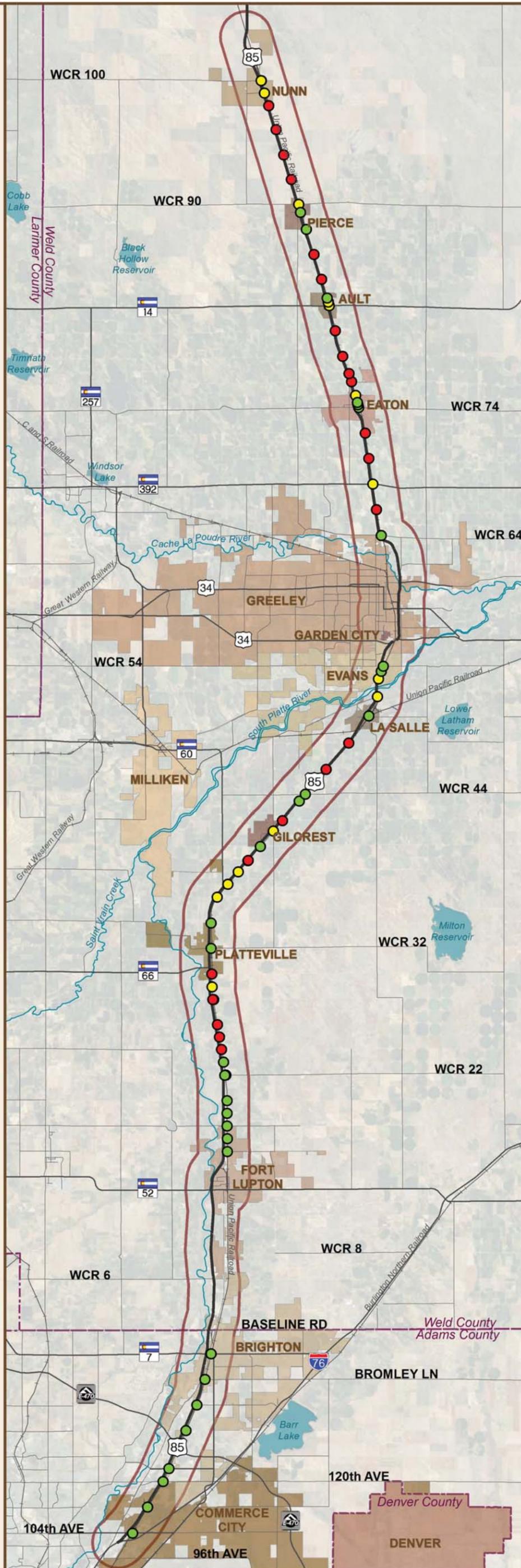
No. of Crashes	Predominant CrashTypes
10	Broadside

9 WCR 18

No. of Crashes	Predominant CrashTypes
23	Approach Turn



High Crash Locations

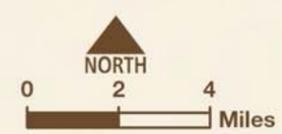


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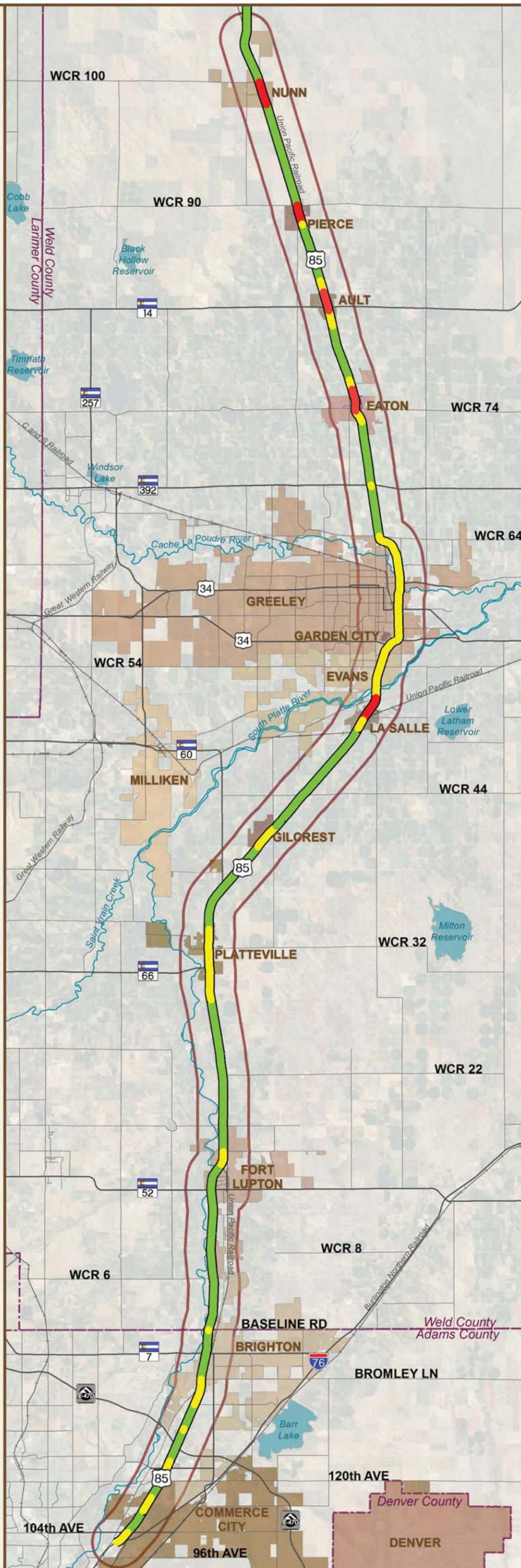
Railroad Crossing Distance

- 0' - 200' from US 85
- 201' - 800' from US 85
- > 800' from US 85

- US 85
- Roads
- Railroad
- ~ Rivers/Streams
- ☪ Lakes
- County Boundary
- Study Area



Railroad Crossing Distance from US 85



Legend

- 35-40 MPH
- 45-50-55 MPH
- 60-65 MPH
- US 85
- Roads
- Railroad
- Rivers/Streams
- Lakes
- County Boundary
- Study Area



Posted Speed Limits



Obstruction too close to roadway



Roadside ditch



Narrow shoulders



Railroad crossing close to US 85



Steep slopes



Steep slopes



Poor sight distance



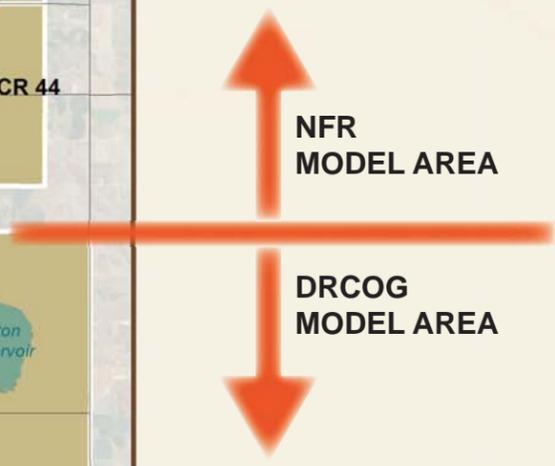
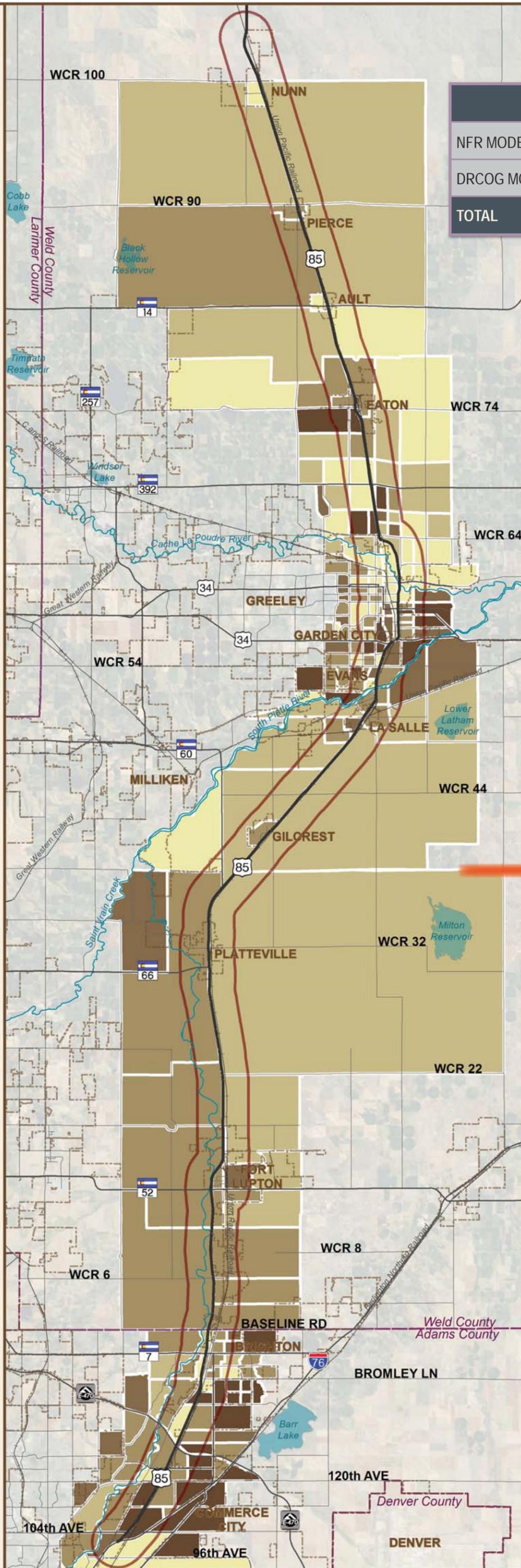
Poor access management

Corridor Deficiencies



	Existing HH	2035 HH	Percent Growth
NFR MODEL AREA	33,401	49,432	48%
DRCOG MODEL AREA	25,645	53,614	109%
TOTAL	59,046	103,046	75%

DRCOG & NFR MPO
Estimated Models (2009-2035)



Legend

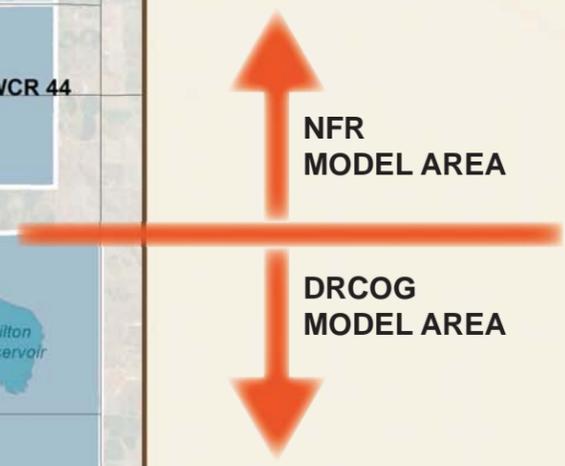
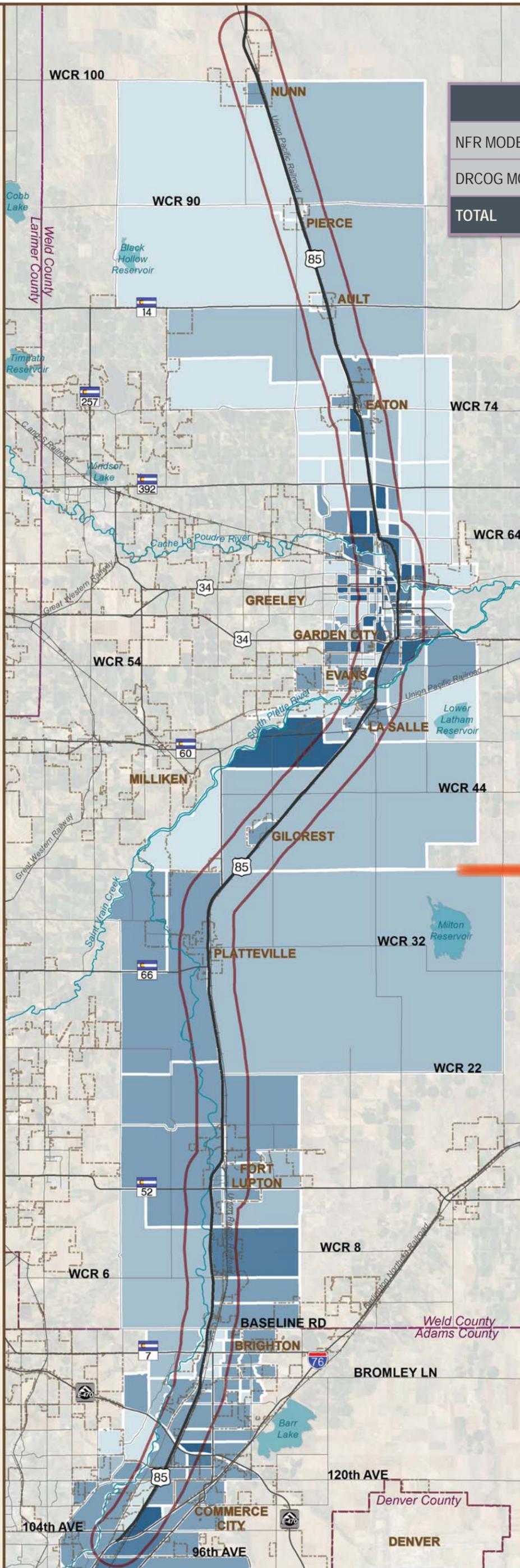
- No Growth
- 0.0 - 0.01 Households per acre
- 0.01 - 0.5 Households per acre
- 0.5 - 1.0 Households per acre
- > 1.0 Households per acre
- US 85
- Roads
- Railroad
- Rivers/Streams
- Lakes
- County Boundary
- Study Area



Estimated Household Density Growth

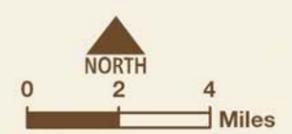
	Existing Jobs	2035 Jobs	Percent Growth
NFR MODEL AREA	41,972	77,634	85%
DRCOG MODEL AREA	25,924	37,839	46%
TOTAL	67,896	115,473	70%

**DRCOG & NFR MPO
Estimated Models (2009-2035)**



Legend

- No Growth
- 0.0 - 0.01 Jobs per acre
- 0.01 - 0.5 Jobs per acre
- 0.5 - 1.0 Jobs per acre
- > 1.0 Jobs per acre
- US 85
- Roads
- Railroad
- Rivers/Streams
- Lakes
- County Boundary
- Study Area



Estimated Employment Density Growth



1,350
55% Growth
2,300
3,600

3,200
30% Growth
6,000
7,700

9,150
60% Growth
13,300
21,300

6,200
60% Growth
10,800
17,400

12,600
60% Growth
17,000
27,000

15,600
40% Growth
24,000
34,100

9,300
95% Growth
17,000
33,000

9,350
135% Growth
17,000
39,800

9,850
135% Growth
19,000
44,900

14,500
45% Growth
33,900
49,700

18,300
40% Growth
36,100
51,500

2,350
5,400
6,200
15% Growth

6,200
9,400
15,600
60% Growth

10,300
14,000
27,100
95% Growth

12,900
19,000
33,700
75% Growth

12,700
20,000
31,500
60% Growth

10,600
21,500
51,500
40% Growth

10,400
25,200
37,300
95% Growth

9,350
22,300
45,200
105% Growth

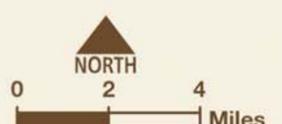
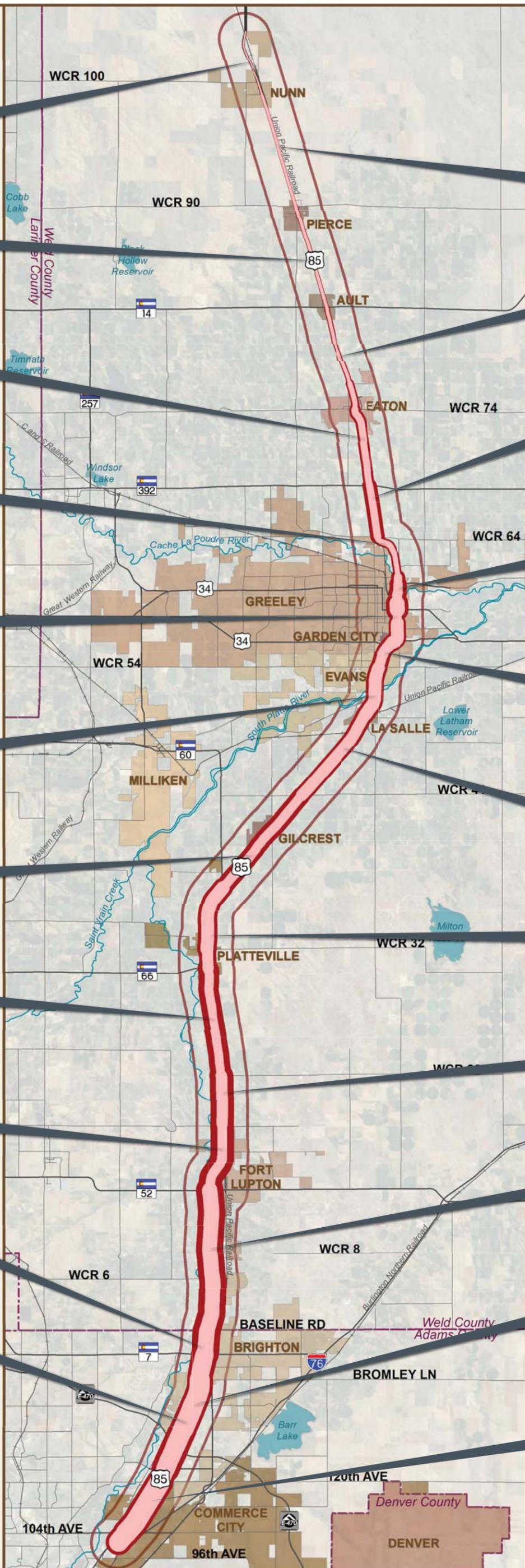
12,500
29,100
52,200
80% Growth

18,300
38,700
50,900
30% Growth

19,400
38,500
50,600
30% Growth

Legend

- XXXX 1990 Daily Traffic Volumes
- XXXX Existing Daily Traffic Volumes
- XXXX 2035 Daily Traffic Projections



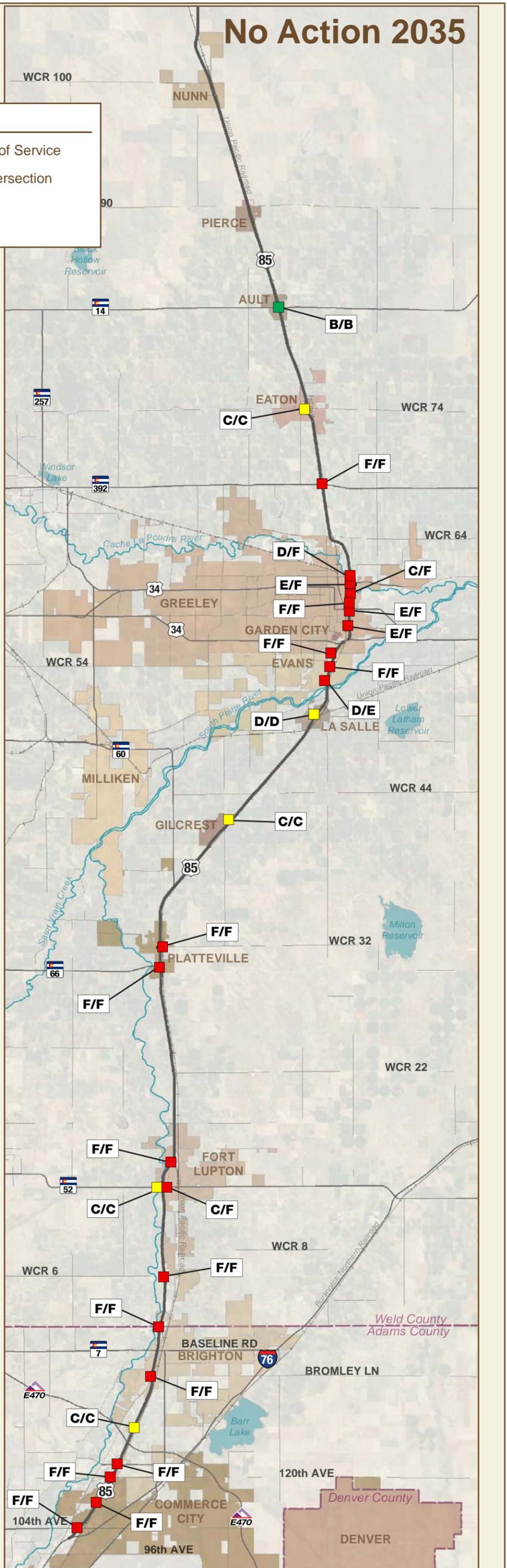
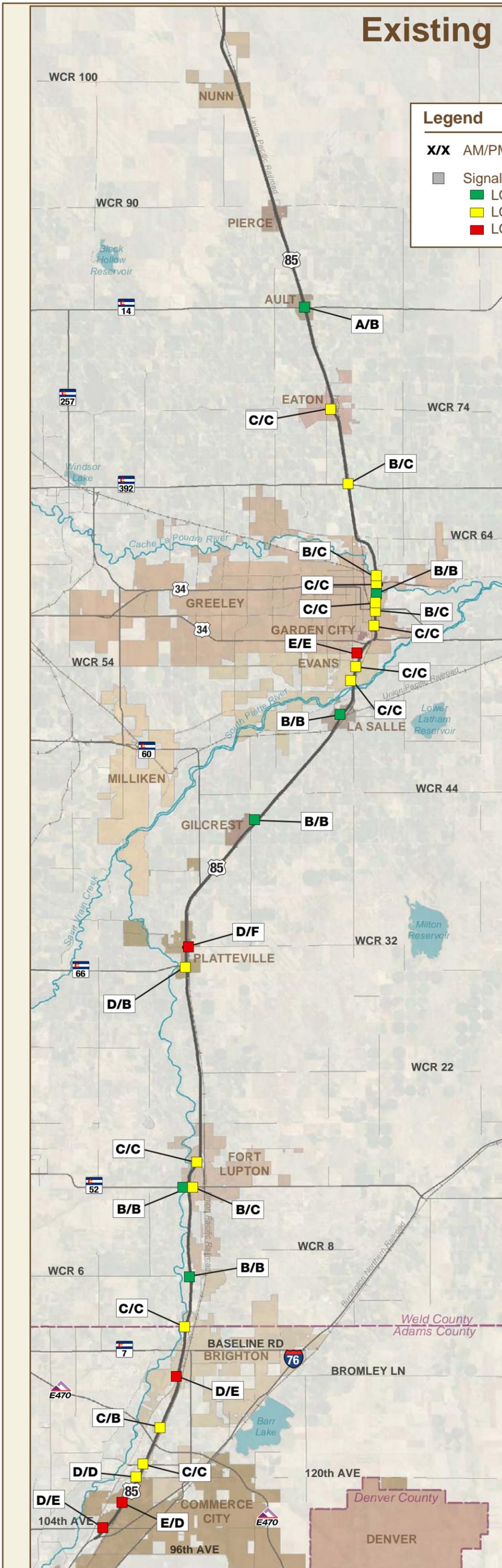
Historic, Existing, and Future Traffic Volumes

Existing

No Action 2035

Legend

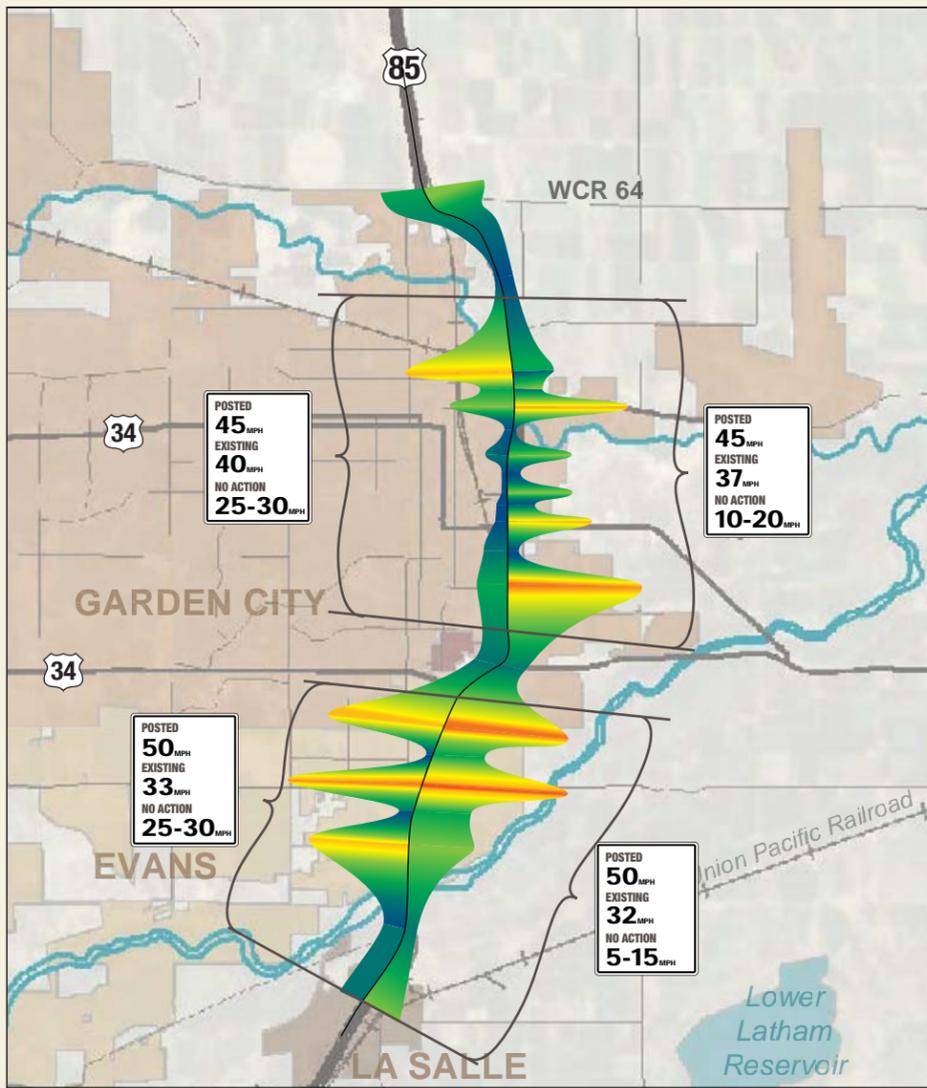
- X/X** AM/PM Level of Service
- Signalized Intersection
- LOS A/B
- LOS C/D
- LOS E/F



Existing and 2035 No Action Level of Service

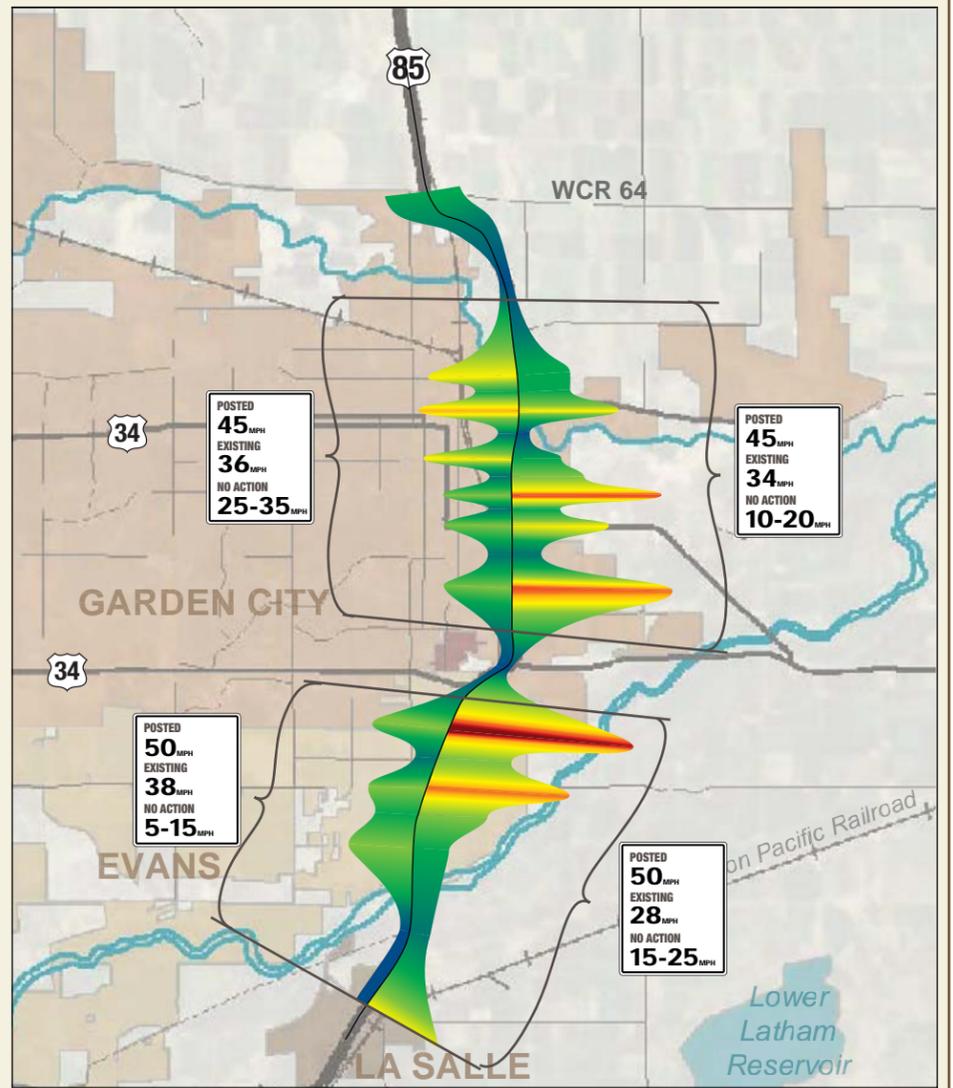
AM

Northern Area
Access Category: Expressway

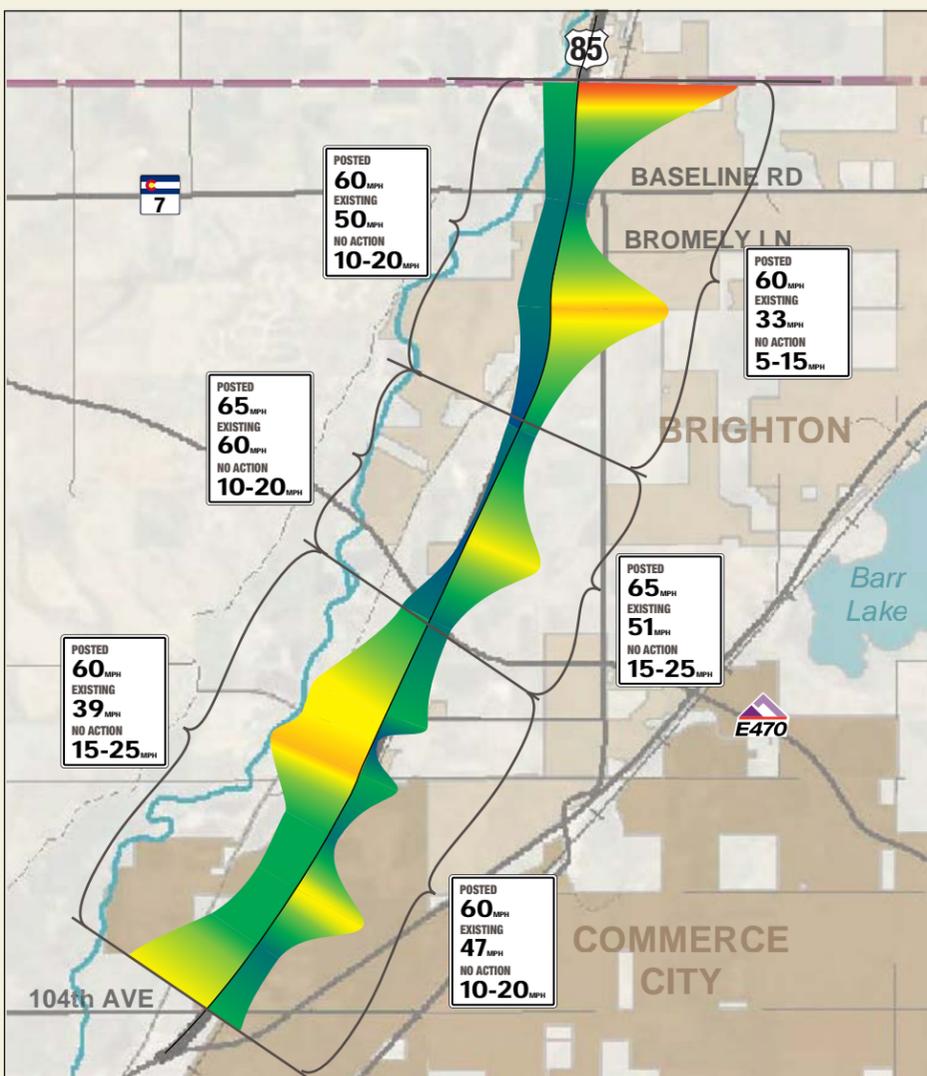


PM

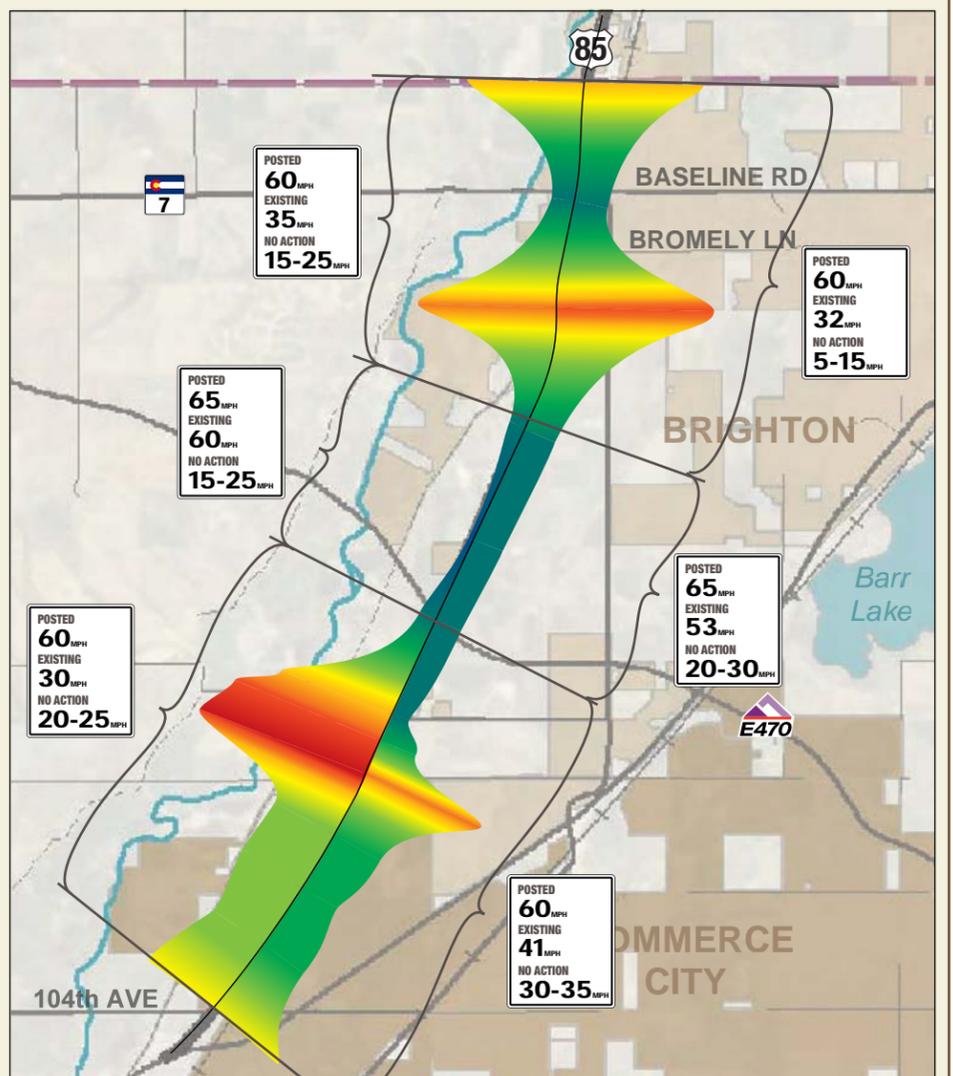
Northern Area
Access Category: Expressway



Southern Area
Access Category: Expressway

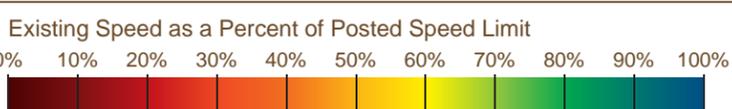


Southern Area
Access Category: Expressway



US 85 Existing and Projected Vehicle Speeds

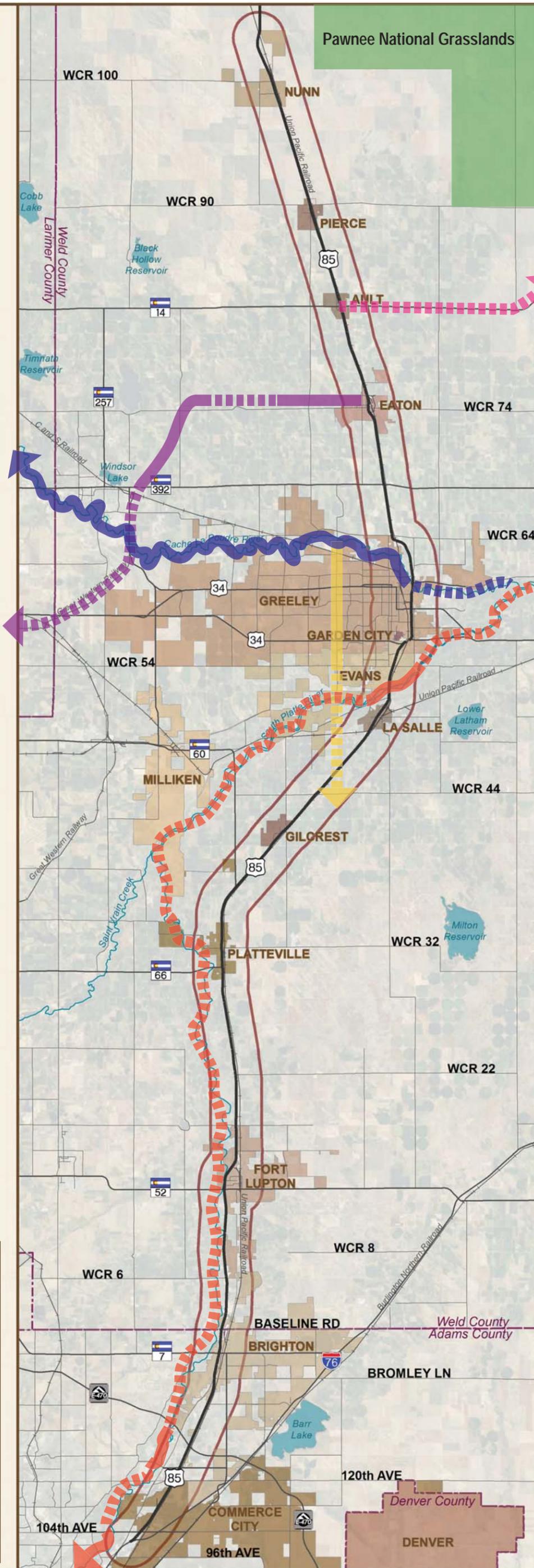
Legend



POSTED XX^{MPH}
EXISTING XX^{MPH}
NO ACTION XX^{MPH}

Average Posted Speed Limit
Average Existing Speed
Average No Action Speed (Year 2035)*

*Future traffic with signal timing optimization

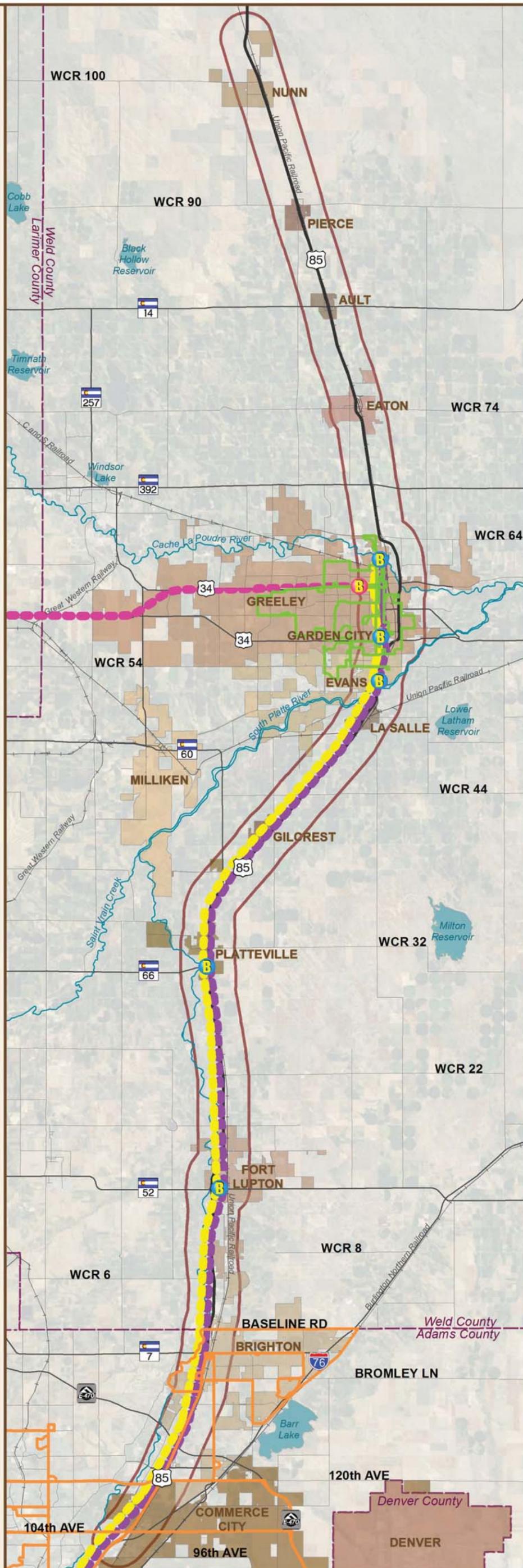


Legend

- Existing Corridor Alignment
- Envisioned Corridor Alignment
- Poudre River Trail
- Pawnee Pioneer Trail
- Great Western
- South Platte / American Discovery Trail / Colorado Front Range Trail
- Greeley / LaSalle



Regional Bicycle and Trail Corridors



Legend

Existing

- Existing Bus Station
- Greeley-Evans Transit Fixed Bus Routes
- RTD Fixed Bus Routes

Proposed

- N I-25 EIS ROD Phase 1 Commuter Bus Stations
- Express Bus (20 minute peak headway)
- Inter-Regional Express/Commuter Bus (60 minute peak and off-peak headway)
- Essential Service Denver to Greeley (~ 3 times per week)
- US 85
- Roads
- Railroad
- Rivers/Streams
- Lakes
- County Boundary
- Study Area



Existing and Proposed Transit Service